

**Notice of Meeting:**

I hereby give notice that an ordinary meeting of the Dunedin City Council will be held on:

**Date:** Thursday 23 April 2026  
**Time:** 9.00 am  
**Venue:** Council Chamber, Dunedin Public Art Gallery, the Octagon, Dunedin

Sandy Graham  
Chief Executive Officer

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**Council**  
**PUBLIC AGENDA**

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**MEMBERSHIP**

**Mayor**  
**Deputy Mayor**

Mayor Sophie Barker  
Cr Cherry Lucas

**Members**

Cr John Chambers	Cr Christine Garey
Cr Doug Hall	Cr Marie Laufiso
Cr Russell Lund	Cr Mandy Mayhem
Cr Benedict Ong	Cr Andrew Simms
Cr Mickey Treadwell	Cr Lee Vandervis
Cr Steve Walker	Cr Brent Weatherall

**Senior Officer**

Sandy Graham, Chief Executive

**Governance Support Officer**

Lynne Adamson

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Lynne Adamson  
Governance Support Officer

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[www.dunedin.govt.nz](http://www.dunedin.govt.nz)

**Note:** *Reports and recommendations contained in this agenda are not to be considered as Council policy until adopted.*

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**1 OPENING**

Rev Te Ata Roy, Māori Chaplain Otago University and Otago Polytechnic will open the meeting with a prayer/karakia.

**2 PUBLIC FORUM**

At the close of the agenda public forum registrations were still being taken. The speakers will be confirmed following the closure of registrations 24 hours before the meeting begins i.e. 9.00 am on Wednesday 22 April 2026.

**3 APOLOGIES**

At the close of the agenda no apologies had been received.

**4 CONFIRMATION OF AGENDA**

Note: Any additions must be approved by resolution with an explanation as to why they cannot be delayed until a future meeting.

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## DECLARATION OF INTEREST

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### EXECUTIVE SUMMARY

1. Members are reminded of the need to stand aside from decision-making when a conflict arises between their role as an elected representative and any private or other external interest they might have.
2. Elected members are reminded to update their register of interests as soon as practicable, including amending the register at this meeting if necessary.
3. Staff members are reminded to update their register of interests as soon as practicable.

### RECOMMENDATIONS

That the Council:

- a) **Notes/Amends** if necessary the Elected Members' Interest Register attached as Attachment A; and
- b) **Confirms/Amends** the proposed management plan for Elected Members' Interests.
- c) **Notes** the proposed management plan for the Executive Leadership Team's Interests.

### Attachments

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Council Interest Register 31 March 2026				
Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
Mayor Sophie Barker	Shareholder	Ayrmed Limited	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Shareholder	Various publicly listed companies	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Property Owner	Residential Property Owner - Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Beneficiary	Sans Peur Trust (Larnach Castle)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Mentor	Business Mentors NZ	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Vegetable Growers Club	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Alexander McMillan Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Patron	New Zealand International Science Festival	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Patron	Dunedin Horticultural Society	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Institute of Directors	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Chairperson	Dunedin Heritage Fund (Council Appointment)	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Grow Dunedin Partnership (Council Appointment)	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Heritage Advisory Group (Council Appointment)	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Local Government New Zealand (Zone 6) (Council Appointment)	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Co-Chair	Ōtepoti Dunedin Destination Management Plan Advisory Panel (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Family Member	Family Member employed at Wilkinson Rogers	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.	
Member	Tertiary Precinct Planning Group (Council Appointment)	No conflict Identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.	
Cr John Chambers	Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Rental Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Otakau Golf Club	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Opera Otago	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Otago Settlers Association (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Toitū Otago Settlers Museum Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Member	Hereweka Harbour Cone Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Okia Reserve Management Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Waikouaiti Coast Community Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Christine Garey	Trustee	Garey Family Trust - Property Ownership - Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Daughter employee	Halo Project	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Ashburn Hall Charitable Trust Board	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Creative Dunedin Partnership (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Performing Arts Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
		Sophia Charter (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	St Paul's Cathedral Foundation (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Theomin Gallery Management Committee (Olveston) (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Doug Hall	Trustee	Cronus Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Clickfix Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	District Licensing Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Public Art Gallery Society (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Toitū Otago Settlers Museum Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	West Harbour Community Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Marie Laufiso	Property Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Moray Place Community Building Trust - which owns property 111 Moray Place	Duty to Trust may conflict with duties of Council Officer	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Women of Ōtepoti Recognition Initiative	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Corso Ōtepoti Dunedin Trust	Potential grants recipient	Withdraw from discussion and leave the table. If in public excluded leave the room. Seek advice prior to the meeting.
	Dunedin Branch Treasurer	P.A.C.I.F.I.C.A Inc	Potential grants recipient	Withdraw from discussion and leave the table. If in public excluded leave the room. Seek advice prior to the meeting.
	Expert Panel Member	Health Coalition Aotearoa Public Health Infrastructure Committee	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	The Ōtepoti Community Builders Charitable Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee/Secretary	Refugee Support Group	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Member	Dunedin Abrahamic Interfaith Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Former Refugee Steering Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Puketai Residential Centre Liaison Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Social Wellbeing Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Cherry Lucas	Trustee	Otago Farmers Market	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Partner	Southway Enterprises	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Henderson Lucas Family Trust - Residential Dunedin Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	NZ Institute of Chartered Accountants	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Shanghai Association (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Local Government New Zealand (Zone 6) (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Tūhura Otago Museum Trust Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Ōtepoti Dunedin Destination Management Plan Advisory Panel (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Taieri Airport Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Tertiary Precinct Planning Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Te Poāri a Pukekura (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Mosgiel-Taieri Community Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Russell Lund	Shareholder	Loan & Mercantile Trust includes:	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Produce Place Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Dunedin Grain Store Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director/Shareholder	Loan & Mercantile 2000 Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Shareholder	Lund South Trust includes:	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director/Shareholder	Lund South Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director/Shareholder	Lund Dunedin Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director/Shareholder	Resource Values Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Sherwood Manor Properties Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Director/Shareholder	Lund Central Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director/Shareholder	Lund South Administration Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Construction Operatives Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Lund South Properties Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	RV Lund Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	BDCRS Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Lund Frankton Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	President	Ariki Amateur Athletic & Harrier Club	Ariki is a member of Athletics Otago which receives grant funding from DCC.	Withdraw from discussion and leave the table. If in public excluded leave the room. Seek advice prior to the meeting.
	Member	Heritage Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Otago Theatre Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Mandy Mayhem	Chairperson	Waitati Hall Society Inc	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Chairperson	Keep Ōtepoti Dunedin Beautiful	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Co-ordinator	Emergency Response Group, Blueskin area	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Blueskin Bay Amenities Society	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Blueskin A & P Society	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Zone Representative and Board Member	Keep New Zealand Beautiful	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Coastal Community Cycleway Network	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Waitati Music Festival Committee	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Property Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Disability Issues Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Gasworks Museum Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Keep Dunedin Beautiful (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Performing Arts Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Social Wellbeing Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Benedict Ong	Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Shareholder	Listed Stocks	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Otago Settlers Association (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Toitū Otago Settlers Museum Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Andrew Simms	Director	Landseer Motor Investments Limited	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Landseer Motor Investments Auckland Limited t/a Andrew Simms - Motor vehicle retail	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Stephen Duff Motors Limited t/a Andrew Simms Dunedin - Motor vehicle retail	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Three Diamond Automotive t/a Ralliart NZ - Race car preparation	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Cambridge Finance Limited - Financial Services	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	The Landseer Group Limited - Investments	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Otago Motorhome Centre Limited - Motor vehicle retail	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Landseer Motor Investments Henderson Limited - Motor vehicle retail	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Landseer Motor Investments Moorhouse Limited - Motor vehicle retail	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Minaret Property Investments Limited - Property Investment	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	The Newfoundland Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	The Moturata Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Taieri Trails Group	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Taieri Cricket Club	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Mosgiel AFC	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Commercial Property, Andersons Bay Road, Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Heritage Fund (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Heritage Advisory Group (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Tūhura Otago Museum Trust Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Taieri Airport Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Micky Treadwell	Director	Atawhai Interactive Tapui Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Contractor	Otago Polytechnic	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Co-owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Member	Green Party of Aotearoa	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Otaru Sister City Society (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Ice Sports Dunedin Incorporated (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Ōtepoti Dunedin Live Music Advisory Panel (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Te Ao Tūroa Partnership (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Otago Peninsula Community Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Lee Vandervis	Director	Lee Vandervis, Antonie Alm-Lequeux and Cook Allan Gibson Trustee Company Ltd - Residential Property Ownership - Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Director	Bunchy Properties Ltd - Residential and Lifestyle Farm Property Ownership - Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Various publicly Audio and Lighting - Hire, Sales and Service Business	May contract and provide service to DCC	Withdraw from discussion and leave the table. If the meeting is in public excluded leave the room. Seek advice prior to the meeting.
	Member	District Licensing Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Heritage Fund (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Okia Reserve Management Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Steve Walker	Trustee	Dunedin Wildlife Hospital Trust	Potential grants recipient	Withdraw from discussion and leave the table. If the meeting is in public excluded leave the room. Seek advice prior to the meeting.
	Member	Orokonui Ecosanctuary	Potential grants recipient	Withdraw from discussion and leave the table. If the meeting is in public excluded leave the room. Seek advice prior to the meeting.
	Member	New Zealand Labour Party	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Residential Property - Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Shareholder	Various publicly listed companies	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	NZ Sea Lion Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Justice of the Peace		No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Predator Free Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Edinburgh Sister City Society (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Heritage Fund (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Art Gallery Acquisitions Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Hereweka Harbour Cone Trust (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	NZ Masters Games Trust Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Name	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
	Member	Otago Regional Transport Committee (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Ōtepoti Dunedin Live Music Advisory Panel	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Predator Free Dunedin (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Cr Brent Weatherall	Owner	Residential Property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Owner	Business George Street, Dunedin	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Brent Weatherall Jeweller Limited	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Weatherall Trustee Company	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Trustee	Residential Rental Properties	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Club	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Dunedin Public Art Society (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Keep Dunedin Beautiful (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Toitū Otago Settlers Museum Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	Member	Strath Taieri Community Board (Council Appointment)	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

Executive Leadership Team - Register of Interest - current as at 18 March 2026					
Name	Date of Entry	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
Sandy Graham	25/07/2019	Owner	Residential property Dunedin	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	25/07/2019	Member	St Clair Golf Club	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	17/09/2024	Vendor	Property purchased by senior member of ORC staff.	No conflict identified	Transaction was arms length through an agent with no direct interaction.
	17/09/2024	Client	Various local contractors (glazing, carpet, fencing and kitchen upgrades)	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	12/11/2025	Family member	Family member works for the DCC	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Nicola Morand	09/05/2022	Owner	Residential Property Dunedin	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	09/05/2022	Owner	Residential Property in Otago	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	20/09/2023	Trustee	Riki Te Mairiki Taiao Trust	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	09/05/2022	Partner	Morand Painting & Decorating	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	12/11/2025	Member	Te Runanga o Otakou	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	12/11/2025	Member	Kati Huirapa Runaka ki Puketeraki	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
	12/11/2025	Family member	Family member works for the DCC	No conflict identified	Seeks advice in advance of meeting if actual conflict arises.
David Ward	28/07/2022	Director	Ward Property Rentals	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	28/07/2022	Member	Water New Zealand	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	28/07/2022	Member	IPWEA (Institute of Public Works Engineering Australasia)	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	21/02/2024	Owner	Residential Property Dunedin	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	28/07/2022	Fellow	The Institution of Civil Engineers	No conflict identified.	Any decisions relating to The Institution of Civil Engineers will be referred to the CEO
Scott MacLean	23/01/2024	Owner	Residential property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	23/01/2024	Trustee	Te Poari a Pukekura Charitable Trust	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	23/01/2024	Spouse is Chair	Dunedin Wildlife Hospital Trust (DWHT)	DCC has funded the DWHT	Take no part in discussions or decision making about the Trust or participate in any transactions between the Trust and DCC.

Executive Leadership Team - Register of Interest - current as at 18 March 2026					
Name	Date of Entry	Responsibility (i.e. Chairperson etc)	Declaration of Interests	Nature of Potential Interest	Member's Proposed Management Plan
Carolyn Allan	01/03/2024	Owner	Residential property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	01/03/2024	Owner	Residential rental property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	01/03/2024	Member	Mountain Bike Otago	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	16/03/2026	Family member	Junior Estimator at DBC building Services	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Paul Henderson	15/01/2025	Owner	Residential property	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	15/01/2025	Playing Member	Dunedin City Royal Football Club	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	10/10/2025	Navigator Member	Taituarā	No conflict identified.	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
	10/10/2025	Partner	Ruru Workplace Solutions Ltd	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.
Mike Costelloe	10/10/2025	Owner	Residential properties x 2	No conflict identified	Seek advice prior to the meeting if actual or perceived conflict of interest arises.

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## CONFIRMATION OF MINUTES

### ORDINARY COUNCIL MEETING - 25 MARCH 2026

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#### RECOMMENDATIONS

That the Council:

- a) **Confirms** the public part of the minutes of the Ordinary Council meeting held on 25 March 2026 as a correct record.

#### Attachments

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## **Council**

### **MINUTES**

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**Minutes of an ordinary meeting of the Dunedin City Council held in the Council Chamber, Dunedin Public Art Gallery, the Octagon, Dunedin on Wednesday 25 March 2026, commencing at 9:01 a.m.**

#### **PRESENT**

**Mayor** Mayor Sophie Barker  
**Deputy Mayor** Cr Cherry Lucas

**Members**

Cr John Chambers	Cr Christine Garey
Cr Doug Hall	Cr Russell Lund
Cr Mandy Mayhem	Cr Benedict Ong
Cr Andrew Simms	Cr Mickey Treadwell
Cr Lee Vandervis	Cr Steve Walker
Cr Brent Weatherall	

#### **IN ATTENDANCE**

Sandy Graham (Chief Executive), Carolyn Allan (Chief Financial Officer), Scott MacLean (General Manager City Services), David Ward (General Manager 3 Waters, Property and Urban Development), Nicola Morand (Manahautū - General Manager Community and Strategy), Paul Henderson (General Manager Corporate and Regulatory Services), Mike Costelloe (General Manager, Arts, Culture and Economic Development), Janet Fraser (Corporate Planner), Bonnie Wright (Manager Compliance Solutions), Paula Dickel (Strategic Property Advisor and Jackie Harrison (Manager Governance), Karilyn Canton (Senior In-House Legal Counsel) and Michael Garbett (Partner, Anderson Lloyd)

**Governance Support Officer** Lynne Adamson

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#### **1 OPENING**

Rebecca Post opened the meeting with a prayer on behalf of the Baha'i community.

The Mayor spoke on Standing Orders use at Council and Committee meetings.

#### **2 PUBLIC FORUM**

There were 5 speakers at Public Forum.

2.1 Duncan Eddy

Mr Eddy spoke to his precirculated information regarding vacant land in Port Chalmers. He requested that Council write to Kāinga Ora and relevant central government ministers seeking a recommitment to the planned social housing rebuild on Albertson Avenue, noting the prior demolition of social housing, tenant relocations, and assurances that tenants would have first right of refusal to the rebuilt housing.

Mr Eddy responded to questions.

2.2 Jo Galer

Ms Galer spoke about Council's recent public consultation on projects including cycleways, the Town Belt trial, and the George Street upgrade. She also commented on upcoming projects and the need to prioritise solutions for South Dunedin, as well as ongoing roading and footpath maintenance.

Ms Galer responded to questions.

2.3 Flynn Nisbett

Mr Nisbett spoke on behalf of the Otago University Students' Association and shared results from a student survey. He noted support for the weekly street bin collection and requested its extension to North Dunedin, with glass waste identified as a priority, consistent with the Sophia Charter.

He identified car parking as the main concern for students living near the University and sought Council consideration of subsidised or free resident permits, along with promotion of car pooling and free bus use.

Mr Nisbett responded to questions

Moved (Mayor Sophie/Cr Cherry Lucas):

That the Council:

**Extends** the meeting beyond 30 minutes.

**Motion carried**

2.4 Jennifer Thomas

Ms Thomas spoke on behalf of the Dunedin Area Citizens Association, expressing concern about current Council expenditure and the impact of potential rates increases on pensioners. She cited examples of projects considered non-essential.

Ms Thomas stated that the Association would prefer funding to be directed toward ongoing local issues and projects that directly affect ratepayer health and safety, including remediation of flooding issues on Surrey Street.

Ms Thomas responded to questions.

2.5 Andrew Whiley

Mr Whiley spoke about Forsyth Barr Stadium and the Peninsula Connection Project. He encouraged continued investment in the stadium as a key city asset and advocated for increased use, including more events aimed at the student population and events such as New Year's Eve celebrations.

Mr Whiley also spoke in support of completing the Peninsula Connection Project, outlining its history and the significant health and safety issues that existed prior to the upgrade. He noted the benefits of the completed sections, including the shared pathway, and encouraged members to travel the route to see the safety improvements first hand.

Mr Whiley responded to questions.

### **3 APOLOGIES**

There was an apology for absence from Cr Marie Laufiso.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

**Approves** the apology from Cr Marie Laufiso.

**Motion carried (CNL/2026/066)**

### **4 CONFIRMATION OF AGENDA**

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

**Confirms** the agenda with the following alterations:

That Items 9 - Code of Conduct Complaint in relation to Councillor Benedict Ong; and Item 11 – Governance and Remuneration Update be taken before Items 7 - Actions from Resolutions of Council meetings and 8 Council Forward Work Programme; and

That Item 17 – Supporting Documents to the Annual Plan Consultation Document be taken before Item 10 – Annual Plan Consultation Document; and

That C5 – DCHL Update and C4 – Investment Framework be taken at 11.00 am to accommodate availability of external representatives to speak to the reports.

**Motion carried (CNL/2026/067)**

## 5 DECLARATIONS OF INTEREST

Members were reminded of the need to stand aside from decision-making when a conflict arose between their role as an elected representative and any private or other external interest they might have.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Notes** the Elected Members' Interest Register; and
- b) **Confirms** the proposed management plan for Elected Members' Interests.
- c) **Notes** the proposed management plan for the Executive Leadership Team's Interests.

**Motion carried (CNL/2026/068)**

## 6 CONFIRMATION OF MINUTES

### 6.1 ORDINARY COUNCIL MEETING - 19 FEBRUARY 2026

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Confirms** the public part of the minutes of the Ordinary Council meeting held on 19 February 2026 as a correct record.

**Motion carried (CNL/2026/069)**

### 6.2 ORDINARY COUNCIL MEETING - 24 FEBRUARY 2026

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Confirms** the public part of the minutes of the Ordinary Council meeting held on 24 February 2026 as a correct record.

**Motion carried (CNL/2026/070)**

**REPORTS**

**9 CODE OF CONDUCT COMPLAINT IN RELATION TO COUNCILLOR BENEDICT ONG**

A report from Civic advised that a Code of Conduct complaint had been laid by the Chief Executive against Cr Benedict Ong. Cr Ong sent an email raising concerns about a staff member, to the Chief Executive and the media simultaneously. The report provided the Code of Conduct investigation to the Council.

The DCC Code of Conduct set out the process to be followed in such an investigation. This process had been followed, and the Council must now decide on its response.

Michael Garbett (Partner, Anderson Lloyd) provided procedural advice on the discussion and consideration of this item.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Temporarily** suspends Standing Orders pursuant to clause 27(4) of Schedule 7 of the Local Government Act 2022 to permit Cr Ong to speak to the Code of Conduct for a period of up to 30 minutes, in accordance with clause 13.6 of the Standing Orders.

**Motion carried (CNL/2026/071)**

Cr Ong spoke to the Code of Conduct.

Cr Ong then withdrew from the item.

During discussion, Mayor Sophie vacated the Chair and Cr Cherry Lucas assumed the Chair

Moved (Cr Cherry Lucas/Cr Steve Walker):

That the Council

**Adjourns** the meeting.

**Motion carried**

The meeting adjourned at 10.16 and resumed at 10.17 am.

Mayor Sophie resumed the Chair at 10.17 am.

Mayor Sophie vacated the Chair at 10.18 am and Cr Cherry Lucas assumed the Chair.

Moved (Cr Cherry Lucas/Cr Steve Walker):

That the Council:

**Adjourns** the meeting.

**Motion carried**

The meeting adjourned at 10.18 am and reconvened at 10.20 am.

Mayor Sophie resumed the Chair at 10.21 am

Moved (Mayor Sophie/Cr Mandy Mayhem):

That the Council:

**Adjourns** the meeting.

**Motion carried**

The meeting adjourned at 10.21 am and reconvened at 10.22 am.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Decides** that:
  - i) Cr Ong has materially breached the Dunedin City Council Code of Conduct;  
and
  - ii) invites Cr Ong to consider In accordance with clause 13.1 of the Dunedin City Council Code of Conduct, Council resigning from the Council.

**Division**

The Council voted by division

For: Crs John Chambers, Christine Garey, Doug Hall, Cherry Lucas, Mandy Mayhem, Andrew Simms, Mickey Treadwell, Steve Walker, Brent Weatherall and Mayor Sophie Barker (10).

Against: Crs Russell Lund and Lee Vandervis (2).

Abstained: Nil

The division was declared CARRIED by 10 votes to 2

**Motion carried (CNL/2026/072)**

**11 GOVERNANCE AND REMUNERATION UPDATE**

A report from Civic advises the Council that the Mayor has exercised her discretionary power under Section 41A(3) of the Local Government Act 2002 (LGA) to remove Councillor Ong from his role as Portfolio Deputy Technology.

The Mayor now sought Council approval to remove Councillor Ong from his roles as the Council Representative on the Otago Settlers Association and the Toitū Otago Settlers Museum Board.

The report also sought approval to amend the Councillors remuneration proposal adopted at the Council meeting on 11 December 2025 to reflect the updated roles of responsibility. The

amended remuneration proposal would be submitted to the Remuneration Authority for approval.

Michael Garbett (Partner, Anderson Lloyd) provided procedural advice, confirming that Councillors were permitted to participate in discussions on remuneration. He noted that an exemption under the Members' Interests Act allowed both participation and voting on this matter.

Mayor Sophie spoke to the report.

It was agreed that resolutions a – c be taken together and d – g be taken separately, together.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Notes** Cr Ong's removal as the Portfolio Deputy Technology.
- b) **Approves** the removal of Cr Ong from the roles of Council Representative on the Otago Settlers Association and the Toitū Otago Settlers Museum Board.
- c) **Appoints** Cr Chambers as the Council Representative on the Otago Settlers Association and the Toitū Otago Settlers Museum Board.

**Division**

The Council voted by division

For: Crs John Chambers, Christine Garey, Doug Hall, Cherry Lucas, Russell Lund, Mandy Mayhem, Andrew Simms, Mickey Treadwell, Lee Vandervis, Steve Walker, Brent Weatherall and Mayor Sophie Barker (12).

Against: Cr Benedict Ong (1).

Abstained: Nil

The division was declared CARRIED by 12 votes to 1

**Motion carried (CNL/2026/073)**

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- d) **Notes** responsibility for the Portfolio Deputy Technology will be reconsidered following the completion of the by-election.
- e) **Approves** annual remuneration of \$100,577 for Councillors.
- f) **Approves** annual remuneration of \$84,496 for Councillors with no additional responsibilities.
- g) **Notes** the updated remuneration proposal reflects changes to Councillor Ong's responsibilities.
- h) **Notes** the updated remuneration proposal will be resubmitted to the Remuneration Authority for approval.

**Division**

The Council voted by division

For: Crs John Chambers, Christine Garey, Doug Hall, Cherry Lucas, Russell Lund, Mandy Mayhem, Mickey Treadwell, Steve Walker, Brent Weatherall and Mayor Sophie Barker (10).  
Against: Crs Benedict Ong, Andrew Simms and Lee Vandervis (3).  
Abstained: Nil

The division was declared CARRIED by 10 votes to 3

**Motion carried (CNL/2026/074)**

**RESOLUTION TO EXCLUDE THE PUBLIC**

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

Pursuant to the provisions of the Local Government Official Information and Meetings Act 1987, exclude the public from the following part of the proceedings of this meeting namely:

<b>General subject of the matter to be considered</b>	<b>Reasons for passing this resolution in relation to each matter</b>	<b>Ground(s) under section 48(1) for the passing of this resolution</b>	<b>Reason for Confidentiality</b>
C4 Investment Framework	s48(1)(d) Check to make report confidential.	s48(1)(d) The exclusion of the public from the part of the meeting is necessary to enable the local authority to deliberate in private on its decision or recommendation.	Commercial sensitivity.
C5 DCHL Update	s48(1)(d) Check to make report confidential.	s48(1)(d) The exclusion of the public from the part of the meeting is necessary to enable the local authority to deliberate in private on its decision or recommendation.	Commercial sensitivity.
C1 Ordinary Council meeting - 24 February 2026 - Public Excluded	S7(2)(b)(ii) The withholding of the information is necessary to protect information where the making available of the information would be likely unreasonably to prejudice the	.	

commercial position of the person who supplied or who is the subject of the information.

S7(2)(g)

The withholding of the information is necessary to maintain legal professional privilege.

S7(2)(h)

The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.

S7(2)(i)

The withholding of the information is necessary to enable the local authority to carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).

C2 Confidential  
Council Action List  
Update

S7(2)(b)(ii)

The withholding of the information is necessary to protect information where the making available of the information would be likely unreasonably to prejudice the commercial position of the person who supplied or who is the subject of the information.

S48(1)(a)

The public conduct of the part of the meeting would be likely to result in the disclosure of information for which good reason for withholding exists under section 7.

S7(2)(g)

The withholding of the information is necessary to maintain legal professional privilege.

C3 Confidential  
 Council Forward Work  
 Programme

S7(2)(h)  
 The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.

S7(2)(i)  
 The withholding of the information is necessary to enable the local authority to carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).

S7(2)(a)  
 The withholding of the information is necessary to protect the privacy of natural persons, including that of a deceased person.

S48(1)(a)  
 The public conduct of the part of the meeting would be likely to result in the disclosure of information for which good reason for withholding exists under section 7.

S7(2)(d)  
 The withholding of the information is necessary to avoid prejudice to measures protecting the health and safety of members of the public.

S7(2)(g)  
 The withholding of the information is necessary to maintain legal professional privilege.

S7(2)(h)  
 The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.

S7(2)(i)

The withholding of the information is necessary to enable the local authority to carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).

This resolution is made in reliance on Section 48(1)(a) of the Local Government Official Information and Meetings Act 1987, and the particular interest or interests protected by Section 6 or Section 7 of that Act, or Section 6 or Section 7 or Section 9 of the Official Information Act 1982, as the case may require, which would be prejudiced by the holding of the whole or the relevant part of the proceedings of the meeting in public are as shown above after each item; and

That Paul Doorn (Chairperson, Dunedin Venues Management Ltd; Tim Loan (Chair, DCHL), Greg Anderson (Deputy Chair DCHL), Chris Milne (Director DCHL), Susie Johnstone (Director DCHL), Shane Ellison (Director DCHL), Peter Hocking (General Manager DCHL), Richard Davey (Treasurer DCTL) and Kirstie Gillon-Wood (Executive Assistant DCHL) be permitted to attend the meeting after the public has been excluded, because of their knowledge of Item C4 and C5. This knowledge, which would be of assistance in relation to the matters being discussed, was relevant because they would be reporting on the item under consideration.

Adjourns the meeting.

**Motion carried (CNL/2026/075)**

The meeting adjourned at 10.51 am and resumed in public at 2.28 pm.

**7 ACTIONS FROM RESOLUTIONS OF COUNCIL MEETINGS**

A report from Civic provided an update in the implementation of resolutions made at Council meetings.

The Chief Executive (Sandy Graham) and General Managers responded to questions.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Notes** the Open and Completed Actions from resolutions of Council meetings.  
**Motion carried (CNL/2026/076)**

**8 COUNCIL FORWARD WORK PROGRAMME**

A report from Civic provided the updated Council forward work programme for the 2025 year.

The Chief Executive (Sandy Graham) and General Managers responded to questions.

Mayor Sophie left the meeting at 2.45 pm and Cr Cherry Lucas assumed the Chair.

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Council:

- a) **Notes** the updated Council forward work programme.

**Motion carried (CNL/2026/077)**

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Council:

**Extend** the meeting beyond 6 hours.

**Motion carried**

## **17 SUPPORTING DOCUMENTS TO THE ANNUAL PLAN 2026/27 CONSULTATION DOCUMENT**

A report from Civic sought approval of the remaining supporting document to the Annual Plan 2026/27 consultation document, to support upcoming community engagement.

The report noted that other supporting documents had already been considered and adopted by the Council at its meeting on 4-5 March 2026.

The Chief Financial Officer (Carolyn Allan) and Corporate Planner (Janet Fraser) spoke to the report and responded to questions.

Cr Doug Hall left the meeting at 2.56 pm and returned at 2.59 pm.  
Mayor Sophie returned to the meeting at 3.01 pm and assumed the Chair.

Moved (Cr Cherry Lucas/Mayor Sophie Barker):

That the Council:

- a) **Adopts** the 2026/27 rating method funding impact statement at Attachment A, for the purposes of developing the Annual Plan 2026/27 and consulting with the community.

**Motion carried (CNL/2026/078)** with Cr Lee Vandervis recording his vote against.

## **10 ANNUAL PLAN 2026/27 CONSULTATION DOCUMENT**

A report from Civic presented the Annual Plan 2026/27 consultation document which provided an explanation on any significant or material changes from year two of the 9 year plan 2025-34 (i.e., from 1 July 2026 to 30 June 2027) for consideration. It was based on the decisions made by Council at its meeting on 4-5 March 2026.

The Chief Financial Officer (Carolyn Allan) and Corporate Planner (Janet Fraser) spoke to the report and responded to questions.

Cr Andrew Simms left the meeting at 3.18 pm and returned at 3.20 pm.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Approves** the 'Annual Plan 2026/27 consultation document'.
- b) **Delegates** the Chief Executive the authority to make any minor editing required to the approved consultation document.
- c) **Adopts** the 'Annual Plan 2026/27 consultation document'.

**Motion carried (CNL/2026/079)** with Cr Benedict Ong recording his vote against

## **12 PROPOSED ROAD STOPPING - ADJOINING 59 CREAMERY ROAD, OCEAN VIEW**

A report from Property advised that the owners of 59 Creamery Road, Ocean View, had applied to stop a section of unformed legal road adjoining their property.

This report seeks a resolution of the Council to publicly notify the Council's intention to stop the road, under section 342 and Schedule 10 of the Local Government Act 1974.

The General Manager, 3 Waters, Property and Urban Development (David Ward) and Strategic Property Advisor (Paula Dickel) spoke to the report and responded to questions.

Cr Mickey Treadwell left the meeting at 3.35 pm and returned at 3.38 pm.

Moved (Mayor Sophie Barker/Cr Cherry Lucas):

That the Council:

- a) **Approves** public notification of the Council's intention to stop a section of legal road at 59 Creamery Road, Ocean view, subject to the Minister for Land Information's consent and the applicants agreeing to:
  - i) Pay the Council the non-refundable fee for processing the road stopping; and sign a conditional agreement for sale and purchase upon terms and conditions approved by the Council's solicitors.
  - ii) Pay the Council the actual costs incurred for the road stopping, regardless of whether or not the stopping reaches a conclusion, and the market value of the stopped road, assessed by an independent registered valuer appointed by the Council.
  - iii) Amalgamate the stopped portion of road with the title of the adjacent land at 59 Creamery Road, Ocean View, being the land contained within Record of Title 15937.
  - iv) Accept the application of the standards contained within the Dunedin City Council Code of Subdivision and Development to the stopped road.

- v) Register any easements over the stopped portion of road in favour of utilities or adjoining owners (if required).

**Motion carried (CNL/2026/080)** with Crs Russell Lund; Benedict Ong and Lee Vandervis recording their votes against

### **13 REVIEW OF KEEPING OF ANIMALS BYLAW**

A report from Corporate and Regulatory sought Council approval to commence the review of the Keeping of Animals Bylaw and recommended that the Council determined a bylaw was the most appropriate way to address issues relating to keeping animals in Dunedin.

The General Manager Corporate and Regulatory Services (Paul Henderson), Manager Compliance Solutions (Bonnie Wright) and Team Leader Animal Services (Cazna Savell) spoke to the report and responded to questions.

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Council:

- a) **Approves** commencement of the Keeping of Animals Bylaw review.
- b) **Determines** that a bylaw is the most appropriate way to address issues relating to keeping animals in Dunedin.

**Motion carried (CNL/2026/081)**

### **14 LEGAL HIGH RETAIL LOCATION POLICY REVIEW**

A report from Corporate and Regulatory and Civic recommended that the Council continued the Legal High Retail Location Policy for a further five years, with no substantive changes, following a staff review.

The Policy restricts legal high (psychoactive substance) retailers to the Dunedin central city area provided they are more than 100 metres away from sensitive sites such as education facilities, churches, libraries, hospitals, mental health facilities and justice premises. The sensitive sites were identified on a map which formed part of the Policy and that if able, the new Dunedin Hospital site would be included.

The General Manager Corporate and Regulatory Services (Paul Henderson); Manager Compliance Solutions (Bonnie Wright) spoke to the report and responded to questions.

Crs Christine Garey and Russell Lund left the meeting at 4.00 pm and returned at 4.02 pm. .

Moved (Mayor Sophie Barker/Cr Mandy Mayhem):

That the Council:

- a) **Notes** the review of the Legal High Retail Location Policy.

- b) **Approves** retention of the Legal High Retail Location Policy .

**Motion carried (CNL/2026/082)**

## 15 REVIEW OF THE TRAFFIC AND PARKING BYLAW

A report from Transport sought approval of the draft Dunedin Traffic and Parking Bylaw 2026 and adoption of a statement of proposal for consultation purposes. The report appended the current Bylaw which had been in effect since 2010.

The General Manager, City Services (Scott MacLean), and Policy Analyst Transport Regulation (Rose Wunrow) spoke to the report and responded to questions.

Cr Steve Walker left the meeting at 4.22 pm and returned at 4.25 pm, Cr Doug Hall left the meeting at 4.26 pm and returned at 4.28 pm and Cr Russell Lund left the meeting at 4.29 pm and returned at 4.30 pm.

Cr Mandy Mayhem left the meeting at 4.38 pm and returned at 4.39 pm.

Moved (Cr Cherry Lucas/Cr Steve Walker):

That the Council:

- a) **Determines** that the proposed Traffic and Parking Bylaw 2026 meets the additional requirements of section 155 of the Local Government Act 2002 in that:
  - i) the proposed Bylaw is the most appropriate form of bylaw, and
  - ii) the proposed Bylaw does not give rise to any implications under the New Zealand Bill of Rights Act 1990.
- b) **Approves** the proposed Traffic and Parking Bylaw 2026 for consultation purposes, subject to any minor amendments.
- c) **Adopts** the statement of proposal for consultation purposes, subject to any minor amendments.
- d) **Notes** that the Chair of the Hearings Committee has the delegation to appoint Council members to the Committee that will receive oral submissions on the Traffic and Parking Bylaw.

**Motion carried (CNL/2026/083)**

## 16 REVIEW OF ALCOHOL (CONTROL OF ALCOHOL IN PUBLIC PLACES) BYLAW

A report from Corporate and Regulatory and Civic sought approval to commence the review of the Alcohol (Control of Alcohol in Public Places) Bylaw 2004.

The Bylaw prohibited people from bringing alcohol into, possessing alcohol in, or consuming alcohol in a specified place within Dunedin's central city area (with some exemptions). The Bylaw also provided the Council with the power to specify additional periods and public places, for example temporary restrictions in relation to a planned public event. The Bylaw was enforced by the Police.

The General Manager Corporate and Regulatory Services (Paul Henderson) and Manager Compliance Solutions (Bonnie Wright), spoke to the report and responded to questions.

Moved (Mayor Sophie Barker/Cr Christine Garey):

That the Council:

- a) **Approves** commencement of the Alcohol (Control of Alcohol in Public Places) Bylaw 2004 review
- b) **Determines** that a bylaw is the most appropriate way to address some alcohol related issues in Dunedin.

**Motion carried (CNL/2026/084)** with Crs Benedict Ong and Brent Weatherall voting their votes against

The meeting concluded at 4.54 pm.

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MAYOR

## REPORTS

### ACTIONS FROM RESOLUTIONS OF COUNCIL MEETINGS

Department: Civic

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#### EXECUTIVE SUMMARY

- 1 The purpose of this report is to show progress on implementing resolutions made at Council meetings.
- 2 As this report is an administrative report only, there are no options or Summary of Considerations.

#### RECOMMENDATIONS

That the Council:

- a) **Notes** the Open and Completed Actions from resolutions of Council meetings as attached.

#### DISCUSSION

- 3 This report also provides an update on resolutions that have been actioned and completed since the last Council meetings. Note that items on the Forward Work Programme are not included in the attached schedules.

#### NEXT STEPS

- 5 Updates will be provided at future Council meetings.

#### Signatories

Author:	Lynne Adamson - Governance Support Officer
Authoriser:	Nicola Morand - Manahautū (General Manager Community and Strategy)

#### Attachments

	<b>Title</b>	<b>Page</b>
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Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS					
OPEN ACTION LIST					
APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
28/11/2023	<b>CNL/2023/277</b>	<b>Right of Way Easement over part Dunedin Town Belt for 139 Harbour Terrace, Dunedin</b>	<p><b>Acting</b> in its capacity as the administering body of the Dunedin Town Belt Recreation Reserve pursuant to the Reserves Act 1977:</p> <p><b>Approves</b> the partial surrender of an existing vehicular Right of Way easement as it relates to Lot 2 DP 390403 (InFstrument 8489286.2)</p> <p><b>Grants</b> a Right of Way easement over part of the Dunedin Town Belt Recreation Reserve to land held as Lot 1 DP 575078 located at 139 Harbour Terrace, Dunedin, subject to the conditions outlined in this report.</p> <p><b>Approves</b> increasing of the existing annual fee for the Right of Way from \$1,265.00 including GST to \$1,500.00 including GST for use of the Dunedin Town Belt Recreation Reserve for access to the property at 139 Harbour Terrace, Dunedin.</p> <p><b>Decides</b> that the criteria for exemption from public notification has been met.</p> <p><b>Acting</b> under delegation from the Minister of Conservation dated 12 June 2013, and pursuant to section 48 of the Reserves Act 1977;</p> <p><b>Approves</b> the partial surrender of an existing vehicular Right of Way easement as it relates to Lot 2 DP 390403 (Instrument 8489286.2) and</p> <p><b>Consents</b> to the grant of a Right of Way easement over part of the Dunedin Town Belt Recreation Reserve to land held as Lot 1 DP 575078 located at 139 Harbour Terrace, Dunedin, subject to the conditions outlined in this report.</p>	Parks and Recreation	April 2026 – Unchanged. DCC is currently registering the partial surrender of an existing easement and then registration of the new easement over DCC land.
30/01/2024	<b>CNL/2024/011</b>	<b>Approval to Grant Electricity Easement to Aurora Energy Limited – Part Local Purpose (Esplanade) Reserve at Burnside, Dunedin</b>	<p><b>Grants</b>, as administering body of the Local Purpose (Esplanade) Reserve, pursuant to Section 48 of the Reserves Act 1977, an easement in gross to Aurora Energy Limited for the installation of an underground fibre cable and associated cabling over part of the Local Purpose (Esplanade) Reserve at Burnside (Record of Title 201821).</p> <p><b>Decides</b> the criteria for exemption from public notification has been met.</p> <p><b>Acting</b> under its delegation from the Minister of Conservation dated 12 June 2013 and pursuant to Section 48 of the Reserves Act 1977, approves an easement in gross to Aurora Energy Limited for the installation of an underground fibre cable and associated cabling over part of the Local Purpose (Esplanade) Reserve at Burnside (Record of Title 201821).</p>	Parks and Recreation	April 2026 – Unchanged. A separate matter relating to mining rights in Council’s reserve land is being co-ordinated by DCC’s Legal Team. These mining rights are to be transferred to Council. Once the mining rights have been transferred then the Aurora Easement will be registered.

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS					
OPEN ACTION LIST					
APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
10/12/2024	CNL/2024/249	Unitary Authority	<b>Progresses</b> discussions about a possible unitary authority for Otago with an initial meeting in early 2025 of elected members of the Councils in Otago or their representatives.	Office of the Mayor	April 2026 – No change. Government changes in this sphere have been announced (Simplifying Local Government). A submission was approved at the 19 February 2026 Council meeting noting that a briefing paper on unitary councils would be prepared for a future Council meeting.
26/02/2025	CNL/2025/079	Memorandum of Understanding with Christchurch City Council – Potential for Shared Services	<b>Approves</b> the Memorandum of Understanding between Dunedin City Council and Christchurch City Council.  <b>Notes</b> that the Council’s consultation document under the Local Government (Water Services Preliminary Arrangements) Act 2024 will record that Dunedin City Council and Christchurch City Council are working together to investigate whether there are opportunities for certain shared water services. Delegates to the Council’s Chief Executive Officer the authority to finalise and sign the Memorandum of Understanding on behalf of Council.	3 Waters/Legal	April 2026 – No change. Staff continue to work constructively with colleagues in Christchurch in the areas of procurement, asset management and engineering/data standards.
26/03/2025	CNL/2025/089	Te Awa Ōtākou – Issues and Opportunities Report	<b>Requests</b> staff arrange a meeting between the DCC and ORC Councillors to discuss governance matters related to Te Awa Ōtākou Issues and Opportunities report.	Parks and Recreation	April 2026 – The schedule of the Catchment Group is being finalised and will allow for at least one DCC representative.
26/05/2025	CNL/2025/154	Dunedin Tunnels Trails Trust funding	<b>Make</b> Caversham Tunnel available for track development by prioritising utility removal, diversion and/or upgrades to allow the Dunedin Tunnels Trails Trust to begin independent fundraising for track completion and to be reported back to Council or the relevant committee.	Transport	April 2026 – No change. An agreement in principle with utility providers has been gained to remove (gas) or underground (electrical) services in Caversham Tunnel. Awaiting outcomes of 3W investigations to confirm timeframes and costs for Council.
26/08/2025	CNL/2025/257	Proposed Road Stopping – Adjoining 40 Halfway Bush Road	<b>Approves</b> public notification of the Council’s intention to stop a section of legal road at 40 Halfway Bush Road, subject to the Minister for Land Information’s consent and the applicants agreeing to:  <b>Pay</b> the Council the non-refundable fee for processing the road stopping.  <b>Pay</b> the Council the actual costs incurred for the road stopping, regardless of whether or not the stopping reaches a conclusion, and the market value of the stopped road, assessed by the Council’s valuer.  <b>Amalgamate</b> the stopped portion of road with the title of the adjacent land at 40 Halfway Bush Road, being the land contained within Record of Title OT11A/251.	Property	April 2026 – There is no change, the conditional agreement for sale and purchase is with the applicant.

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS OPEN ACTION LIST APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
			<p><b>Accept</b> the application of the standards contained within the Dunedin City Council Code of Subdivision and Development to the stopped road.</p> <p><b>Register</b> any easements over the stopped portion of road in favour of utilities or adjoining owners (if required).</p>		
11/12/2025	CNL/2025/341	NZTA State Highway Response to the new Dunedin Hospital	<p><b>Requests</b> that NZTA works with Health New Zealand to establish sufficient additional car parking in very close proximity to the new hospital.</p>	Transport	April 2026 – No change. NZTA continues to work alongside Health NZ for the best outcome for the NDH’s relationship to the SH network but NZTA is not responsible for providing parking in local authority areas.
11/12/2025	CNL/2025/350	Sports Ground Management Plan Review	<p><b>Approves</b> the Statement of Proposal and Sports Ground Management Plan Feedback Form</p> <p><b>Authorises</b> the commencement of the public consultation process required by s. 41(5) of the Reserves Act 1977 as outlined in Option One below.</p>	Parks and Recreation	April 2026 – The Draft Sportsground Management Plan is currently being written.
11/12/2025	CNL/2025/351	Town Belt Active Travel Trial	<p><b>Approves</b> the approach outlined in this report to implement the Town Belt Active Travel Trial.</p> <p><b>Notes</b> that the trial is the first step in delivering on the Town Belt Reserve Management Plan’s objectives and implementation plan.</p> <p><b>Notes</b> that at the conclusion of the trial staff will report back to Council on the results of the trial, including community feedback.</p>	Parks and Recreation	April 2026 – No change. The Town Belt Active Travel Trial started on Monday 26 January 2026 and will run for 3 months. The road has been temporarily closed to traffic. Submissions are being collated and will form the basis of a report to Council on the results of the trial once it is completed. The trial finishes on 19 April 2026.
12/02/2026	CNL/2026/022 CNL/2026/023	Grants Review	<p><b>Requests</b> that staff:</p> <ul style="list-style-type: none"> <li>a) Draft an updated Grants Management Policy reflecting the agreed policy settings;</li> <li>b) Develop implementation and transition arrangements, including any required updates to delegations and committee terms of reference;</li> <li>c) Undertake pre-procurement analysis for a grants management system aligned with the agreed policy framework; and</li> <li>d) Report back to Council through a future Annual Plan process with detailed options to strengthen alignment between grant-making.</li> </ul>	Civic	<p>April 2026 – Staff continue to implement the actions as outlined in Resolutions CNL/2026/022 and CNL/2026/023 as follows:</p> <ul style="list-style-type: none"> <li>a) Draft an updated Grants Management Policy reflecting the agreed policy changes. Policy is leading this work and is currently working with Council teams who administer grants. Once a draft is finalised, policy will present this to Council for feedback and approval.</li> <li>b) The required updates to delegations and committee terms of reference for the Grants Subcommittee have been completed.</li> <li>c) Undertaking pre-procurement analysis for a grants management system aligned with the agreed policy framework has not yet started but will follow the update to the Grants Management Policy.</li> <li>d) Staff are preparing to report to Council through a future Annual plan process with detailed options to strengthen alignment between grant making.</li> </ul>

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS					
OPEN ACTION LIST					
APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
			<p>e) Confirms its preferred Governance Structure for Grant Decisions is a Grants Subcommittee + Staff Delegations + Council Oversight where:</p> <p>i) Staff allocate small grants under delegation.</p> <p>ii) A Grants Subcommittee (which include community representatives) allocates mid-tier grants, multi-year and large grants.</p>		<p>e)</p> <p>i) Allocating small grants (\$5k and under) under staff delegation is underway currently.</p> <p>ii) The creation of a Grants Subcommittee (which includes community representatives) to allocate mid-tier grants (over \$5k), multi-year and large grants is underway with community representatives to be selected within the next month.</p>
12/02/2026	CNL/2026/025	<b>Appointment of Advisory Panel to Consider District Licensing Committee Commissioner Applications</b>	<p>Appoints an Advisory Panel of Councillor Lucas (as Chair), Crs Garey and Walker to consider the applications received to be commissioners of the Dunedin District Licensing Committee. Approves the draft Terms of Reference for the Advisory Panel</p> <p>Requires the Advisory Panel to:</p> <p>i) make recommendations for appointment to the Chief Executive; and</p> <p>ii) appoint a chairperson for the District Licensing Committee; and</p> <p>iii) (if desired) appoint a Deputy Chairperson</p>	Civic	April 2026 – The interviews have been completed and the panel have made their recommendations to the Chief Executive who is considering them prior to appointment.
26/05/2025	CNL/2025/129	<b>Performing Arts Revenue Update 9 Year Plan 2025-2034</b>	<b>Includes</b> \$3.35 million in year 3 of the 2025-34 9 year plan (financial year 2027/2028) as grand funding to the Dunedin Repertory Society Incorporated.	Ara Toi	April 2026 – No change. The funding is subject to an agreement being negotiated between Council and the Dunedin Repertory Society Incorporated upon terms and conditions acceptable to both parties; and Council passing a resolution approving the terms and conditions of the agreement (such approval to be granted or withheld in Council's sole and absolute discretion). An agreement has yet to be negotiated.
12/02/2026	CNL/2026/027	<b>Resolution to stop part of Neill Street, Abbotsford</b>	<p><b>Resolves</b> that under Section 342 of the Local Government Act 1974 the part of unformed road described as Sections 1 &amp; 2 SO 619506 are stopped.</p> <p><b>Notes</b> that no objections were submitted during the public notice period.</p> <p><b>Authorises</b> a public notice declaring that the road is stopped.</p>	Property	April 2026 – A new title for stopped road has been ordered, the settlement date will be confirmed once this has been issued.
24/02/2026	CNL/2026/038	<b>Administration of CNZ Creative Communities Schemes Grants</b>	<b>Approves</b> the establishment of the Creative Communities Scheme Assessment Panel to undertake CCS assessment and decision-making, the composition of which was as follows:	Ara Toi	April 2026 – 32 nominations were received for the 5 places available; 4 withdrew upon learning that their time would not be remunerated.

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS OPEN ACTION LIST APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
			<ul style="list-style-type: none"> <li>i) 4 general community representatives, across a range of creative disciplines</li> <li>ii) 1 youth representative (aged 24 or under)</li> <li>iii) 1 mana whenua representative (or their nominee, if mana whenua did not have capacity to sit on the committee at the current time)</li> <li>iv) 1 or 2 elected officials with appropriate arts knowledge and experience (Mayoral nomination, drawn from Council and/or Community Boards).</li> </ul>		<p>The selected panellists (four community and one youth) draw upon experiences in visual and object art, dance, Pasifika arts, music and performing arts, lived experience with disability, arts education.</p> <p>Ngā Rūnaka o Ōtākou and Kāti Huirapa ki te Puketeraki have nominated a member to represent mana whenua. The elected panellist/s have not yet been nominated.</p> <p>The first training session was held on Thursday 9 April. Ongoing support is being provided for the group.</p>
04/03/2026	<b>CNL/2026/064</b>	<b>Proposed Event Road Closures</b>	<p><b>Resolves</b> to close the roads detailed in the agenda (pursuant to Section 319, Section 342 and Schedule 10 clause 11€ of the Local Government Act 1974 (LGA 1974) for the following events:</p> <p>Anzac Day Service and Parades – Mosgiel and Outram – Saturday 25 April 2026.</p>	<b>Transport</b>	<b>April 2026 – the roads will be closed for the Anzac Day Service and Parades</b>
	<b>CNL/2026/074</b>		<p><b>Approves</b> annual remuneration of \$100,577 for Councillors.</p> <p><b>Approves</b> annual remuneration of \$84,496 for Councillors with no additional responsibilities.</p>	<b>Civic</b>	<b>April 2026 – the information has been sent to the Remuneration Authority and will be actioned as soon as the changes have been gazetted.</b>
25/03/2026	<b>CNL/2026/080</b>	<b>Proposed Road Stopping – Adjoining 59 Creamery Road, Ocean View</b>	<p><b>Approves</b> public notification of the Council’s intention to stop a section of legal road at 59 Creamery Road, Ocean view, subject to the Minister for Land Information’s consent and the applicants agreeing to:</p> <p>Pay the Council the non-refundable fee for processing the road stopping; and sign a conditional agreement for sale and purchase upon terms and conditions approved by the Council’s solicitors.</p> <p>Pay the Council the actual costs incurred for the road stopping, regardless of whether or not the stopping reaches a conclusion, and the market value of the stopped road, assessed by an independent registered valuer appointed by the Council.</p>	<b>Property</b>	<b>April 2026 – Owners and their surveyor to confirm scheduling and co-ordination of the survey work, conditional sale and purchase agreement and valuation assessment.</b>

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS					
OPEN ACTION LIST					
APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Status
			<p>Amalgamate the stopped portion of road with the title of the adjacent land at 59 Creamery Road, Ocean View, being the land contained within Record of Title 15937.</p> <p>Accept the application of the standards contained within the Dunedin City Council Code of Subdivision and Development to the stopped road.</p> <p>Register any easements over the stopped portion of road in favour of utilities or adjoining owners (if required).</p>		

Key	
Changes to timeframes	
Progress to date update	Bold

PUBLIC COUNCIL RESOLUTIONS COMPLETED ACTION LIST APRIL 2026					
Meeting Date	Resolution	Report	Resolution or Action to be Taken	Group	Completion
12/02/2026	<b>CNL/2026/030</b>	<b>Proposed Event Road Closures</b>	<b>Resolves</b> to close the roads detailed in the agenda (pursuant to Section 319, Section 342, and Schedule 10 clause 11(e) of the Local Government Act 1974 (LGA 1974)) for the following events:  Baseline – Saturday 28 and Sunday 29 March 2026	Transport	<b>The roads were closed for the event</b>
04/03/2026	<b>CNL/2026/064</b>	<b>Proposed Event Road Closures</b>	<b>Resolves</b> to close the roads detailed in the agenda (pursuant to Section 319, Section 342 and Schedule 10 clause 11(e) of the Local Government Act 1974 (LGA 1974)) for the following events:  2026 Rally Otago – Wednesday 8 April, Thursday 10 April, Friday 11 April and Saturday 12 April.  Hyde Street Party – Saturday 18 April 2026	Transport	<b>The roads were closed for the events</b>
25/03/2026	<b>CNL/2026/073</b>	<b>Governance and Remuneration Update</b>	<b>Approves</b> the removal of Cr Ong from the roles of Council Representative on the Otago Settlers Association and the Toitū Otago Settlers Museum Board.  <b>Appoints</b> Cr Chambers as the Council Representative on the Otago Settlers Association and the Toitū Otago Settlers Museum Board.	Civic	<b>The Otago Settlers Association and Toitū Otago Settlers Museum Board have been advised of the changes.</b>





- d) **Notes** that funding for 3 years for whichever model is chosen will be incorporated into the draft annual plan budget and included in the draft Long-Term Plan.
- e) **Notes** the next steps are dependent on the selected delivery model.

## BACKGROUND

- 6 On 23 September 2025 Council agreed in principle to establishing a Housing Outreach Service and requested a report outlining the costs and implications.
- 7 On 27 November 2025, Council confirmed its commitment to establishing and funding the service and directed staff to undertake an EOI process and report back on the outcome, alongside the in-house option, for a decision on a preferred provider/model and confirmation of the funding.
- 8 The November report identified that while existing services provide valuable support, they have not, on their own, achieved sustained reductions in homelessness. It proposed a proactive outreach model to strengthen coordination, improve service access, and support pathways into stable housing.

*Moved (Cr Andrew Simms/Cr Russell Lund):  
That the Council:*

- a) **Confirms** its commitment to establishing and funding a Housing Outreach service in the city;

### **Motion carried (CNL/2025/324)**

*Moved (Cr Andrew Simms/Cr Russell Lund):  
That the Council:*

- b) **Authorises** the CEO to develop the terms for the service as an Expression of Interest (EOI) for an outreach service noting that the terms would include:
  - i) an initial 3 year service beginning in April 2026,
  - ii) performance measures and community outcomes
  - iii) annual reporting requirements.
- c) **Directs** staff to undertake an EOI process to seek proposals from community providers to deliver the outreach service;
- d) **Reports** back to Council on the EOI outcome, alongside the in-house option, for a decision on the preferred provider/model and confirmation of the final funding for the first three years of the service.

### **Division**

*The Council voted by division*

*For: Crs Cherry Lucas, Russell Lund, Benedict Ong, Jules Radich, Andrew Simms, Lee Vandervis, Brent Weatherall and Mayor Sophie Barker (8)*

*Against: Crs John Chambers, Christine Garey, Dough Hall, Marie Laufiso, Mandy Mayhem, Mickey Treadwell and Steve Walker (7)*

*Abstained: Nil*

*The division was declared CARRIED by 8 votes to 7*

***Motion carried (CNL/2025/325)***

## EXPRESSION OF INTEREST PROCESS

- 9 An Expression of Interest (EOI) process was initiated through GETS on 12 December 2025.
- 10 The opportunity was also shared with organisations working in the homelessness and housing support sector.
- 11 Six proposals were received.
- 12 A cross-Council evaluation panel assessed the proposals against the criteria identified in the EOI documentation with procurement staff ensuring compliance with Council policies.
- 13 Of the six proposals received, three were shortlisted for the Tenders Board to consider.
- 14 The Tenders Board selected a preferred supplier, based on the service model, capacity to meet demand and demonstrated sector experience. This is discussed further in the confidential part of this report.

## COMPARISON OF DELIVERY MODELS

- 15 In considering the options, Council may wish to have regard to factors such as integration with Council Services, operational capability, level of oversight, implementation requirements, sector relationships and cost.
- 16 Each proposed model presents different advantages and risks but, in all cases, Council retains ultimate accountability for outcomes.
- 17 The decision is not whether the service can be delivered, but how it is best delivered. The key trade-offs are:
  - a) **External delivery:** faster implementation, and lower short-term risk.
  - b) **In-house delivery:** greater control, integration, and long-term system capability.
  - c) **Hybrid delivery:** a staged approach balancing early control with future flexibility.

Criteria	In-House (Council)	External	Hybrid (Phased)
Implementation speed	Slower (setup required)	Fast (ready to deliver)	Moderate
Short-term risk	Higher	Lower	Moderate
Council control	High	Moderate (via contract)	High initially

Criteria	In-House (Council)	External	Hybrid (Phased)
Integration with Council	Strong	Moderate	Strong initially
Sector relationships	Strong	Established	Mixed
Data access and insight	High (direct access)	Moderate (via reporting)	High initially
Flexibility/adaptability	High	Moderate	High
Long-term system impact	High	High	High (if well managed)
Cost (approx.) per annum	\$270k	As detailed in the public excluded part of this report.	Variable
Complexity	Moderate	Low	High

- 18 All models are viable. The decision depends on Council’s preferred balance between speed and certainty (external), control and integration (in-house), or a staged approach (hybrid).

### SERVICE DELIVERY FRAMEWORK

- 19 Regardless of the delivery model selected, the service focuses on:
- a) Proactive outreach engagement
  - b) Service navigation and referrals.
  - c) Crisis response and prevention
  - d) Sector coordination.
  - e) Data collection and reporting

### PERFORMANCE MEASURES

- 20 Performance would be monitored through a range of indicators, including:
- a) Number of people engaged.
  - b) Referrals to services
  - c) Transition into housing.
  - d) Reduction in repeat rough sleeping
  - e) Contribution to Functional Zero initiatives
  - f) Timely resolution to complaints or concerns raised by public.

### OPTIONS

- 21 Three options for delivering the Homelessness Outreach Service are presented to Council for consideration. These are:

- a) Delivery by Dunedin City Council (in-house);
  - b) Delivery by an external provider; or
  - c) A hybrid/phased delivery model, transitioning from Council to an external provider.
- 22 The cost of all options is similar.
- 23 While it is open to Council to choose a provider other than the Preferred Supplier or to initiate another EOI process or proceed to a Request for Proposals, these options have not been discussed further because the Preferred Supplier has been selected based on an extensive assessment process and can start within required timeframes. Any new procurement process would inevitably create delay and the outcome is uncertain.

### **Option One – In-House Delivery**

- 24 An in-house model enables Council to fully integrate the outreach service within its existing functions, providing strong alignment with strategy, direct oversight, and long-term system influence. However, this approach requires time and investment to establish, with higher short-term risk and a slower path to full operational effectiveness.

#### *Advantages*

- *Strong integration with Council systems*
- *Greater operational control and accountability*
- *Development of internal capability and system insight*
- *Strong alignment with long-term strategic influence*

#### *Disadvantages*

- *Higher setup risk*
- *Time to full operational effectiveness*
- *Workforce and capability development requirements*
- *Resource and organisational focus*

### **Option Two – External Delivery**

- 25 An external delivery model allows the service to be implemented quickly using an established provider with existing capability, systems, and sector relationships. This reduces short-term risk and provides early delivery certainty, although it involves less direct Council control and relies on contractual mechanisms for oversight and integration.

*Advantages*

- *Established delivery capability*
- *Demonstrated sector experience and credibility*
- *Integrated service approach*
- *Readiness to implement*

*Disadvantages*

- *Reliance on contractual oversight*
- *Indirect Integration with Council functions*
- *Limited Data access and system insight*

**Option 3 – Hybrid Phased Delivery Model**

26 A hybrid model enables Council to establish and shape the service in its early stages, building integration, insight, and local understanding before transitioning delivery to an external provider. This approach balances control and flexibility over time but introduces additional complexity, including transition risks, duplicated effort, and a longer overall decision pathway. This approach is intended to combine the benefits of early Council-led system integration with external delivery.

*Advantages*

- *Strong initial alignment with Council systems and strategy*
- *Opportunity to design and refine the service model*
- *Reduced long-term delivery risk through staged transition*
- *Retention of strategic influence with future flexibility*

*Disadvantages*

- *Higher initial set-up risk*
- *Complexity of transition between delivery models*
- *Requirement for a future procurement process*
- *Potential duplication of effort and cost*

**KEY CONSIDERATIONS AND FUNDING IMPLICATIONS**

- 27 The service is not currently budgeted.
- 28 Establishing the service during the current financial year would therefore require unbudgeted expenditure for approximately one month. This can be covered from the existing operating surplus.
- 29 Ongoing funding will need to be added to the draft Annual Plan budget and included in the draft Long-Term Plan.
- 30 For all options, annual reporting will be required and a review will be undertaken by Council.

**NEXT STEPS**

- 31 Subject to the Council’s decision, staff will either:
- a) If Council selects in house delivery:
    - i) Recruit staff.
    - ii) Establish operational procedures.
    - iii) Commence service delivery by June 2026.
  - b) If Council selects external delivery:
    - i) Proceed to contract negotiations with the Preferred Supplier.
    - ii) Complete due diligence.
    - iii) Finalise the service contract.
    - iv) Commence delivery by June 2026.
  - c) If Council selects hybrid phased delivery:
    - i) Agree phased transitional approach.
    - ii) Recruit staff on a fixed term basis.
    - iii) Establish operational procedures and deliver outreach service.
    - iv) Finalise a service contract.
    - v) Transition service to an external provider (to be selected at a later date).

**Signatories**

Author:	Nicola Morand - Manahautū (General Manager Community and Strategy)
Authoriser:	Sandy Graham - Chief Executive

**Attachments**

There are no attachments for this report.

**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision promotes the social well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Other strategic projects/policies/plans	✓	<input type="checkbox"/>	<input type="checkbox"/>

This work supports Council’s goals of creating inclusive, liveable communities, reducing housing inequities, and achieving functional zero homelessness. The service strengthens alignment across policy, planning, and frontline delivery, advancing Council’s vision across all wellbeing strategies and plans, in particular the Housing Action Plan.

***Māori Impact Statement***

Manawhenua have expressed that affordable housing and homelessness is a key priority for whānau. The Housing Policy Team contributes to the aspirations of Te Taki Haruru – Māori Strategic Framework by supporting equitable housing outcomes for Māori and strengthening partnerships with mana whenua. This includes supporting kaupapa Māori approaches to housing, collaborating with mana whenua in strategic planning, and ensuring that Council’s housing responses contribute to Māori communities in Ōtepoti.

***Sustainability***

Sustainability in this context relates to social sustainability.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

Any funding changes will be included in the Annual Plan and ten year plan.

***Financial considerations***

The provision of the service is currently unbudgeted. The report deals with possible funding options.

***Significance***

The approval of this report is identified as low significance.

***Engagement – external***

External engagement has consistently identified the lack of outreach services in Dunedin as problematic.

***Engagement - internal***

Engagement with People and Capability and Finance has been undertaken.

**SUMMARY OF CONSIDERATIONS**

***Risks: Legal / Health and Safety etc.***

These positions will have health and safety oversight, utilising lone worker safety protocols and provisions.

***Conflict of Interest***

There are no known conflicts of interest.

***Community Boards***

Homelessness is an issue in all areas of the city including Community Board areas.

## **231 STUART STREET - PROPOSED SALE PROCESS**

Department: Property and Legal Services

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### **EXECUTIVE SUMMARY**

- 1 The Council owns the property at 231 Stuart Street (the Property). This was formerly operated by the Fortune Theatre Trust but has been vacant since 2018.
- 2 On 30 July 2025, Council passed a resolution which asked staff to prepare the Property for sale and noted that staff would provide a further report to Council prior to a potential sale.
- 3 In preparing the Property for a potential sale, staff have considered Council's obligations under the Public Works Act 1981, the need to protect the Property's heritage values, appropriate marketing approaches, and feedback from interested parties.
- 4 This report provides an update on these matters and recommends that Council market the Property for sale on the open market through a request for proposals process.
- 5 A request for proposals process provides Council with the flexibility that will be required to assess proposals that are potentially for a wide range of potential uses.

### **RECOMMENDATIONS**

That the Council:

- a) **Market** the Property for sale on the open market through a request for proposals process.

### **BACKGROUND**

- 6 On 30 July 2025, Council resolved as follows:

"Moved (Mayor Jules Radich/Cr Brent Weatherall):

That the Council:

- a) **Directs** staff to prepare the property at 231 Stuart Street legally described as 468 square metres more or less being part Section 16, Block XIV, Town of Dunedin all Record of Title OT287/25, for sale.
- b) **Notes** that staff would provide a further report to Council and records that it would not be making a final decision regarding a potential sale of 231 Stuart Street until it had received the further report from staff.

### **Division**

The Council voted by division

**For:** Crs Bill Acklin, David Benson-Pope, Christine Garey, Carmen Houlahan, Marie Laufiso, Cherry Lucas, Mandy Mayhem, Jim O’Malley, Steve Walker, Brent Weatherall, Andrew Whiley and Mayor Jules Radich (12).

**Against:** Crs Sophie Barker, Kevin Gilbert and Lee Vandervis (3).

The division was declared CARRIED by 12 votes to 3

### **Motion carried (CNL/2025/206)”**

- 7 In preparing the Property for a potential sale, staff have considered Council’s obligations under the Public Works Act 1981, the need to protect the Property’s heritage values, appropriate marketing approaches, and feedback from interested parties.
- 8 This report provides an update on these matters and recommends that Council market the Property for sale on the open market through a request for proposals (RFP) process.

### **Property History**

- 9 The Property was constructed for the former Trinity Methodist Church and was opened in 1870.
- 10 It was owned and operated by the Fortune Theatre Trust for professional theatre between 1978 and 2000. In 2000, the Fortune Theatre Trust approached the Council to assist with the Property because it was finding ownership financially unsustainable. As a result of discussions, it was purchased by the Council in 2000 for \$220,000 plus GST (if any) with the Fortune Theatre Trust leasing the Property from the Council.
- 11 In May 2018, the Trust closed the Theatre. The Property has remained vacant since then.
- 12 The Property currently generates no revenue. It has annual operating costs of approximately \$122,000 which includes rates, electricity, insurance, depreciation, and maintenance (dehumidifiers and ventilation to prevent mould growth).

### **Land Status**

- 13 The Property is 468 square metres in area and is held in fee simple in Record of Title OT287/25 (Title). There are no encumbrances registered against the Title.

### **Condition of the Building**

- 14 The Property has been in a “holding” state since it was vacated. Urgent repairs have been made where necessary and emergency systems are functioning.
- 15 The building has a current Building Warrant of Fitness. Emergency warning systems are compliant as is the emergency lighting – *it is compliant based on the building being vacant.*
- 16 The former church has been designed in a Gothic revival style. The building is constructed in solid masonry and timber, the primary roof is slate, and the secondary roof is clad in copper. Later alterations are constructed in steel and concrete.



- b) Specify a timeframe for redevelopment, so as to prevent “land banking” and demolition by neglect.

### **Interested Parties**

- 27 In the years since the Property has been vacant, there has been informal interest in the Property by both community groups and commercial parties. DCC’s Property Services Team has taken a number of people through the Property, and there appears to be genuine interest in the reuse of the Property.

### **Method of Sale**

- 28 The Property is located within the central city in a prominent, high-profile position and is recognised as an iconic heritage building.
- 29 The Property’s location in the CBD zone provides for a wide range of activities including commercial, recreational, residential, community support, large scale retail, licensed premises, and commercial residential activity.
- 30 Staff have considered a range of potential sale and marketing methods. In doing so, staff have considered various factors including the following:
  - a) This is a unique Property and there are a wide range of considerations that Council may want to consider.
  - b) There are a wide range of potential uses for the Property. For example, the Property could be used for predominantly community purposes or predominantly commercial purposes, or a mix of both. Council may or may not be prepared to sell the Property at a lower price if certain non-financial outcomes are achieved.
  - c) The process for any sale and marketing process needs to provide Council with flexibility, because it will not necessarily be possible to compare “apples with apples” given the wide range of potential financial and non-financial considerations.
  - d) A sale process will need to reach both community based and commercial interest groups.
  - e) Prospective purchasers will require sufficient time to undertake initial due diligence, particularly given the implications and responsibilities associated with owning a classified heritage property.
  - f) The sale process needs to find a balance so that any requirements are not so onerous so as to unduly preclude interested parties.
  - g) Any method of sale should comply with Council’s Asset Disposal and Write-Off Policy. That Policy records that all disposals of assets (which includes real property) will be through an open, fair and transparent process.

### **Request for Proposals**

- 31 Given the above factors, staff recommend that Council market the Property for sale on the open market through an RFP process. This approach would allow Council to gauge the level and nature of interest, and to understand each respondent’s proposed future use of the Property. It would also ensure that community, not-for-profit, and commercial initiatives are given an equal opportunity to present their proposals.

- 32 In addition, an RFP process would:
- a) Comply with Council’s Asset Disposal and Write-Off Policy; and
  - b) Provide Council with the flexibility that will be required when dealing with a situation where it may not be possible to compare “apples with apples”. For example, Council may need to consider a response for a commercial development against a response for a development for community purposes.
- 33 The RFP would be coordinated through the Property Services Team. An information pack would be made available on GETS, the DCC website, and the opportunity would be promoted through DCC social media channels, newspaper advertising, and media coverage.
- 34 The information pack will include the Title, plans of the building, the Flanders Marlow condition report of January 2024 and an RFP application form.
- 35 In addition, a real estate agent would be engaged to market the Property. This engagement would probably be based on a fixed fee.
- 36 Given that the Property is unique and there are a wide range of potential uses, staff do not propose using fixed evaluation criteria. However, to support Council’s assessment of proposals, respondents would be asked to comment on, or present information, that:
- a) Demonstrates an understanding of the building’s condition and the level of construction required to bring the Property up to an acceptable standard for occupation.
  - b) Describes how the intended ‘end use’ of the Property would benefit the city.
  - c) Describes an indicative timeline for undertaking the works to make the building fit for purpose, along with a confidential explanation of how the works would be funded.
  - d) Describes how the building’s significant heritage and historic fabric will be conserved.
  - e) Acknowledges that Council will require some form of legal mechanism to ensure that the Property is not land banked or subject to demolition by neglect.
  - f) Includes the following details:
    - i) Purchase price, proposed settlement date, and any special conditions; and
    - ii) Any extended due diligence requirements.
- 37 Parties will be given the opportunity to view the Property. This would be undertaken in a managed way.
- 38 Proposals may offer a range of benefits to Dunedin. Some may be primarily financial, while others may deliver broader social or community outcomes. To allow Council to assess each proposal on its individual merits, it is proposed that the Property be marketed on a “price by negotiation” basis.

## **RFP PROCESS**

- 39 In summary:

- a) The RFP would be an open market process, allowing responses from all interested parties;
  - b) The RFP would be open for around six weeks;
  - c) Respondents would be given a full information pack; and
  - d) Respondents would be asked to provide certain information.
- 40 The RFP would link to Council’s standard RFP process Terms and Conditions. This would mean that Council would be free to select or reject any proposal(s). The RFP process would not commit Council to a sale or any other course of action.
- 41 Staff would review all RFP responses and report back to full Council with an overview of the proposals received, including attaching all proposals received, and potentially a staff recommendation.
- 42 It would then be for the full Council to decide which proposal, if any, it wants to progress towards a binding agreement for sale and purchase. This part of the process will likely be in a non-public Council meeting.

**OPTIONS**

- 43 While there are a range of potential methodologies for a sale, staff consider that an RFP is the only suitable process in this situation given the considerations outlined in this report.

**NEXT STEPS**

- 44 If Council markets the Property for sale through an RFP process, then the RFP will be released shortly and the RFP will be open for around six weeks.
- 45 Staff will then report back to Council with an overview of the proposals received and potentially include a staff recommendation. Given that responses are likely to contain confidential information, at least part of the report back to Council will be in public excluded.
- 46 It will be for full Council to decide which proposal (if any) it wants to progress towards a binding agreement for sale and purchase.

**Signatories**

Author:	Anna Nilsen - Group Manager, Property Services Karilyn Canton - Chief In-House Legal Counsel
Authoriser:	David Ward - General Manager, 3 Waters, Property and Urban Development Sandy Graham - Chief Executive

**Attachments**

	<b>Title</b>	<b>Page</b>
<a href="#">↓A</a>	Condition Report by Flanders Marlow	58
<a href="#">↓B</a>	Land Disposal Investigation Report by The Property Group	130

**SUMMARY OF CONSIDERATIONS**
***Fit with purpose of Local Government***

An RFP for 231 Stuart Street has the potential to promote the social, economic, environmental, and cultural well-being of communities in the present and for the future, depending on the nature of the responses and Council's subsequent decision regarding the responses.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Environment Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Arts and Culture Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
3 Waters Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Other strategic projects/policies/plans	✓	<input type="checkbox"/>	<input type="checkbox"/>

The fit with Council's strategic framework will depend on the nature of the responses to the RFP and cannot yet be assessed.

***Māori Impact Statement***

There has been no engagement with mana whenua over this proposal.

***Sustainability***

The possible reuse of a heritage building supports Council's sustainability goals.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

The future of the Property was consulted on as part of the 9 Year Plan.

***Financial considerations***

Any financial considerations will be considered in a future report to Council.

***Significance***

This decision is considered low in terms of the Council's Significance and Engagement Policy.

***Engagement – external***

Flanders Marlow were engaged to assess the condition and cost of repairs. Valuation advice has been received from Colliers International. The Property Group provided advice on whether there are any obligations under section 40 of the Public Works Act 1981. Numerous interested parties have been shown through the Property.

***Engagement - internal***

Internal engagement has been undertaken with Property Services, City Advisory, Legal, Marketing and Website teams.

***Risks: Legal / Health and Safety etc.***

There are no known risks associated with this report. Risks associated with any potential sale would be addressed in a later report.

**SUMMARY OF CONSIDERATIONS**

***Conflict of Interest***

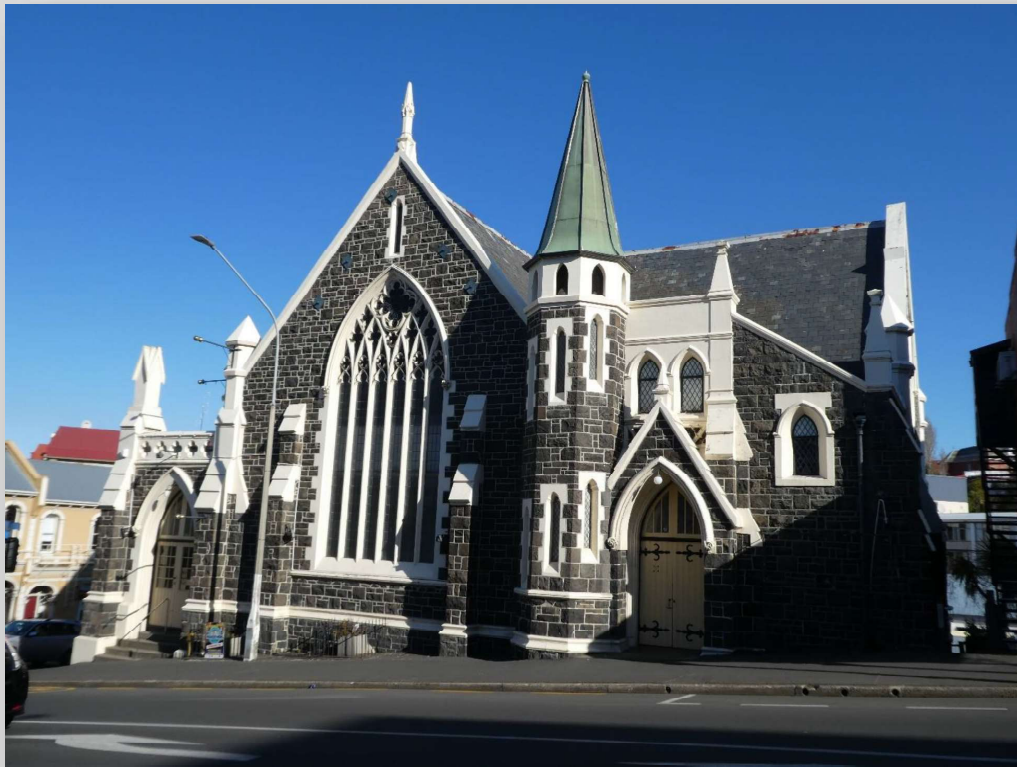
There are no known conflicts of interest.

***Community Boards***

There are no known implications for Community Boards.

**FLANDERS | MARLOW**  
PROJECT & PROPERTY

## CONDITION REPORT



### FORTUNE THEATRE, 231 STUART STREET, DUNEDIN

69 MACANDREW ROAD, P.O. BOX 1283 DUNEDIN 9054 NEW ZEALAND | TEL: (03) 455-1499 | [admin@flandersmarlow.co.nz](mailto:admin@flandersmarlow.co.nz)

*In 1877, William Morris pleaded for those entrusted with old buildings 'to stave off decay by daily care'.*

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**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

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**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

**1.0 INTRODUCTION**

**Report commissioned by:** Dunedin City Council.

**Report and site inspection undertaken by:** Grant Parker MSc Arch MNZIBS  
Registered Building Surveyor

**Date of Inspection:** Friday 8<sup>th</sup> September 2023 @ 1:30pm

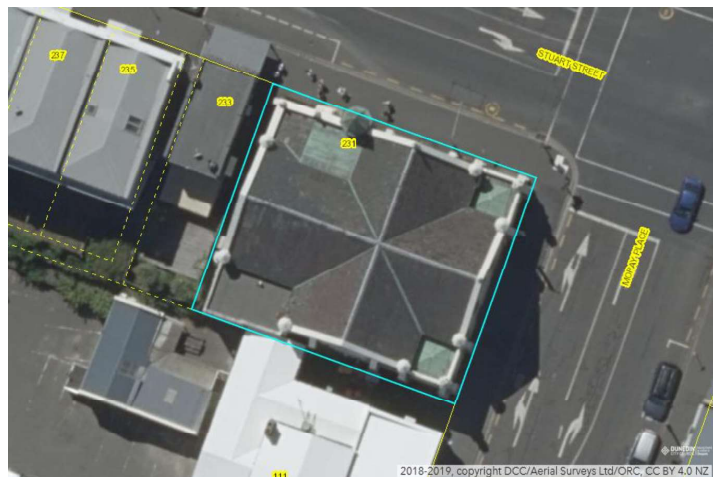
**Date of Issue:** Monday 22<sup>nd</sup> January 2024

**Other persons present:** None

**Weather:** The weather conditions were clear with light cloud; the outside temperature was approximately 13°C

**Orientation:** For the purposes of orientation throughout the body of this report, Stuart Street shall be deemed to run east / west, with the main entrance on the northern elevation.

Where items are described as to the left or right-hand side, the descriptions are made as if facing the element being described unless otherwise stated.



**General Description:** The building is located on the corner of Moray Place and Upper Stuart Street in the city of Dunedin.

The subject building was the former Trinity Methodist Church designed as an example of Gothic Revival Architecture by R. A. Lawson and was opened in 1870.

**General Construction:** The structure of the building is believed to comprise corbelled brick / stone and concrete slabs and foundations, local solid stone external wall construction, and steel, and timber joinery under a pitched slate and flat copper roofs.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

- 1.1 **Roofs:** The roofs comprise natural slate tiles understood to be Welsh, with associated metal ridge, and copper valley gutters and flashings with metal and replacement uPVC spouting's and stormwater disposal to the under-ground drainage system.
- 1.2 **Exterior walls:** The main elevations typically comprise un-reinforced solid local Dunedin blue stone and painted Oamaru stone walls.
- 1.3 **Floors:** The floors comprise suspended timber which could be supported on brick / stone, concrete, or timber pile foundations however, this was not confirmed. The perimeter walls comprise a mixture of larger blue stone blocks, solid brick, assumed to be projected at each course and no fines concrete foundations. It is understood that steel beams were proposed and introduced as part of the strengthening works in 1966.
- 1.4 **Joinery:** Single glazed leaded steel windows with solid timber and glazed panel doors.
- 1.5 **Interior:** Typically, the interior areas are accessed from the two main entrances and comprise the main theatre (formally the church) the entrance foyer / booking office and lounge / bar area and the lower entrance and small or studio theatre, along with toilets, kitchen and actors and staff facilities.

Ceilings are typically exposed diagonal timber sarking above the king post trusses and fibrous plaster. The internal walls are painted solid plaster over stone and brickwork and fibrous plaster over lathes with decorative timber panelling. The floors typically comprise original timber floor boards with the majority of coverings having been removed. Internal door joinery is a mixture of solid, glazed, and hollow core flush fitting doors.

**1.6 Services:**

**Power & Lighting:-** Mains electricity is supplied from the national electricity grid via underground cables for power and lighting. Power is provided which supplies the Main Switchboard (MSB) housed in the basement foyer. Power is typically distributed above ceilings, surface mounted conduit and within partitions. Lighting is typically pendant and surface mounted fittings with some hi bay lighting to the main theatre.

**Mechanical:-** Heating and cooling is provided by reverse-cycle ducted air to air conditioning units with ducted and high level, 'heat pump' units. These were not tested for operation at the time of inspection. Mechanical extract ventilation systems are installed to the WC's and ducted to the exterior.

**Hydraulic Services:-** Water supply is mains-fed with the meter and toby device being located to the south eastern corner on Moray Place. Hot water is via low and mains pressure hot water cylinders, which supplies the sinks and wash hand basins.

**Drainage:-** Drainage is understood to be uPVC pipework, which is believed to connect to the mains council drainage system. Underground drainage may be earthenware however, this would require confirmation.

**2.0 INSTRUCTIONS / EXCLUSIONS**




- 2.1 Specific instructions were received from Ciaran Cooper, Asset and Facilities Management, Dunedin City Council, to undertake a condition report of the Fortune Theatre, located on the corner of Moray Place and Upper Stuart Street, Dunedin.
- 2.2 The fire, mechanical and electrical services are specifically excluded from the condition report, with only general comments being made.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**










**3.0 BRIEF HISTORY**

- 3.1 The Trinity Methodist Church was designed as an example of Gothic Revival Architecture by R. A. Lawson and was opened in 1870.
- 3.2 The north western stairwell was constructed in the 1930's. The south western yard area was converted to toilets in the early 1960's.
- 3.3 In 1966 the building was structurally strengthened and waterproofed and then the church was refitted in 1977 and then operated as the Fortune Theatre until 2018.
- 3.4 There was a fire in 1979 within the western transept with the restoration of the roof following this fire damage undertaken in 1980 including replacement timbers and internal linings.
- 3.5 Repointing was carried out in 2004 / 2005.
- 3.6 The property is listed as a Category 1 Historic Place with Heritage New Zealand formally Historic Places Trust (NZHPT) and further information will be required in this regard so that you are familiar with the conditions placed on the property. The registration number is noted as 3378.

**4.0 EXECUTIVE SUMMARY**

- 4.1 The executive summary provides the main findings, any items which require immediate attention or further investigation, including recommendations for future repairs and any other issues pertinent to the investment in this property. Reference will need to be made to the main body of the report, any appended specialist reports, costings, and additional analysis as necessary.
- 4.2 This executive summary is a précis of the findings and therefore, it is recommended and important that the report is read in full.
- 4.3 The subject property comprises the former Trinity Methodist Church and Fortune Theatre. The building was designed by R. A Lawson, a prominent Architect who played a significant role in shaping the architectural landscape of Dunedin.
- 4.4  Considering the buildings age, there were no significant signs of movement or deflection observed to the building structure at the time of inspection. The building, its components and systems are typically aged with areas of deferred maintenance. The building is currently void, with all soft furnishings having been removed and the constant use of dehumidifiers.
- 4.5  The roof is considered to be complex with complex junctions which will require diligent monitoring and cleaning with removal of any debris or vegetation growth on regular basis. We are advised that the slates are original and are Welsh in origin and there were some areas observed with localised undulations, which require further investigation and monitoring. The metal flashings including the apron flashings to the rear of the parapets require closer inspection with corroded flashings likely in need of replacement.
- 4.6  The hidden flat roof to the south western corner of the property is understood to comprise a single ply membrane which will require further investigation. These roof coverings and their associated structures are considered higher risk for defects associated with weathertightness failure and condensation and require diligent monitoring and maintenance.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

- 4.7  Similarly, the copper roofs being low pitch with standing seams which pose a higher risk potential for defects and weathertightness failure.
- 4.8  Rainwater is discharged over earthenware gully traps and direct into the under-ground drainage system. There have been some inappropriate replacements to the rainwater disposal system with white uPVC pipework. The older iron pipework requires checking for corrosion and then being suitably prepared or replaced prior to painting with rust-resistant paint. Gully traps require monitoring and cleaning on a regular basis.
- 4.9  The property is located within exposure Zone C which has a medium risk for exposure to wind driven salt laden air, and this has potential to accelerate deterioration of the materials at the property.
- 4.10  The stone walls comprise Leith Valley trachyandesite (Dunedin Bluestone) and Oamaru stone. These walls being of solid construction have inherent defects and have been subject to continual wetting with some of the subsequent defects observed as the moisture staining and efflorescence to the interior faces.
- 4.11  Consideration will need to be given to the external ground levels and surface water drainage and attenuation to alleviate undue dampness into the building. Specialist advice will be required in this regard taking into account all potential sources of moisture along with their ongoing maintenance, so that an effective solution can be implemented. Further consideration will also need to be given to the inclusion of an external moisture resistant barrier to the basement walls and floors or internal tanking system with a drained cavity to the interior with a sump and pump to remove moisture, although this would result in a loss of space. Suitable heating and ventilation to these areas will also assist and specialist advice would be required.
- 4.12  There has been inappropriate cement based repairs to the mortar joints and to the Oamaru stone which have previously been repaired and painted. The paint was typically observed as aged and defective with blisters and delamination. Further specialist advise is also required in this regard to ensure the ongoing integrity and durability of the stonework and its pointing.
- 4.13  The capping to the gable ends were observed to overhang the external walls however, are considered to be a higher risk for weathertightness issues and are typically subject to cracking with potential for undue moisture to ingress to the solid walls below. There is a build-up of dirt, moss and lichen growth which should be suitably cleaned. It is recommended that professional advice is sought in terms of a suitable waterproofing application which remains breathable to the gable and parapet capping and rear of the parapet walls to roof junctions.
- 4.14  Check and maintain the structural strengthening plates and monitor structural beams for any signs of corrosion, or deflection. Regular inspections and periodic assessments are critical to identifying issues early and implementing appropriate maintenance or repairs to ensure the long-term stability and safety of the structure.
- 4.15  The steel window frames, and glass require regular washing to prevent build-up of surface contaminants. The windows would benefit from easing, adjusting and lubrication. The external frames should have defective paint removed, and any areas of corrosion treated prior to re-decoration. Further investigation is required to the leadlight windows with effective assessment and repairs with cracked glass being replaced. Ideally, all services and electrical conduit that penetrate the glazing should be re-routed and glazing replaced.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

- 4.16  Check and repair any gaps or cracks around stone windows and walls. Install robust flashings to the door thresholds to redirect water away from these vulnerable areas. Further investigation and monitoring are required to the entrance doors and associated flashings to ensure that they are robust and weathertight. The entrance doors and lancet top lights would benefit from general repair, maintenance, and painting.
- 4.17  The timber floor structure at the perimeter edges will require further inspection and should only be retained if found to be sound and durable. These were typically built into the solid stone / masonry walls and would be subjected to moisture with a high risk potential for defects and decay. Any embedded structural timbers will likely require replacement with materials less sensitive to moisture.
- 4.18  The stairs to the north western stairwell will require reinstatement, preferably to match the existing. This work should only be undertaken once the undue dampness or suitable alternative solution to the penetrating dampness has been resolved or alleviated.
- 4.19  Similarly, moisture damaged material has been removed from the ceilings and the rafters have been left exposed to dry out. Further investigation is required to any timber exposed to moisture and appropriate remedial works will require specialist advice, design, and remediation.
- 4.20  Internally the facilities have not been in use for a number of years with the majority of soft furnishings and floor coverings removed. The interior now requires extensive refurbishment, and some serious consideration will need to be given to the building's future use. All internal timber panelling will need to be removed to fully assess the condition of the solid external and basement masonry walls. Generally, the toilet facilities are aged, poor quality and will benefit from upgrading with consideration given to disabled access.
- 4.21  The western elevation is currently utilised as a buttress to the neighbouring retaining wall with concrete braces installed. There was no information or calculations provided in this regard and this requires further inspection and comment from a structural engineer.
- 4.22  Dependent upon the future usage of the buildings, it is recommended that an accessibility report is commissioned with access and facilities for persons with disabilities being fully considered.
- 4.23  The heating and air conditioning systems are aged, and specialist consultant reports are required dependant on the future usage of the building.
- 4.24  No specialist laboratory testing has been carried out to confirm the presence of asbestos or any other material hazardous to health with comments herein based upon a visual inspection only. The older thermoplastic tiles observed have potential to contain asbestos (ACM's). Further clarification is required, and any hazardous material reports should be consulted or commissioned as necessary prior to undertaking any remediation works.

(Green) – Low Risk – Normal and planned maintenance required. Further investigation and clarification may also be required which may impact on future maintenance expenditure.



(Yellow) – Moderate Risk – Some immediate action and management in the short to medium term with potential to impact on the building occupant's usage.



(Red) – High Risk – Immediate action required with potential to affect critical operations.



**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

**5.0 ROOFS & RAINWATER GOODS**

- 5.1 The roof was viewed from ground levels as it was unable to be accessed. Accessibility for monitoring and maintenance of roofs was considered to be difficult and care must be taken when accessing the roofs. It is recommended that planks or roof ladders are utilised to spread the load evenly over the tiles.
- 5.2 The roof comprises natural slate tiles, which appeared in reasonable to average condition. The slates were noted as being differing colours which may suggest replacements have occurred over time although if original, these will be around one hundred and fifty (150) years old. Depending on the circumstances, new slates are typically mixed with old ones across the roof or should be reserved for the less prominent slopes.



Photograph 1 – Showing the south western roofs and flat roof area above the extension.



Photograph 2 – Showing the south western roofs and copper valley gutter.



Photograph 3 – Showing the south western roofs and flat roof area above the extension.



Photograph 4 – Showing the south western roof noting slipped slates and extensive rust to the ridge capping.

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**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

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- 5.3 Roofs using good natural slate, properly maintained should last in excess of 100 years. It is typically fixings and the supporting timber battens / structure that deteriorate before slates. The interior roof space should be inspected to assess the underside of the slates which can be subject to delamination, which would make them a higher weathertightness failure risk. Similarly, fixings should be checked for signs of corrosion and tile battens for signs of decay due to insect or fungal attack, which may be the reason behind the slipped slates noted and the undulations noted to the roof slopes.
- 5.4 Natural slate is relatively non-porous, and weathertightness depends primarily on adequate lap and gauge rather than material thickness. The tiles are likely head nailed and double lapped which makes them more robust in terms of their weathertightness due to fixings being covered by two thicknesses of slate, although the actual fixing type was not confirmed. The roof is considered to be complex in its design being that of a Latin Cross and as such require good maintenance to keep the weather out.
- 5.5 The slates viewed remain clear of any significant moss or lichen growth. The tiles appeared to remain secure although there were some isolated areas with slight slippage noted which will need suitably secured and monitored.
- 5.6 There was a slight undulation observed to the slate roof covering, on the northern slope at the western end in particular. This is possibly due to aging and potential failure or movement of tile battens or roof structure which will require further investigation and monitoring.
- 5.7 Isolated slipped, broken or missing slates should be re-secured or replaced using copper wire ('tingles') or equivalent proprietary fixings. If battens are replaced, nails could be used instead of tingles. It should be noted that old slates should not be reversed, and any broken slates should be stored for re-dressing and re-use elsewhere on the roof in the future.
- 5.8 Foam or bituminous-type remedial treatments are not advisable as they prevent proper inspection, hinder the re-use of old slates and, by reducing ventilation, increase the risk of timber decay.
- 5.9 If new slates are required, they should usually match the type, colour, texture, size, and thickness of the existing ones. Although sourcing slates may prove difficult, artificial materials are considered an inappropriate material.
- 5.10 The ridges, comprise metal flashings with copper valley gutters. The ridge flashings were observed to be showing signs of deterioration and red-rust corrosion, with some associated staining noted to the slates below these areas.
- 5.11 It appears that an original lead flashing has been utilised at the four-way junction to the ridge. This is aged and did not appear to be well dressed into position when viewed from ground levels and requires closer inspection when accessing the roof.
- 5.12 Roofs using slate tiles, are considered to be robust however, repair or re-tiling should always be entrusted to an experienced and reputable roofer with knowledge of this particular style of installation. If not already done so, it would be considered prudent to commission a specialist roofing contractor to periodically assess the current condition of the roofs.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 5 – Showing the corroded ridge flashings to the north.

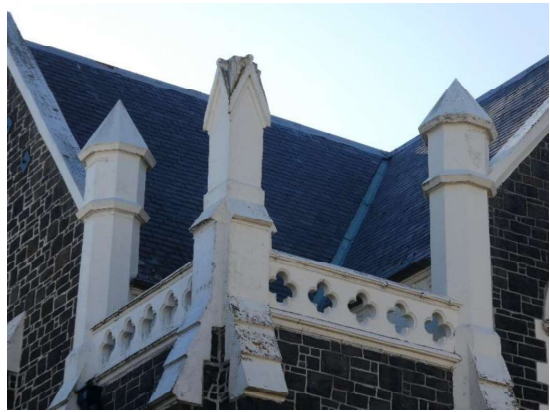


Photograph 6 – Showing the corroded ridge flashings to the north.

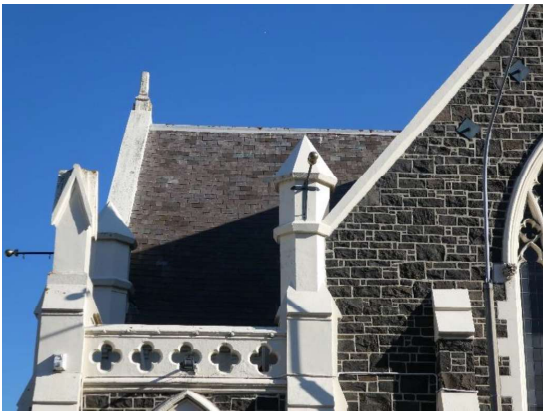
5.13 Any moss or algae growth should be periodically cleaned, and damaged tiles that are found to be cracked or damaged should be replaced upon discovery.



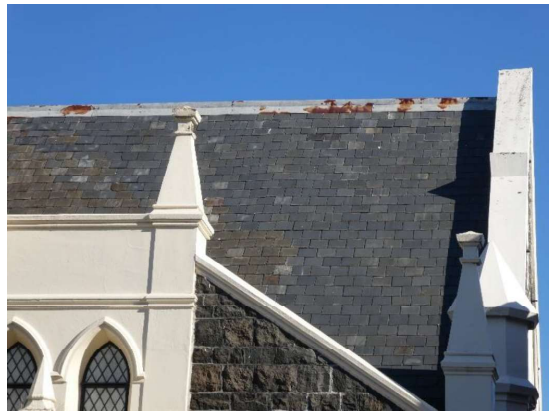
Photograph 7 - Showing the slate roof to the south eastern corner.



Photograph 8 – Showing the slate roof to the south eastern corner.



Photograph 9 - Showing the slate roof to the north eastern corner.



Photograph 10 – Showing the slate roof to the north western corner.

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- 5.14 The flat roof areas to the original church were not able to be viewed during the inspection. It is understood that these are copper standing seam roofs. There were some areas observed to the north western roof, where the edge of the flashing with the slate have lifted and these areas will require further investigation and closer inspection.
- 5.15 The flat roof above the south western addition to the original building was not able to be viewed during the inspection.

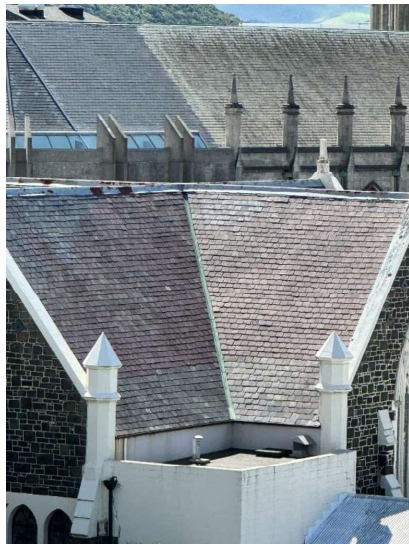


Photograph 11 – Showing the edge detail to the copper standing seam roof to the north western corner.



Photograph 12 – Showing the flashings detail to the rear of the parapet wall to the north western corner.

- 5.16 It is understood to comprise a single ply membrane which will require further investigation. These roof coverings and their associated structures are considered higher risk for defects associated with weathertightness failure and condensation.
- 5.17 The rear of the concrete masonry parapet walls was not able to be viewed. These walls are not suitably flashed or capped, and it is unlikely that an effective damp proof course would have been provided. Therefore, there is an inherent risk and potential for moisture to be able to transfer directly into the top of these walls.



Photograph 13 – Showing the south western roofs and flat roof area above the extension.



Photograph 14 – Showing the south western roofs and flat roof area above the extension.

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5.18 The rear of the parapet walls was not able to be fully viewed. It is unlikely that an effective damp proof course would have been provided. Therefore, there is an inherent risk and potential for moisture to be able to transfer directly into the top of these walls. Although the capping overhangs these walls and provides for a traditional drip edge affording some protection to the plastered walls, this is limited, and an accumulation of dirt and subsequent staining was observed to the rear of these walls.

5.19 The flashings at the roof to rear of the parapet walls and gable ends were not able to be fully viewed at the time of inspection.

5.20 Where observed these appeared to be painted lead or metal flashings. It is assumed that these are chased into the wall and stepped however, the plaster is installed over these flashings, and these should be further inspected to ensure that they remain intact and suitably sealed.



Photograph 15 – Showing the replacement copper roof to the bell tower.

5.21 The turret roof comprises a copper covering and appeared in reasonable condition when viewed from ground level. Copper is known for its durability, longevity, and the turret roof has an aged patina with a greenish hue appearance. Periodic inspections are recommended to ensure that these roofs remain in reasonable condition.

**Rainwater Fittings**

5.22 The efficiency of rainwater fittings (gutters and downpipes) can only be assessed properly during the inspection if there is heavy rain. Weather conditions are stated earlier.

5.23 Rainwater is typically discharged via uPVC spouting and via hoppers from the hidden flat roof areas to copper and uPVC rainwater downpipes direct into the under-ground drainage system.



Photograph 16 – Showing the hopper and rainwater downpipe from the south eastern roof.

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Photograph 17 - Showing the rainwater hopper and iron downpipe to the north western corner.



Photograph 18 – Showing the rainwater hopper and iron and uPVC downpipe to the south western corner. Noting the poor detailing between the stone and concrete masonry extension.

5.24 The scupper outlets and hoppers will require monitoring and cleaning on a regular basis.

5.25 The lower levels to the rainwater downpipes are typically in average condition, with areas of paint failure. The offset bend in the downpipe from the bell tower appears poorly sealed and requires further investigation.



Photograph 19 – Showing the replacement uPVC rainwater downpipe and inspection point to the southern elevation.



Photograph 20 – Showing the uPVC rainwater pipework to the western elevation.



Photograph 21 – Showing the uPVC rainwater pipework to the western elevation.

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**6.0 EXTERIOR**

6.1 The walls typically comprise a combination of split faced stone and painted decorative Oamaru stone, with glazed timber and steel joinery.



Photograph 22 - Showing the south western elevations.



Photograph 23 – Showing the southern elevation.



Photograph 24 - Showing the eastern elevation.



Photograph 25 – Showing the eastern elevation.

- 6.2 The stones are differing size and roughly squared being laid in horizontal courses with rising stones projecting through the courses of smaller stones with smaller fillers called snecks also occurring in the courses. The masonry generally appeared to remain true to line, level and plumb to all elevations although no measurements were taken.
- 6.3 Oamaru stone has been subject to cement based repairs and painted. If possible, it would be recommended that the paintwork to the Oamaru stone is carefully removed, and suitable stone repairs are undertaken, with advice sought from a suitably qualified conservation specialist with regard to sealing these areas with a breathable system.
- 6.4 The stonework to the main structural masonry walls is understood to be local grey trachyandesite igneous rock or Dunedin bluestone, which contrasts the now unfortunately painted Oamaru stone which was utilised for the decorative facings and decorative mouldings.
- 6.5 Notwithstanding the age of the building and the inappropriate repairs over time, the main stonework generally remains in reasonable condition with no obvious or significant cracks noted during the visual inspection.

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- 6.6 The stonework joints appear to have been ribbon or strap pointed to give a more regular joint to the irregular masonry. This is likely a cement based mortar which will trap moisture in the joints and cause erosion and effect the durability of the original stonework. The cracking observed remains at risk of moisture penetration, and further deterioration and cracking due to loss of adhesion, which could also be accelerated due to frost action. The pointing should be removed if this can be done without causing further damage, and the stonework re-pointed with a softer lime mortar.
- 6.7 The north and south eastern corners of the building house porches under flat copper roofs with quatrefoil mouldings to the balustrade. The balustrade is understood to be concrete and was reinforced with stirrups and steel pins drilled into the finials circa 1964 with the removal of the original Oamaru stone balustrade.
- 6.8 Vertical ornamental pinnacles crown the stone buttresses, which are also believed to be painted Oamaru stone. There are areas of paint failure to weathering at the top of the buttresses. The lichen growth to the apex stones and finials should be removed, and then further assessment of their condition confirmed.
- 6.9 The louvres installed to the gable ends are in average condition with some of the louvres noted as broken or missing and these should be replaced along with any bird protection measures that may be required.



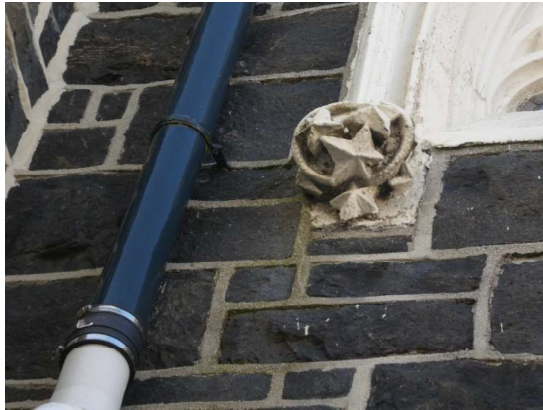
Photograph 26 - Showing typical pinnacle detail to the northern elevation.



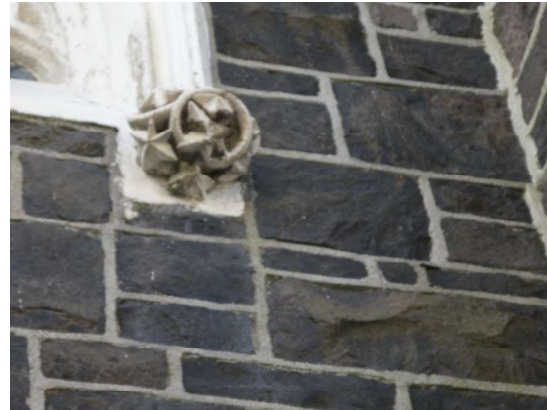
Photograph 27 - Showing the pinnacle to the northern gable.

- 6.10 There is an octagonal bell turret to the eastern Stuart Street elevation. This was also subject to structural strengthening circa 1964. The spouting to the turret was observed to be stained and it appears to have had some bird protection added although the extent was not confirmed. This would require further investigation to ensure that it remains secure and fit for purpose. The lancet vents to the turret have galvanised chicken wire installed to deter bird entry and these areas should be checked to ensure correct fitting and renewed and replaced as required.
- 6.11 The windows are topped with lancet arches with the label moulds to the exterior edge of the voussoirs with the label stops being intricate foliage carved bosses. The tracery windows and lancet arches to the entrance doors and the windows are understood to be Oamaru stone and these have received a paint coating. Periodic inspection is required for any signs of damage, such as cracks, chips, or missing pieces. along with cleaning, repairing, and, if necessary, replacing damaged tracery moulded elements. Additionally checks should be made for any loose or deteriorating mortar around the tracery.

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Photograph 28 - Showing typical boss detail to the windows.



Photograph 29 – Showing typical boss detail to the windows.

6.12 There are areas with blistering paintwork, which are typically as a result of moisture penetration, entrapment, and dampness behind the plaster, along with the improper or inadequate surface preparation. Blisters in paint usually occur when moisture evaporates to form a vapour bubble under an impermeable layer of paint, especially if the coatings are thin.

6.13 The stone gables were observed to have been strengthened and are understood to be post tensioned metal tie rods which should have been designed to reduce the risk of lateral movement.

6.14 The louvres installed to the gable ends which provide for cross ventilation to the roof space are in average condition with some of the louvres noted as missing which should be replaced. Calculations may be required to ensure these smaller roof spaces remain positively ventilated and further specialist advice may be required in this regard.



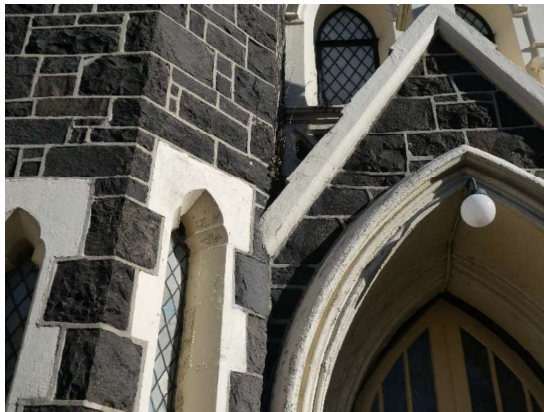
Photograph 30 – Showing the metal tie rods to the northern gable.

6.15 The painted Oamaru stone string course is in average condition with paint failure and vegetation growth to some areas which should be removed.

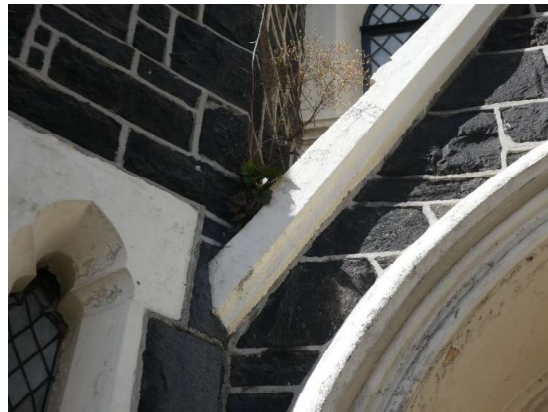
6.16 Some consideration may need to be given to the security cameras to the north western elevations as there were some areas which have suffered from graffiti. This should be suitably cleaned and has the potential to increase maintenance / painting costs.

6.17 Vegetation or biological growth was observed to the stonework on the northern and western elevations which is unsightly and typically a sign of undue dampness to these particular areas. Without regular maintenance and cleaning, biological growth has the potential to cause subsequent moisture issues or possibly structural defects. It is recommended that all biological growth is assessed and carefully removed, and this may require specialist evaluation and advice depending on the type of growth.

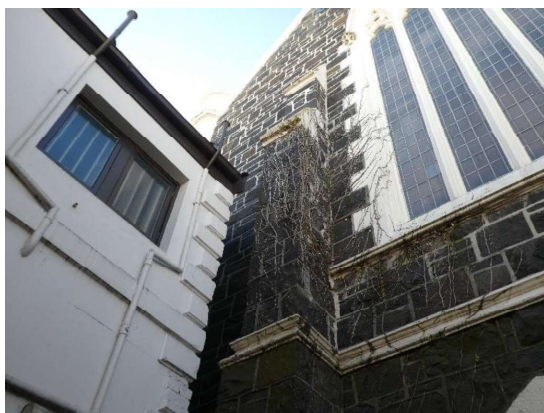
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Photograph 31 – Showing the vegetation growth above the northern entrance.



Photograph 32 – Showing the vegetation growth above the northern entrance.



Photograph 33 – Showing the vegetation growth to the western courtyard area.



Photograph 34 – Showing the vegetation growth to the western courtyard area.



Photograph 35 – Showing the vegetation growth to the south eastern buttress.



Photograph 36 – Showing the vegetation growth to the north western buttress.

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Photograph 37 - Showing the vegetation growth to the north western buttress. Photograph 38 – Showing the graffiti to the north western buttress.

- 6.18 The extension to the south western corner of the building comprises painted concrete masonry blocks.
- 6.19 Staining was observed from the parapets which do not appear to be suitably capped or flashed. The junction where concrete masonry abuts the original stonework does not appear robust. These areas require further investigation with suitable flashings being designed and installed as required.
- 6.20 The louvres to the ventilation grilles were all broken and require replacement along with suitable flashings to ensure their weathertightness.
- 6.21 There is potential for defects to arise as a result of the inadequacies of a single skin unfilled concrete masonry wall and whilst there were no obvious or significant issues noted at the time of inspection these areas should be monitored to ensure that there is no penetrating dampness occurring to these areas.
- 6.22 This wall appears to be constructed off of an existing brick wall with an insitu concrete beam installed below the concrete masonry blocks. The horizontal junctions would be considered higher risk areas for moisture penetration with vegetation growth noted to the top of the brickwork. Whilst there was no obvious movement noted, it is recommended that further inspection and comment is obtained from a structural engineer.



Photograph 39 – Showing the two-story extension to the south western corner.

Photograph 40 – Showing the two-story extension to the south western corner.

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Photograph 41 – Showing the two-story extension to the south western corner.



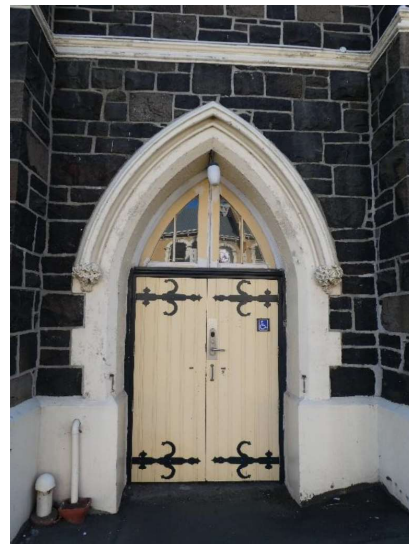
Photograph 42 – Showing the two-story extension to the south western corner.

**External joinery, windows, doors, and flashings**

6.23 The double entry doors to the south eastern elevation on Stuart Street appear in reasonable to average condition. There have been some previous repairs undertaken and the doors and top lights require re-painting. The mock hinges installed to the timber entrance doors appear reasonable however, should be sanded, suitably prepared and re-painted.

6.24 The concrete entrance steps and associated stainless steel handrails to the south eastern entrance doors remain in reasonable condition.

6.25 All entrance doors would benefit from easing and adjusting to ensure their correct operation.

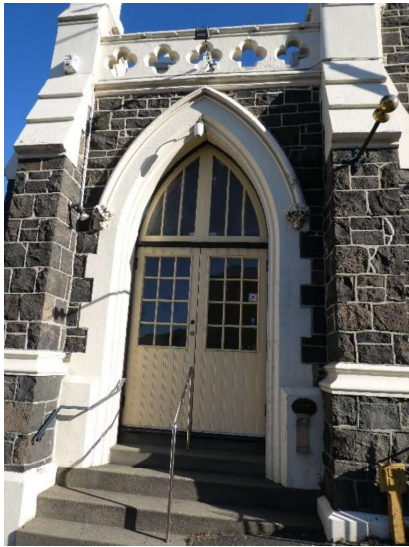


Photograph 43 – Showing the south western double entrance doors.

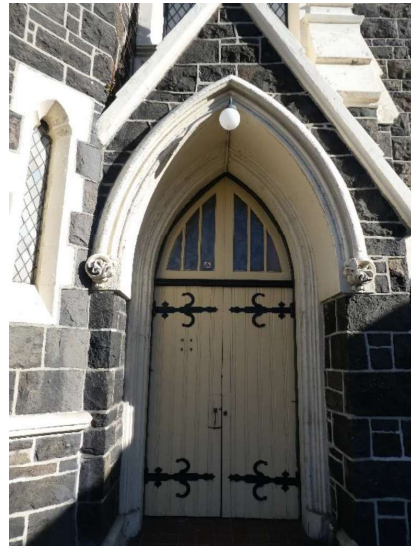
6.26 The double entry doors to the north western elevation on Stuart Street appear in average to poor condition and are ill-fitting. Consideration will need to be given to their replacement.

6.27 The weatherstripping to all external doors should be checked and upgraded as required to ensure prevention of water infiltration around the door frames.

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Photograph 44 - Showing the north eastern double entrance doors.



Photograph 45 – Showing the north western double entrance doors.

6.28 The windows typically comprise single glazed leaded steel and timber windows which are aged and in reasonable to average condition with some minor damage and cracked glazing observed. There is some corrosion noted to these steel window frames to some areas, due to degradation of the protective paint coating likely because of weathering, age, and lack of maintenance.

6.29 The putty to some of the glazing is deteriorating and requires replacement and painting. It is recommended that specialist advice is obtained prior to remediation work with regard to the ornate leadlight windows.

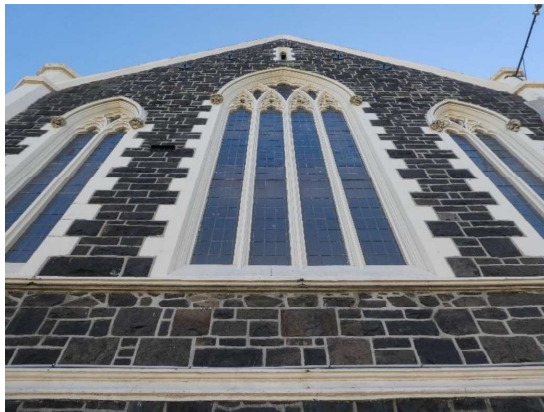


Photograph 46 - Showing the glazed top light above the northern entrance door.

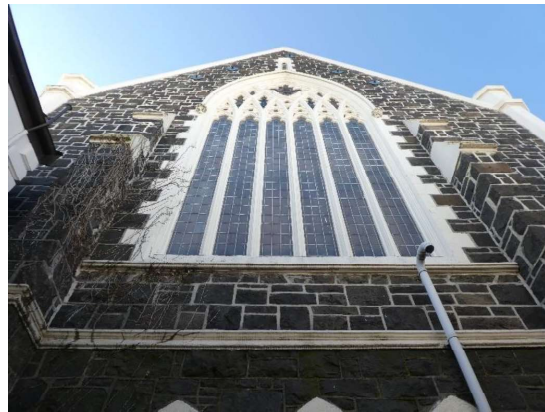


Photograph 47 – Showing the unfinished painting to the glazed top light above the southern entrance door.

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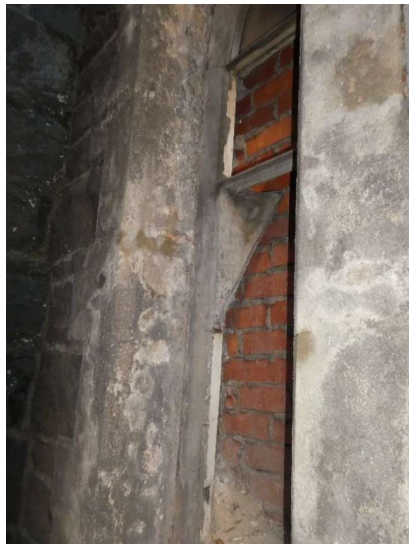


Photograph 48 - Showing the eastern tracery window.



Photograph 49 – Showing the southern tracery window.

- 6.30 These original single glazed windows are not considered to be robust in terms of energy efficiency and poor fitting openings and gaps will cause heat loss. Steel being a good conductor of heat and therefore, a poor insulator will always be at risk of condensation to the frames and possibly the glazing during periods of colder weather.
- 6.31 Steel window frames and glass require regular washing to prevent build-up of surface contaminants. The external frames should have defective paint removed, and any areas of corrosion treated prior to re-decoration. The opening casements require easing and adjusting, with consideration given to improving draught proofing and locking mechanisms.
- 6.32 The glazing is broken to the bricked up window within the alcove to the south western courtyard area. There are cracks to the glazing of the windows to the toilets with poorly installed and sealed pipe penetrations through the glazing. A temporary hood is installed to the central window above the extract fan which is without a suitable grille or cowl. The windows require glazing and putty replacement with consideration given to relocation or suitable alterations and sealing of pipe penetrations.

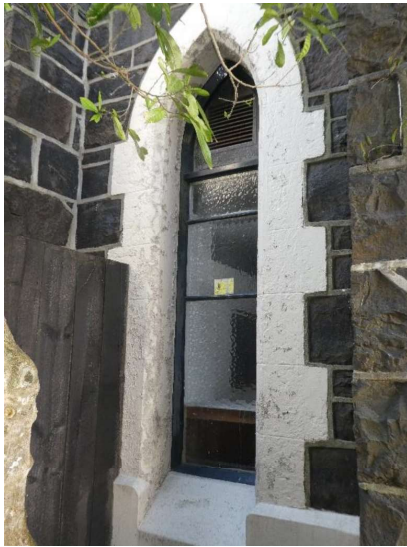


Photograph 50 - Showing the broken glazing to the steel framed windows which has been bricked up to the south western courtyard area.



Photograph 51 – Showing the steel framed windows on the western elevation. Noting inappropriate positioning of services.

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Photograph 52 - Showing the steel framed windows to the south western courtyard area. Noting paint failure and replacement putty.



Photograph 53 – Showing the steel framed windows to the south western courtyard area. Noting paint failure, and replacement putty.



Photograph 54 - Showing the steel framed windows on the western elevation. Noting inappropriate positioning of services.



Photograph 55 – Showing the steel framed windows on the western elevation.



Photograph 56 - Showing the steel framed windows on the southern elevation. noting paint failure, cracking to the sill and un-painted replacement putty.



Photograph 57 – Showing the steel framed windows on the southern elevation.

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Photograph 58 - Showing the steel framed windows on the southern elevation.



Photograph 59 – Showing the tracery window on the southern elevation.



Photograph 60 - Showing the leadlight lancet window and buttress to the north eastern elevation.



Photograph 61 – Showing the leadlight lancet window and buttress to the north eastern elevation.



Photograph 62 - Showing the rose window to the western elevation.



Photograph 63 – Showing the western elevation. Noting corrosion to the steel window frame

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**Deleterious Materials**

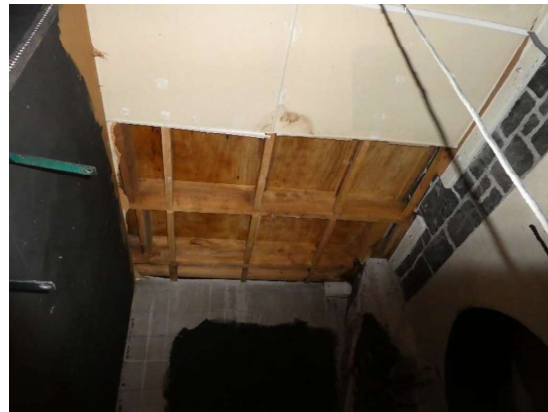
- 6.33 Asbestos products have often been used in building construction. It can be difficult to identify products containing asbestos, particularly if they are covered or painted.
- 6.34 This report is not an asbestos survey however, based on the timing of the alterations and upgrades at the property, it is possible that there may be asbestos containing materials which may include the thermoplastic tiles observed.
- 6.35 Inhalation of asbestos fibres is hazardous to health and therefore, care should be taken in any operations, which involve disturbance or removal of materials suspected of containing asbestos and specialist advice should be sought before undertaking any remediation works at the property.
- 6.36 There are uPVC materials present at the property and although commonly used in buildings these contain chemicals which could lead to adverse health risks.
- 6.37 Where the basement areas have been subject to excess moisture over a number of years with mould growth observed, it is recommended that a detailed inspection is undertaken along with sampling to identify the microorganisms present along with a scope of remedial works for its safe removal.
- 6.38 Whilst there were no other obvious observations during the inspection that would be considered deleterious material, no comment can be made with regard to any concealed areas; this would typically include VOC's formaldehyde or lead, although these may be present in paint, custom-wood and insulation products.

**7.0 ROOF SPACE**

- 7.1 The upper roof spaces were not accessed at the time of inspection due to height restrictions. There is an open access within the control room which should have a suitable cover or hatch fitted.
- 7.2 The main roof structure comprises timber tile battens of rafters supported onto timber king trusses. The majority of the roof structure is exposed with timber tongue and groove panelling installed which appears to remain in reasonable condition although the perimeter in-built timber structure has the potential to be moisture compromised and would require further investigation.
- 7.3 Ventilation is required to cold roof spaces, and this is currently provided to the upper roof spaces via thin rectangular gable louvres with chicken wire installed to the exterior. These would benefit from cleaning and re-painting.

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7.4 Where the ceiling has been removed to the south eastern stairwell moisture staining was observed to the timber sarking below the roof slates. Some further investigation would be required and there is likely to be varying levels of slate deterioration which may require remedial works.



Photograph 64 – Showing the exposed and uninsulated flat roof to the south western extension.

7.5 Where the ceiling lining has been removed to the south western area / stage right, the plywood and timber roof structure was observed to be uninsulated, with moisture staining to the remaining fibre cement lining.



Photograph 65 - Showing the paint failure to the hidden walls within the roof space above the kitchen / bar.



Photograph 66 – Showing the paint failure to the hidden walls within the roof space above the kitchen / bar.



Photograph 67 - Showing the ceiling joists and the hidden top to the leaded lancet window within the roof space above the kitchen / bar.

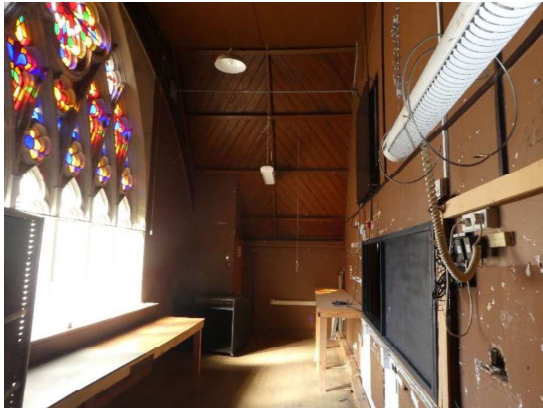


Photograph 68 – Showing the original ceiling joists and strengthening works to the rafters within the roof space above the kitchen / bar

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**8.0 INTERIOR**

- 8.1 Floor surfaces and under-floor spaces are examined as far as they are reasonably accessible (furniture, floor coverings and other contents are not moved or lifted). If a part or area normally examined was found not accessible, this is reported, if a problem is suspected, advice is given on what action should be taken. (In some cases, when furniture and pictures are removed internal decorations may prove to be damaged or faded).
- 8.2 The interior of the building has been partially stripped of its fixtures and fittings although there are a number of aged storage cupboards and shelving units that will require removal.

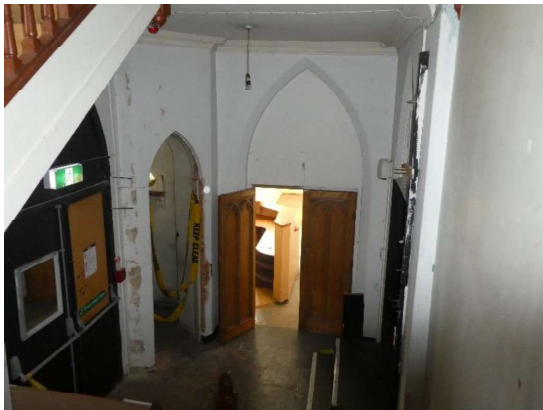


Photograph 69 - Showing the meeting / theatre technical room.



Photograph 70 – Showing the meeting / theatre technical room.

- 8.3 The painted steps leading from stage left to the stage area are in poor condition and would benefit from being removed or replaced.



Photograph 71 - Showing the north western vestibule / stage left.



Photograph 72 – Showing the staircase from the north western vestibule / stage left.

- 8.4 The south eastern entrance lobby was locked shut and therefore not accessed with limited viewed obtained through the glazed doors which are in reasonable condition. The floors, ceilings and external walls are expected to be in similar condition to the entrance foyer and bar areas although further inspection is required.

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Photograph 73 - Showing the reception desk and main entrance foyer.



Photograph 74 – Showing the reception desk and main entrance foyer.



Photograph 75 - Showing the reception desk and doors to the north western vestibule.



Photograph 76 – Showing the double doors leading from the entrance foyer to the bar.



Photograph 77 - Showing the eastern bar area.



Photograph 78 – Showing the eastern bar area.

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Photograph 79 - Showing the southern bar area and stairs to the lower entrance and rear entrance to the main theatre.

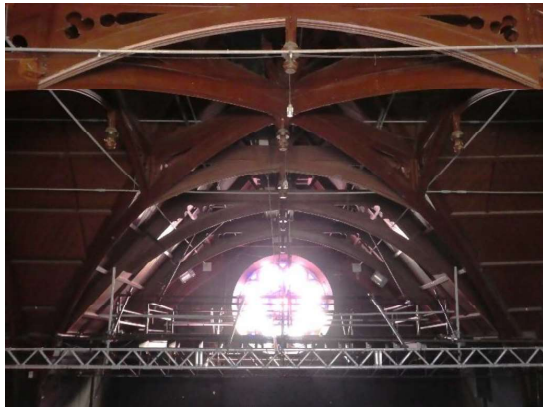


Photograph 80 – Showing the southern bar area looking towards the entrance foyer.

- 8.5 The timber / custom wood terracing to the main theatre remains insitu however, depending on the building’s usage will likely require removal or upgrading works such as nosings to steps.
- 8.6 The custom wood stage floor is aged and in average condition and similarly will likely require removal dependant on the propose future usage of this space.



Photograph 81 – Showing the southern kitchen adjacent to the bar area. Noting electrical sub-distribution board to the southern wall.

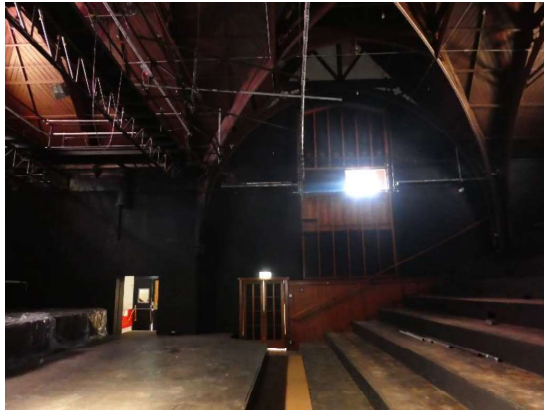


Photograph 82 - Showing the rose window looking west



Photograph 83 – Showing the main theatre looking to the east.

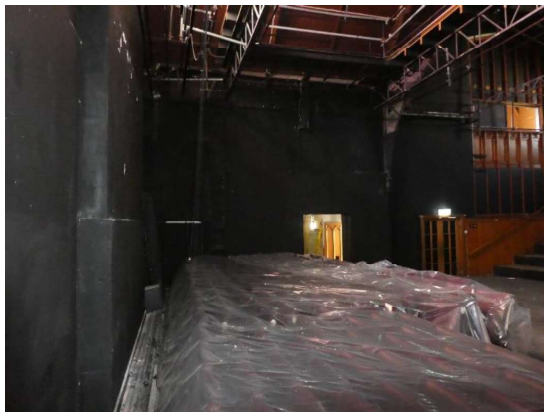
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Photograph 84 - Showing the main theatre looking to the north.



Photograph 85 – Showing the main theatre looking to the south.



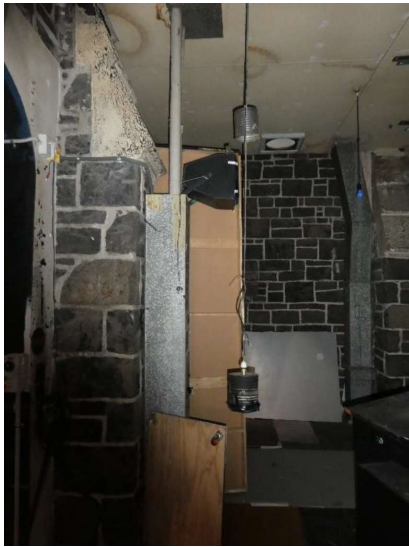
Photograph 86 - Showing the main theatre stage looking to the north.



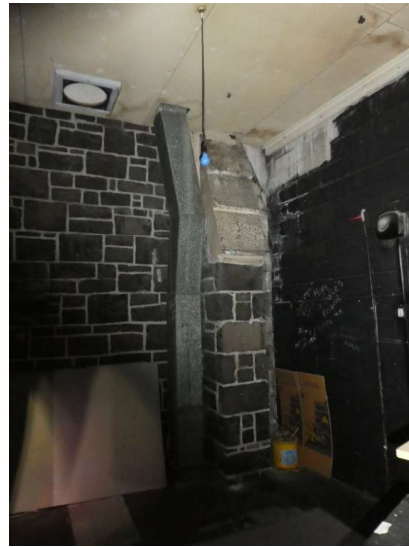
Photograph 87 – Showing the main theatre looking to the south.

- 8.7 The original external stone walls remain in the now enclosed stage right area which remain in reasonable condition. However, the ceiling linings are moisture stained and will require further investigation and replacement. It was noted that that there is limited or no ventilation to this space. The concrete masonry walls constructed circa 1978 have been painted black to the interior.
- 8.8 The south western stairwell and stage right area comprise partially painted concrete masonry blocks believed to be hollow core. Typically, these are concrete filled however sometimes they remain either partially filled or unfilled and this was not able to be confirmed at the time of inspection.
- 8.9 The blocks are laid in stretcher bond and appeared true to line, level and plumb and the mortar pointing remains reasonable. Being solid masonry, these external walls are not insulated.

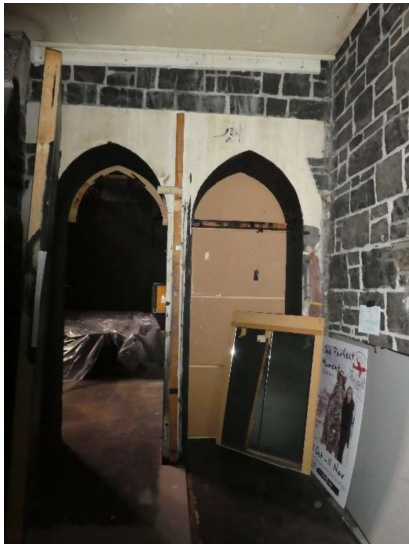
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Photograph 88 - Showing the exterior walls to the now enclosed area to the south western corner. Noting moisture staining to the ceiling linings.



Photograph 89 – Showing the exterior walls to the now enclosed area to the south western corner. Noting moisture staining to the ceiling linings.



Photograph 90 - Showing the exterior walls to the now enclosed area to the south western corner. Noting the original steel sashes removed.



Photograph 91 – Showing the exterior walls to the now enclosed area to the south western corner. Noting the concrete block wall and timber flat roof structure above. Noting the original steel sashes removed.

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Photograph 92 - Showing the south eastern entrance and vestibule.



Photograph 93 – Showing the south eastern entrance and vestibule.



Photograph 94 - Showing the lobby to the toilets and basement theatre.



Photograph 95 – Showing the entrance to the basement offices.



Photograph 96 - Showing the lobby to the basement theatre, toilets and offices..

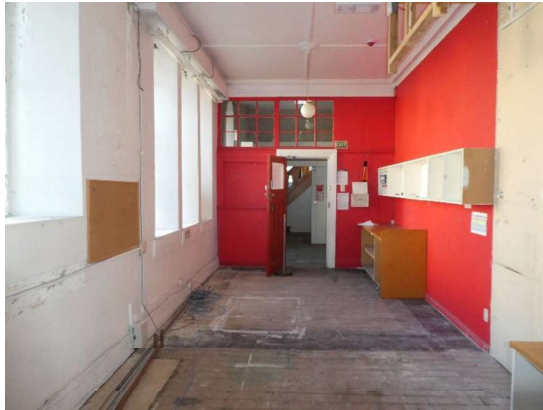


Photograph 97 – Showing the basement offices.

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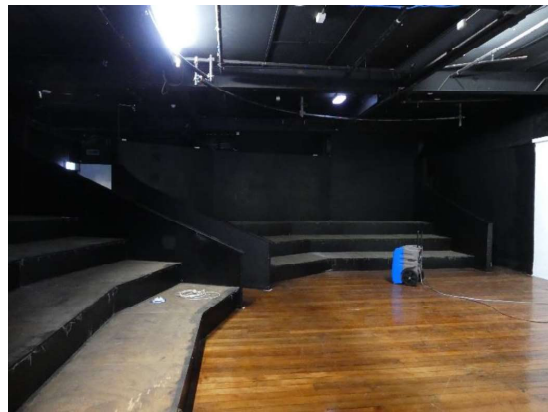
Photograph 98 - Showing the basement offices.



Photograph 99 – Showing the rear of the seating to the studio theatre.



Photograph 100 - Showing the basement studio theatre space.



Photograph 101 – Showing the basement studio theatre space.



Photograph 102 - Showing the Green Room and kitchenette behind the basement studio theatre. The upper levels appear to be the original glazing which has been painted.



Photograph 103 – Showing the Green Room behind the basement studio theatre. The upper levels appear to be the original glazing which has been painted.

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Photograph 104 - Showing the Green Room and kitchenette behind the studio theatre.



Photograph 105 – Showing the storage area / control room above the Green Room kitchen, accessed via ladder.



Photograph 106 - Showing the blocked up fire place within Dressing Room 1 accessed off the Green Room.



Photograph 107 – Showing Dressing Room 1 accessed off the Green Room.



Photograph 108 - Showing the unlined / plastered walls to Dressing Room 2 accessed off the Green Room.



Photograph 109 – Showing the unlined / plastered walls to Dressing Room 2 accessed off the Green Room.

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Photograph 110 – Showing the south western stairwell looking to the Operations Managers Office.



Photograph 111 – Showing the Operations Managers Office.



Photograph 112 - Showing the corridor formed behind the studio theatre seating.



Photograph 113 – Showing the corridor formed behind the studio theatre seating.

**Ceilings**

- 8.10 Typically, the ceilings to the main theatre are open to expose the King Post roof trusses and roof structure which remain in reasonable condition. These ceilings typically comprise stained timber tongue and groove panelling installed diagonally to the roof framing and painted fibrous plaster with decorative mouldings.
- 8.11 The ceilings throughout the remainder of the building are typically fibrous plaster, plasterboard, hard / softboard and fibre cement which are generally in poor condition and decorative order. The plasterboard ceiling to the basement toilets has been damaged where lighting has been removed.

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8.12 The moulded cornices to the basement areas were observed to be cracked, stained and defective likely due to ongoing moisture ingress and would need to be removed, and replaced with new, along with the renewal of the fibrous plaster ceiling linings.



Photograph 114 - Showing the fibrous plaster ceiling to the meeting / theatre technical room. Noting poorly cut and secured roof access hatch.



Photograph 115 – Showing typical moisture staining to the original fibrous plaster ceilings.

8.13 The original fibrous plaster ceiling has been partially removed to the north western stairwell exposing the timber ceiling battens, timber rafters and sarking below the slate tiles.

8.14 It is assumed that this ceiling was damaged as a result of moisture ingress and is being monitored for any signs of ongoing or progressive leakage.



Photograph 116 - Showing the open underside of the roof structure to the north eastern stairwell.



Photograph 117 – Showing typical moisture staining to the original fibrous plaster ceilings.

8.15 The rafters to the north western stairwell were observed to be built into solid brick external wall which have been exposed to undue moisture over time. Further investigation and comment are required from a structural engineer. It is likely that these areas will require remedial works and strengthening to maintain their structural integrity and prevent further or future damage.

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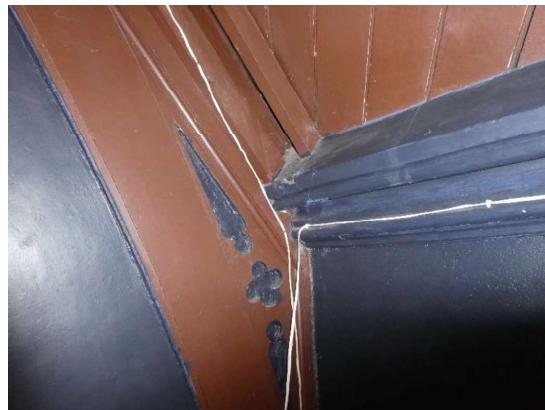
Photograph 118 - Showing the timber rafters built into the solid brick external walls to the western wall of the north western stairwell.



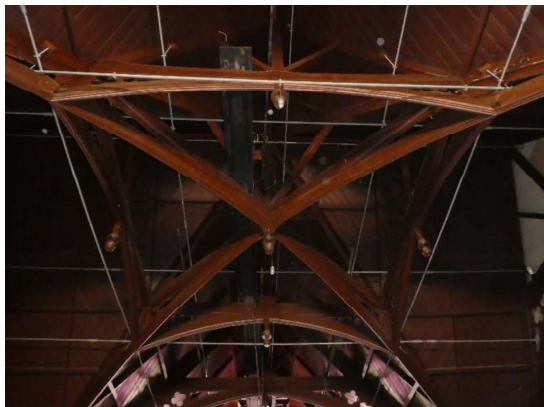
Photograph 119 – Showing the timber rafters built into the solid brick external walls to the western wall of the north western stairwell.



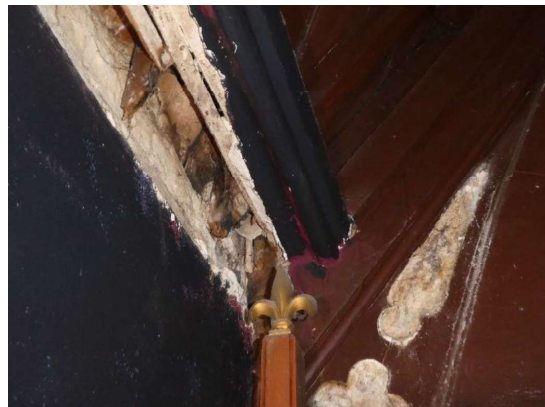
Photograph 120 - Showing typical painted timber rafter detail within the main theatre.



Photograph 121 – Showing typical painted timber rafter detail within the main theatre.



Photograph 122 - Showing the tie beams to the truss detail within the main theatre.

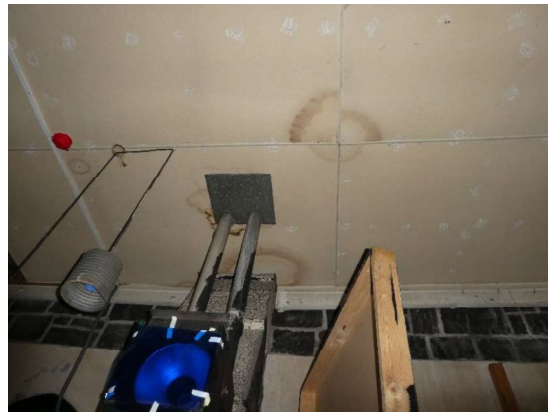


Photograph 123 – Showing typical moisture damage and decay to the rafters built into the solid external walls to the southern wall of the main theatre.

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Photograph 124 - Showing the moisture stained ceilings to the now enclosed south western area adjacent to the main stage.



Photograph 125 – Showing the moisture stained ceilings to the now enclosed south western area adjacent to the main stage..



Photograph 126 - Showing the storage area above the south western toilets.



Photograph 127 – Showing the storage area above the south western toilets.



Photograph 128 - Showing the moisture stained ceilings to the south western stairwell.



Photograph 129 – Showing the defective paint to the ceiling within the public male toilet.

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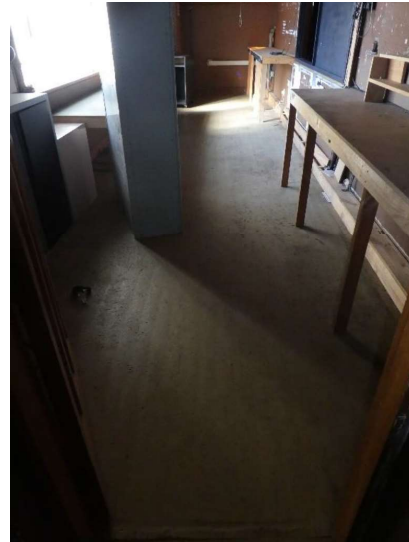
**Floors**

8.16 All soft floor coverings installed to the building have been removed. The floors typically comprise suspended timber tongue & groove and concrete flooring and appeared to be level with no significant deflection noted at the time of inspection, although no measurements were taken. The timber tongue and groove floors throughout are aged and would benefit from being sanded and re-varnished.

8.17 The floors to the bathrooms and entrance foyer typically comprise sheet vinyl over timber and concrete which are aged and in average to poor condition. Notwithstanding the proposed usage of the building, these floor coverings will require removal or replacement.

8.18 The carpet has been removed from the main entrance foyer exposing the timber floor boards although the carpet tack strips remain to the perimeter edges.

8.19 There are metal plates and custom wood panels installed to a redundant penetration through the boards in the foyer and bar areas. The thermoplastic tiles remain adjacent to the reception desk, and these should be tested to see if they were reinforced or adhesive fixed with asbestos.



Photograph 130 – Showing the damaged vinyl flooring to the meeting / theatre technical room.



Photograph 131 - Showing the timber floor boards to the main entrance foyer.



Photograph 132 – Showing the thermoplastic floor tiles to the main entrance foyer. **Note these tiles may contain asbestos.**

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Photograph 133 - Showing the timber floor boards and target repairs within the bar area.



Photograph 134 – Showing the timber floor boards to the south eastern vestibule.



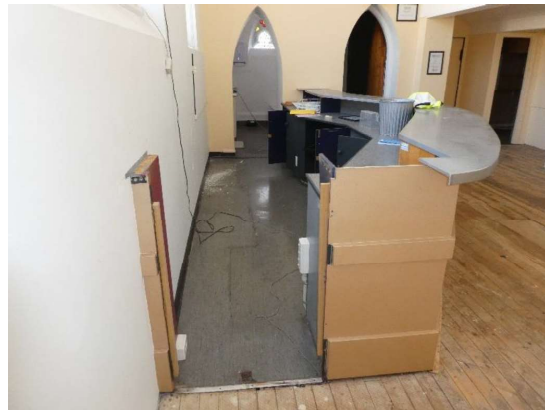
Photograph 135 - Showing the custom wood flooring off the south eastern vestibule leading to the toilets.



Photograph 136 – Showing the timber floor boards to the south eastern vestibule.



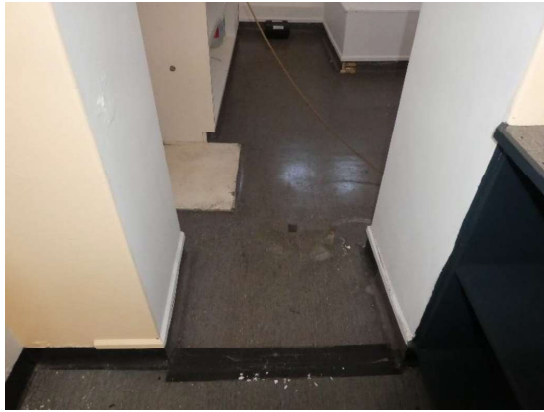
Photograph 137 - Showing the floor to the former basement administration office.



Photograph 138 – Showing vinyl flooring to the bar and kitchen area.

8.20 The bar area has been decommissioned although the cabinetry and countertops remain. Some further investigation, repair and possible levelling will be required to the timber floors to the entrance and the bar areas.

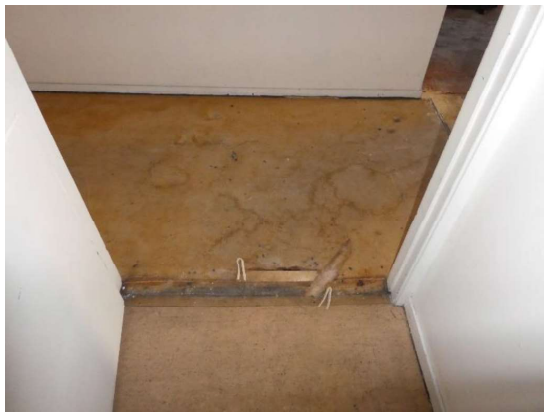
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Photograph 139 - Showing vinyl flooring to the bar and kitchen area.



Photograph 140 – Showing the vinyl to the basement female toilets.



Photograph 141 - Showing the vinyl to the basement corridor and threshold to the male toilets.



Photograph 142 – Showing the piecemeal repairs to the basement office floor noting concrete, timber and custom wood.



Photograph 143 - Showing the floorboards partially removed to the Dressing Room 2 behind the studio theatre within the basement.

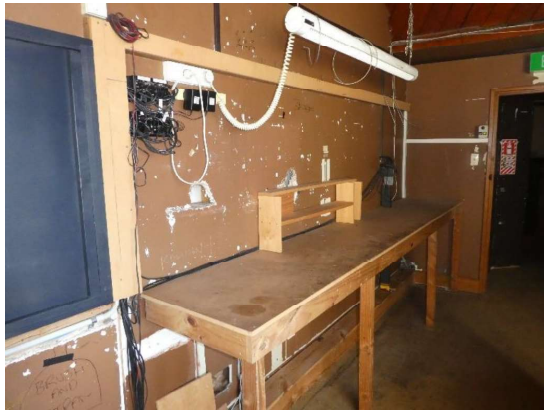


Photograph 144 – Showing the patched flooring / underfloor access outside of the Green Room.

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**Internal walls and partitions**

- 8.21 The interior walls and internal partitions are typically believed to comprise painted plaster over brick or stone and painted fibrous plaster, plasterboard, and hardboard over timber framing.
- 8.22 The alterations to the toilets utilised concrete masonry blockwork walls to some areas and fibre cement lining panels with uPVC jointers to the toilets which appear in reasonable to average condition.
- 8.23 The upper theatre technical room has fibrous plaster walls which are in average to poor condition.



Photograph 145 - Showing the damaged fibrous plaster wall and benching to the meeting / theatre technical room.



Photograph 146 - Showing the damaged fibrous plaster wall and benching to the meeting / theatre technical room.

- 8.24 There is paint failure, efflorescence, and defective paint and plaster to the inside of the external brick walls within the north western stairwell.



Photograph 147 - Showing typical hairline cracking to the plaster on the external walls.



Photograph 148 - Showing original painted timber panelling to the basement area.

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- 8.25 The interior of the exterior basement walls is in extremely poor condition, the majority of solid plaster now being removed to allow these stone and brick walls to breath.
- 8.26 The remaining plaster is aged and typically in average to poor condition with cracking, efflorescence and deterioration including mould growth to some areas.
- 8.27 Further inspection and specialist advice is required, with all defective plaster being removed.
- 8.28 Suitable robust design and remedial works are required to be carried out in accordance with current best practice prior to replastering/relining/redecoration and new furnishings being installed.



Photograph 149 – Showing the vertical cracking to the plaster wall within the north western staircase.



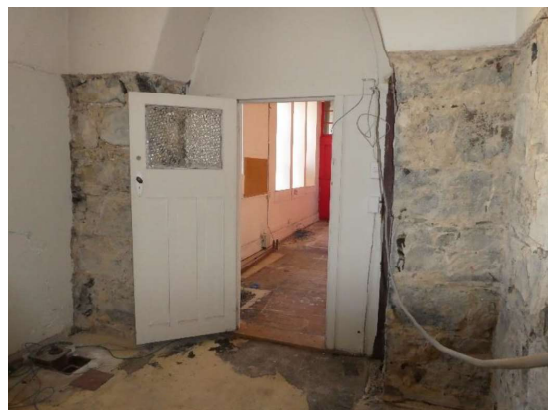
Photograph 150 - Showing the defective plaster to the basement walls within the marketing office.



Photograph 151 - Showing the poor condition to the walls and opening within the studio theatre.



Photograph 152 - Showing the plaster removed from the basement administration office.



Photograph 153 - Showing the plaster removed from the basement administration office.

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Photograph 154 - Showing the defective plasterwork above the timber lancet windows to the south eastern stair.



Photograph 155 – Showing the defective plasterwork to the sill of the timber lancet windows to the south eastern stair.

- 8.29 The bell tower is currently temporarily cordoned off with 'Keep Clear' hazard warning tape installed to the opening, albeit poorly.
- 8.30 Internally the plastered walls appeared to remain reasonable however, the brickwork arch has is exposed where the door frame has been removed. the turret is blocked off at the upper floor level and a smoke detector has been installed.
- 8.31 The windows to the turret have been blacked out and it is recommended that these windows are further inspected, suitably repaired where required and have the glazing paint carefully removed to allow for natural daylight.



Photograph 156 - Showing the aged vinyl floor covering within the bell tower.

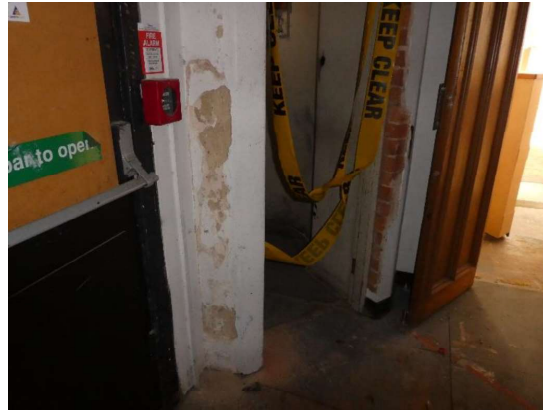


Photograph 157 – Showing the ceiling and sprinkler installed within the bell tower.

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Photograph 158 - Showing the exposed brick arch to the bell tower.



Photograph 159 – Showing the defective paint and plaster adjacent to the bell tower.



Photograph 160 - Showing the bell tower / turret from within the basement area.



Photograph 161 – Showing the ceiling / timber floor installed within the bell tower looking up from the basement area.

8.32 The main entrance foyer comprises timber panel and batten (wainscoting) walls at low level and painted plasterboard which appear in reasonable condition. There were some areas where the panelling has warped which may be as a result of moisture and these areas would need to be removed with further investigation to ascertain if there is any deterioration and decay.

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Photograph 162 - Showing the painted T&G panelling to the south western stairwell.



Photograph 163 – Showing the painted T&G panelling to the south western stairwell.



Photograph 164 - Showing the typical condition of the basement walls below the south eastern stairs.



Photograph 165 – Showing the typical condition of the basement walls below the south eastern stairs.

**Internal Joinery**

- 8.33 Skirtings, cornices, architraves, doors, and door frames all appeared in average to poor condition and decorative order.
- 8.34 The stone tracery and mullions require careful cleaning, preparation, and re-painting. Further specialist advice is required as to the paint specification and also if the paint is to be removed and the original stonework exposed which may require sealing.

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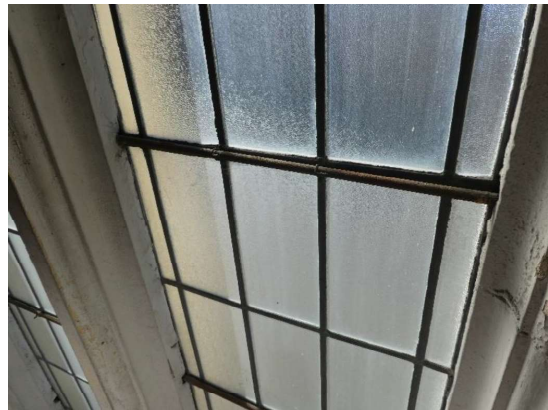
Photograph 166 - Showing the stained glass to the eastern tracery window within the meeting / theatre technical room.



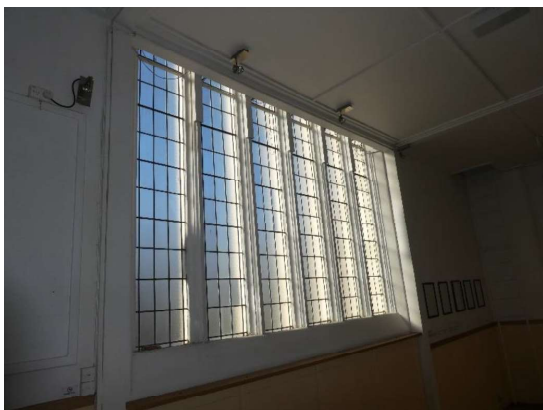
Photograph 167 – Showing the horizontal steel reinforcing / strengthening bar to the inside of the mullioned window. Noting cracking to the stone surround.



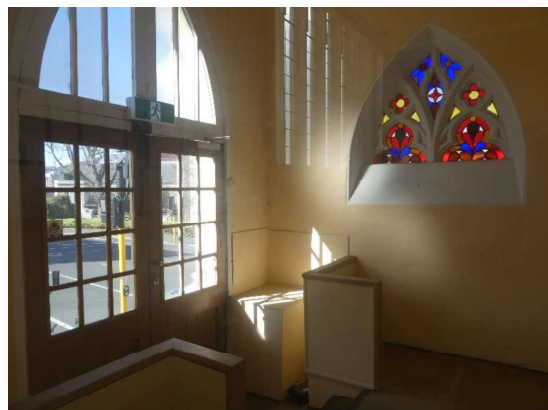
Photograph 168 - Showing the corroded horizontal steel reinforcing and strengthening bars to the inside of the mullioned window.



Photograph 169 – Showing the corroded reinforcing bars to the inside of the mullioned window. Noting paint failure to the stone mullions.



Photograph 170 - Showing the ledged windows and stone mullions at the base of the stained glass window to the east within the entrance foyer.



Photograph 171 – Showing the south eastern entrance and stained glass to the small lancet tracery window.

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Photograph 172 - Showing the base of the leaded windows to the south western corner elevations from within the kitchen.



Photograph 173 – Showing the base of the leaded windows to the south western corner elevations from within the kitchen.



Photograph 174 - Showing inappropriate glazing repair and also noting cracking to glass panels.



Photograph 175 – Showing inappropriate and unsealed copper pipe and electrical cable penetration through the glazing from the hot water cylinder.

- 8.35 The internal plaster around the upper steel leaded lancet windows were observed to be moisture damaged with cracking and efflorescence manifesting to the interior. This will likely be in part to the aged and poorly sealed windows and the inherent issues with solid walls in particular around window penetrations.
- 8.36 The windows have typically been blacked out when the building was in use as a theatre. It is recommended that these windows have the glazing paint carefully removed. To allow natural daylight back into these areas.
- 8.37 The steel frames will require further investigation for signs of excessive rust and corrosion once paint has been carefully removed. Any broken panes will require replacement and opening sashes and furniture will need to be eased, adjusted, and overhauled.
- 8.38 The internal plaster around the southern steel framed windows were observed to be moisture damaged with cracking and efflorescence manifesting to the interior and plaster dust to the floor. This will likely be in part to the aged and poorly sealed windows and the inherent issues with solid walls in particular around window penetrations. Repairs will be required to the windows once they have been repaired and suitably sealed.

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Photograph 176 - Showing the defective plasterwork to the steel ledged lancet windows at the stair landing on the eastern elevation.



Photograph 177 – Showing the defective plaster and efflorescence to the jamb and sill of the steel ledged lancet windows at the stair landing on the eastern elevation.



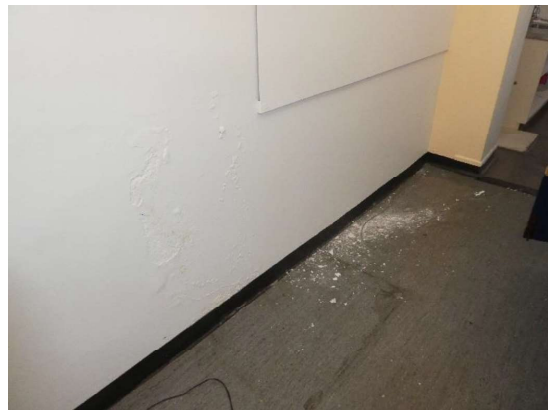
Photograph 178 - Showing the defective cracked plasterwork to the steel ledged lancet windows at the stair landing on the western elevation.



Photograph 179 – Showing the defective cracked plasterwork to the steel ledged lancet windows at the stair landing on the western elevation.

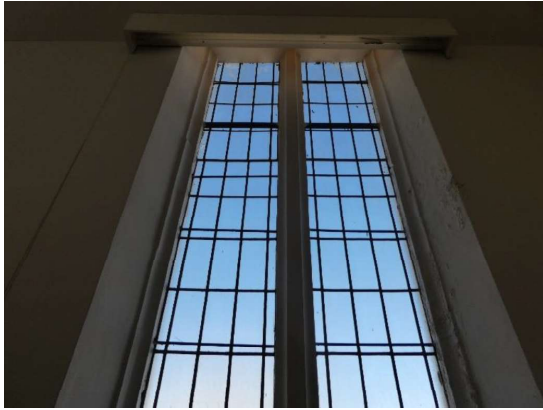


Photograph 180 – Showing the defective paint and plaster to the window sill on the southern wall.



Photograph 181 – Showing typical plaster dust to the vinyl flooring in the bar area.

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Photograph 182 - Showing the leaded windows and stone mullion of the stained glass window to the south within the bar area.



Photograph 183 – Showing the southern stained glass to the lancet tracery window on the southern elevation above the seating area of the theatre.



Photograph 184 – Showing the defective paint and plaster to the window in the south western toilets.



Photograph 185 – Showing the defective paint and plaster to the window in the south western toilets.



Photograph 186 - Showing the painted windows within the south western stairwell. Noting missing pane.



Photograph 187 – Showing the painted windows within the south western stairwell.

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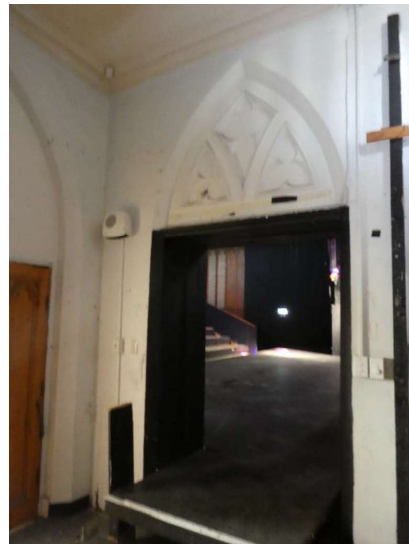
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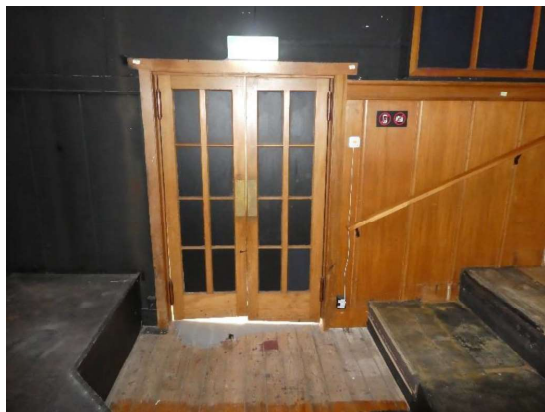
- 8.39 The timber entrance doors to the north western vestibule are aged and in average condition and ill-fitting with large gaps to the lock stiles and jambs and would benefit from replacement.
- 8.40 The majority of the original timber panelling has been painted black and would require closer inspection.



Photograph 188 - Showing the north western entrance door and the open internal panel door to the north western vestibule.



Photograph 189 – Showing the double entry door to stage left from the north western vestibule.



Photograph 190 - Showing the double opening hinged doors from the entrance foyer to the main theatre.



Photograph 191 – Showing the damaged steps leading to the main stage from stage left.

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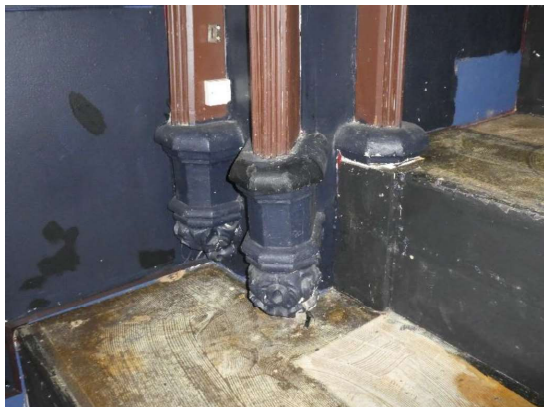


Photograph 192 - Showing the poor condition of the timber lancet top-light window to the south eastern entrance. Noting the maintained fire exit signage.



Photograph 193 – Showing the condition of the timber infill panel above the internal doorway to the basement foyer. Noting efflorescence to the solid walls

- 8.41 There are areas of paint failure and efflorescence and moisture ingress noted to the plaster internally, especially along the northern and western basement walls. Efflorescence or "mineral salts" is a whitish crystalline or powdery deposit on damp masonry walls.
- 8.42 Efflorescence is left behind as moisture comes through the wall and evaporates into the building interior. Efflorescence is not mould, though it is an indicator of wet conditions that have likely contributed to the other unidentified mould growth observed during the inspection.
- 8.43 Unless the cause of the moisture ingress is addressed then walls, plaster finishes and decorations will continue to deteriorate.
- 8.44 The north western staircase provides access from the north western (stage left) entrance vestibule to the upper floor meeting / theatre technical room. The upper stairs are constructed with custom wood with a painted stringer attached to the wall. The custom wood stairs have no coverings installed and are in average to poor condition and decorative order. The stained timber handrail, balustrades and newel posts remain reasonable.



Photograph 194 - Showing the typical timber moulding at the base of the columns within the main theatre.



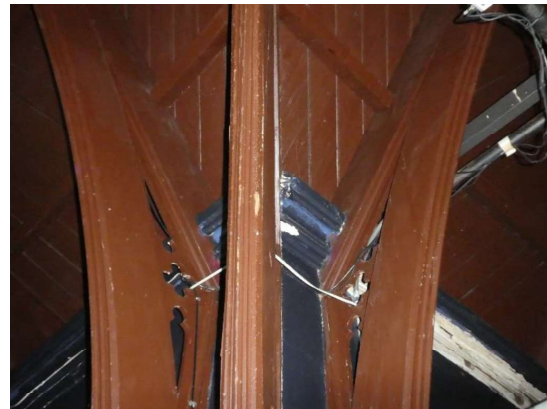
Photograph 195 – Showing the typical timber moulding at the base of the columns within the main theatre.

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**Stairs**

8.45 The lower stair case to the north western stairwell has been removed and this area is in poor condition with the plaster and other linings having been removed which will need to be replaced once suitable damp proofing works have been considered and installed.

8.46 The remaining timber balustrading and handrails to the north western stair generally appear to be in reasonable condition, however would benefit from a varnish recoat.



Photograph 196 – Showing the typical timber detailing at the base of the timber trusses at the columns within the main theatre.

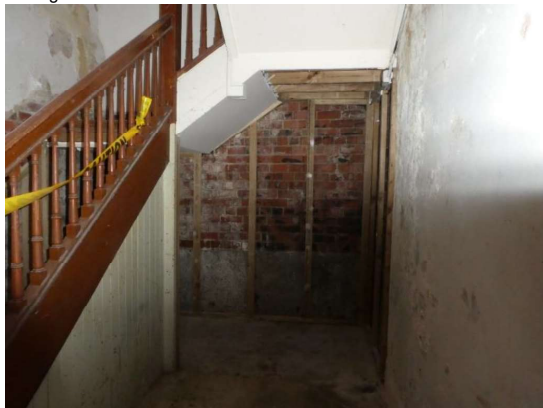
8.47 The timber balustrade to the upper stairs remained secure at the time of inspection, with no obvious movement or deterioration although would benefit from re-varnishing. Regular checks should be made to establish that stair treads are properly maintained in a firm condition and that stair coverings remain secure and safe.



Photograph 197 - Showing the basement floor to the former project managers office / stairs at the north western stairwell.



Photograph 198 – Showing the basement floor to the stairs at the north western stairwell.



Photograph 199 - Showing the linings removed from the basement floor to the former project managers office at the north western stairwell.



Photograph 200 – Showing the linings removed from the basement floor to the former project managers office at the north western stairwell.

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Photograph 201 - Showing the linings removed from the basement floor to the former project managers office / stairs at the north western stairwell.



Photograph 202 – Showing the temporary propping within the north western stairwell. Noting unidentified blackspot mould to the plaster walls



Photograph 203 - Showing the damage and staining to the ceiling linings within the north western stairwell.



Photograph 204 – Showing the entrance doors leading to the Green Room from the north western stairwell.



Photograph 205 - Showing defective flooring and low level plaster within the north western stairwell.



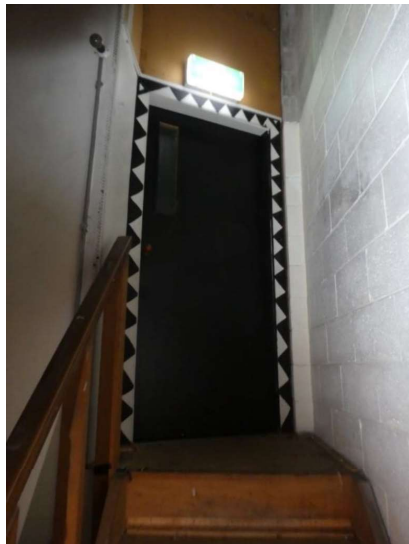
Photograph 206 – Showing the defective plaster within the north western stairwell.

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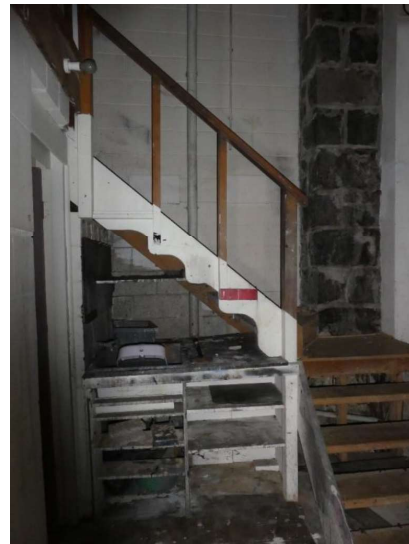
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- 8.48 There is an open tread timber stair case constructed to the south western stairwell which provides access from the main stage right to the basement staff toilets and accommodation to the rear of the studio theatre. The carpets have been removed.
- 8.49 This stairwell was installed prior to the introduction of the Building Act and is considered to be a service or minor private stairway and lighting is considered to be inadequate. The treads span between a timber wall stringer and outer stringer. An opening timber fire exit door is installed at the top of the stairwell. The handrail and balustrade to the stairs are timber and were generally secure. However, there are large gaps between the balustrades which do not accord with current best practice or the acceptable solution in the building code.
- 8.50 There are reservations about the safety of open-tread stairs, and they are not generally recommended. It is therefore recommended that dependent on the future usage of the building that if this stairwell is to remain, it is brought in line with current standards to reduce the risk of accidents from falling.



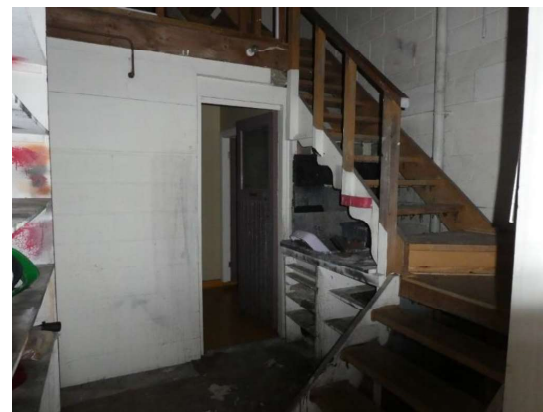
Photograph 207 - Showing the fire exit door and landing with maintained emergency lighting to the south western stairwell from stage right.



Photograph 208 – Showing the open tread stairs to the south western stairwell.



Photograph 209 - Showing the open tread stairs to the south western stairwell.

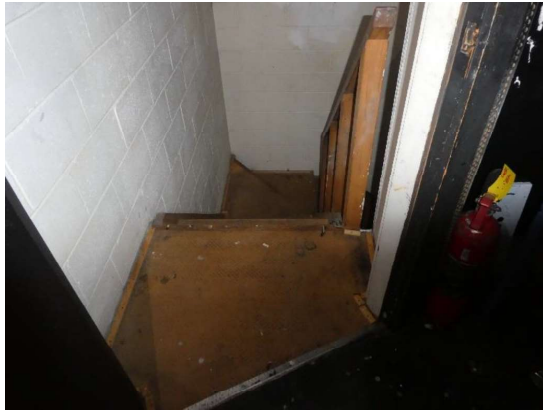


Photograph 210 – Showing the open tread stairs to the south western stairwell.

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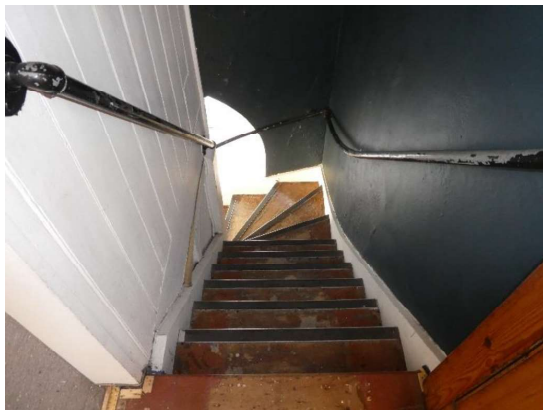
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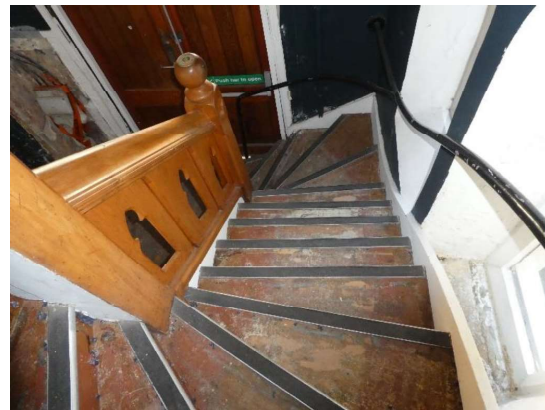
Photograph 211 - Showing the timber stairs to the south western stairwell.



Photograph 212 – Showing the timber stairs to the south western stairwell.



Photograph 213 - Showing the timber stairs to the south eastern stairwell.



Photograph 214 – Showing the timber stairs to the south eastern stairwell.

**Kitchen – Basement**

8.51 Decor appears in average to poor condition to the floors, walls, and ceilings. This space generally contains the following fixtures and fittings:

- The whiteware to the kitchen has been removed with plinths remaining in place.
- The plumbing and drainage remain in place
- Stainless steel and Formica bench tops with inset sinks and mixer tap over
- Various base cupboard and base and wall mounted shelving units.

8.52 Mechanical extract ventilation is installed to the kitchen by means of a window mounted extract fan.

8.53 Dependent on the proposed future usage of the building, it is recommended that the kitchen is re-designed and re-refurbished in accordance with the current Food Act and Food Regulation Requirements.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 215 - Showing the kitchen within the basement area.



Photograph 216 – Showing the kitchen within the basement area.



Photograph 217 - Showing the kitchen within the basement area.



Photograph 218 – Showing the kitchen within the basement area.

**Staff Actors / Male Toilet – Basement Floor**

8.54 The original yard area to the south west was covered in the 1960's with the construction of a new toilet block.

8.55 Decor appears reasonable to average to the floors, walls, and ceilings. These spaces generally contain the following fixtures and fittings:

- Toilet cubicle with ceramic WC's, single flush low-level cistern, plastic seats, and flap and toilet roll dispenser
- Stainless steel urinal and high level cistern
- A corner ceramic wash hand basin with hot and cold chrome taps
- Wall mounted soap and paper towel dispenser.



Photograph 219 – Showing the corner wash hand basin in the male toilet.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 220 – Showing the stainless steel urinal to the male toilet. Photograph 221 – Showing the cubicle to the male toilet.

**Staff Actress / Female Toilet & Shower – Basement Floor**

8.56 Decor appears reasonable to average to the floors, walls, and ceilings. These spaces generally contain the following fixtures and fittings:

- 2 No. toilet cubicles with ceramic WC's, single flush low-level cistern, plastic seats, and flap and toilet roll dispenser
- Stainless steel shower trays, shower rose, and mixer tap with Seratone linings
- A vanity unit with stainless steel wash hand basin with chrome mixer tap and wall mounted mirror above.



Photograph 223 – Showing the stainless steel shower tray and Seratone linings.

Photograph 222 – Showing the lobby to the shower and the female toilets.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 224 – Showing the stainless steel shower tray and Seratone linings.



Photograph 225 – Showing the wash hand basin in the male toilet.



Photograph 226 – Showing the cubicle to the female toilet.



Photograph 227 – Showing the cubicle to the female toilet.

**Public Male Toilet – Ground Floor**

8.57 Decor appears reasonable to average to the floors, walls, and ceilings. These spaces generally contain the following fixtures and fittings:

- Toilet cubicles with ceramic WC, single flush low-level cistern, plastic seat, and flap and toilet roll dispenser
- 2 No. wash hand basins with hot and cold taps and wall mounted mirror above
- Wall mounted paper towel and hand soap dispensers.

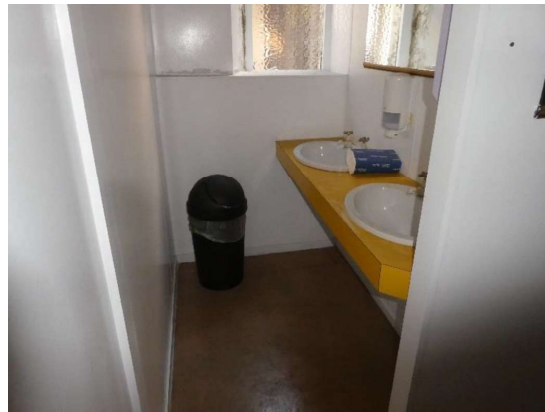


Photograph 228 – Showing the cubicle and hand rail within the male toilet.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 229 - Showing the urinal to the male toilet.



Photograph 230 – Showing the wash hand basins to the male toilet.

**Public Female Toilet – Ground Floor**

8.58 Decor appears reasonable to average to the floors, walls, and ceilings. These spaces generally contain the following fixtures and fittings:

- 2 No. toilet cubicles with ceramic WC's, single flush low-level cistern, plastic seats, and flap and toilet roll dispenser
- 2 No. wash hand basins with hot and cold taps and wall mounted mirror above
- Wall mounted paper towel and hand soap dispensers.



Photograph 231 - Showing the cubicles to the female toilet.



Photograph 232 – Showing the wash hand basins to the female toilet.

8.59 The sanitary fittings to both male and female toilets are considered to be aged and would benefit from upgrading with improvements to the ventilation systems.

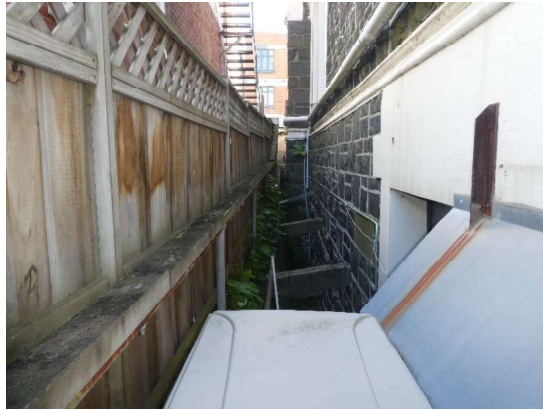
**9.0 GROUNDS & SERVICE AREAS**

9.1 The site is a rectangular shape which occupies a corner section and slopes from west to east. The main entrance is from Stuart Street with basement access from Moray Place to the south east. There is no allocated parking associated with the building.

9.2 The boundaries are typically defined by the masonry walls, with a retaining wall and timber fence to the neighbouring property to the western boundary.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

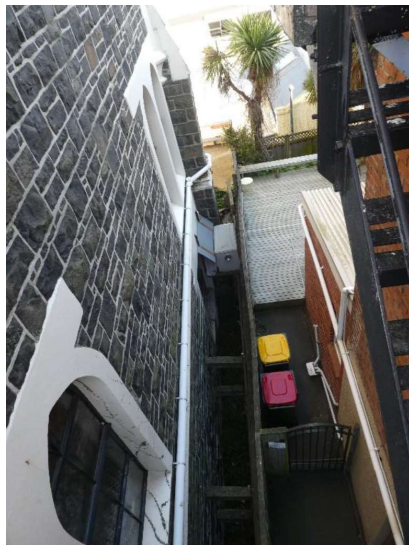
- 9.3 Ownership of the boundaries should be ascertained by your legal adviser in order that you may be aware of your liabilities in this respect.
- 9.4 The lower stonework on the western elevation is currently utilised as a buttress to the western neighbouring retaining wall with concrete braces installed. There was no information or calculations provided in this regard and this requires further inspection and comment from a structural engineer.



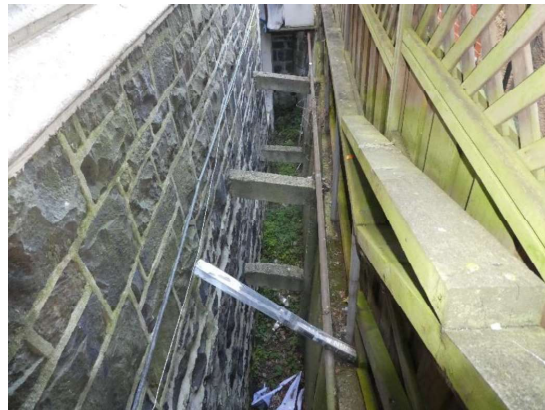
Photograph 233 - Showing the concrete braces buttressing the retaining wall to the western elevation.



Photograph 234 – Showing the concrete retaining wall to the south western elevation.



Photograph 235 - Showing the concrete braces buttressing the retaining wall to the western elevation.



Photograph 236 – Showing the concrete braces buttressing the retaining wall to the western elevation.

- 9.5 The metal flag signage brackets secured to the buttresses appeared to remain in reasonable condition and secure however, some red rust corrosion is apparent which will require monitoring and would benefit from the paint being suitably stripped back, with corrosion treated prior to re-decoration.
- 9.6 The wrought iron metalwork around the blocked basement windows / lightwells to the north elevation remain in reasonable condition however, some red rust corrosion is apparent which will require monitoring and would benefit from the paint being suitably stripped back, with corrosion treated prior to re-decoration.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 237 - Showing the iron railings and blocked up basement windows to the north elevation.



Photograph 238 – Showing the iron railings and blocked up basement windows to the north elevation.

9.7 At the junction of the wall with the adjacent cafe, there is an area where an original window has been bricked up and the glazing has been broken. It is recommended glazing is removed and suitable and sympathetic remedial works are undertaken to make this area safe and weathertight.

9.8 The courtyard located to the southern elevation between the cafe and the Fortune Theatre exhibits significant deterioration and is cluttered with debris.

9.9 It is recommended that comprehensive cleaning and tidying of this space and the lower western area is undertaken.



Photograph 239 – Showing the vegetation growth to the southern courtyard area.

**10.0 SERVICES**

10.1 A detailed survey of the building services is beyond the scope of this report and comments are made based purely on a visual inspection. No testing of these systems was undertaken to confirm operation or performance.

10.2 Building Warrant of Fitness (BWF) WOF-300237 is recorded for the building, issued on the 20<sup>th</sup> February 2023, which expires on 8<sup>th</sup> March 2024. The maximum number of occupants that can safely use the buildings is 250.

10.3 The Compliance Schedule lists specified systems and features within the building. These systems and features ensure that a building is safe and healthy for members of the public to enter, occupy or work in and are noted as follows:

SS2	- Emergency Warning Systems	SS14/2	- Signs for SS 1 –13
SS4	- Emergency Lighting Systems	SS15/2	- Final Exits
		SS15/4	- Signs for Evacuation

10.4 The building owners are responsible to ensure that maintenance is carried out and that “IQPs” or Independent Qualified Persons do the work.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

10.5 A Form 12A is the form issued by an IQP to verify that the inspection, maintenance, and reporting procedures on the compliance schedule for a specified system have been carried out during the previous 12 months. The building owner is then responsible to issue the Building Warrant of Fitness and copies to Council.

**Fire and Security Systems**

10.6 The fire report was not reviewed as part of this inspection. The Fire alarm panels are located within the south eastern vestibule and displayed in the western window.



Photograph 240 – Showing the fire alarm panel on the eastern elevation.



Photograph 241 – Showing the rear of the fire alarm panel within the basement.

10.7 It is likely that this will require review and upgrades dependant on the future proposed usage of the building.

10.8 The fire safety systems currently incorporate automatic smoke / heat detection, fire hose reels and manual emergency break glass units.

10.9 Escape routes and exit-ways were noted as being unobstructed and not used for storage, with a mixture of maintained and non-maintained fire exit signage installed. However, the exterior door to the south eastern vestibule is noted as being partially blocked by the staircase.

10.10 A vertical steel access / escape ladder is provided from the control room above.



Photograph 242 – Showing the vertical fire escape ladder from the meeting / theatre control room to the entrance foyer.

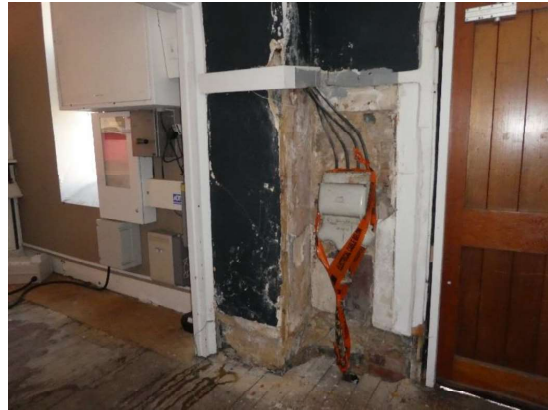
**Electricity**

10.11 Mains electricity is supplied from the national electricity grid via underground cables for power and lighting. Three-phase power is provided which supplies the Main Switchboard (MSB) housed in the basement entrance foyer / vestibule. Power is typically distributed above ceilings, surface mounted conduit and within partitions.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 243 – Showing the main switchboard to the south eastern basement foyer.



Photograph 244 – Showing the old electric meter.

10.12 The property appeared to be earthed with a stake noted on the eastern elevation. There was no obvious equipotential bonding to the metal pipes which would need to be checked by a suitably qualified person.



Photograph 245 – Showing the earth stake to the eastern elevation.

10.13 Lighting is typically pendant and surface mounted fittings with some hi bay lighting and spot light fittings to the theatres. There are numerous lighting booms and stage rigging installed for the specific theatre purpose and these will likely need to be removed with areas being made good.

10.14 Electrical services to the meeting / theatre technical and store rooms are haphazard and should be checked by a suitably qualified person. It is likely that these will need to be removed or upgraded dependant on the proposed usage of the building.



Photograph 246 – Showing the sub-distribution board to the bar / kitchen.



Photograph 247 – Showing old redundant VIR cables within the internal walls.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

- 10.15 The sub-distribution board in the bar kitchen is aged and appears to be an older 'Bakelight product with older re-wireable fuses. This should be checked by a suitably qualified person to ensure that older tough rubber sheathed (TRS) or vulcanised Indian rubber (VIR), are not in current usage. Both TRS and VIR was widely used in the 1950's and 1960's in New Zealand and these cables are prone to perishing due to age and heat.
- 10.16 It is recommended that a full electrical report is commissioned and where older cabling is still in usage it is recommended that it is renewed. It should be noted that some Bakelite products contain asbestos.
- 10.17 The external lighting at the property comprises pendant lighting to the porches, along with various spot and up-lights and bracket fixed pole flood lighting. The wiring and connections should be checked for signs of wear, damage, or loose connections and to ensure that all electrical components and fittings remain securely in place and weatherproof.

**Water**

- 10.18 Water is mains-fed, and the toby device is located beneath a manhole to the public footpath at the south eastern corner of the building. This was not tested for operation at the time of inspection.
- 10.19 Water pressures were not tested at the sinks or wash hand basins and will need to be checked to ensure correct operation and that water freely drains away from all fixtures.



Photograph 248 – Showing the water shut off point to the south east.

- 10.20 Hot water is understood to be provided by hot water cylinders located above the bar / kitchen ceiling and within the basement toilet area.
- 10.21 The older cylinder was not seismically restrained. Cylinders generally have a life expectancy in the region of 10-30 years and should be monitored for any signs of deterioration or leakage.
- 10.22 There is an internal mains pressure gas cylinder manufactured by Rheem. This cylinder has a capacity of 170 litres and a manufacture date of February 2002.



Photograph 249 – Showing the old low pressure hot water cylinder housed within the roof space above the bar / kitchen.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 250 – Showing the mains pressure gas hot water cylinder housed within the basement toilets.



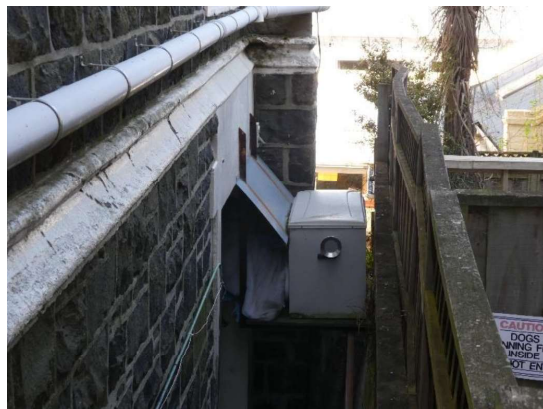
Photograph 251 – Showing the mains pressure gas hot water cylinder housed within the basement toilets.

**Heating, Ventilation and Air Conditioning (HVAC)**

- 10.23 There is a reverse cycle air to air-conditioning / heat pump unit installed to the entrance hall. Further clarification is required as to its operation as the outdoor unit was not located.
- 10.24 There is a ducted heating system to the basement rooms with the exterior unit mounted to the western elevation.
- 10.25 The installation appears makeshift and requires specialist comment as to its condition and suitability.
- 10.26 Mechanical extract ventilation systems are installed to the WC's. These were not tested for operation at the time of inspection.



Photograph 252 – Showing the flexible ducting penetrations to the concrete masonry wall from the south western basement toilets.



Photograph 253 – Showing the exterior unit of the gas ducted heating system to the western elevation.



Photograph 254 – Showing insulated warm air heating duct to the western basement rooms.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 255 – Showing poor installation of the insulated warm air ducting to the western basement rooms.



Photograph 256 – Showing the extract ventilation fan in the basement kitchen.



Photograph 257 – Showing the insulated warm air ducting below the seating in the main theatre.



Photograph 258 – Showing the insulated warm air ducting below the seating with outlet to the bar area.

10.27 There are wall mounted electric fan heaters installed to the stage right and stage left areas. The model numbers should be checked as model No's 3108 and 3109 have a product safety recall and are considered unsafe and should not be used.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**



Photograph 259 – Showing the insulated warm air ducting below the seating in the main theatre.



Photograph 260 – Showing the electric fan heaters adjacent to the main theatre entrances.

**Gas**

10.28 There is a gas bottle locker which can be secured which is located to the southern elevation.



Photograph 261 – Showing the gas bottle storage cupboard to the southern courtyard.



Photograph 262 – Showing the gas pipe penetration through the window.

**Plumbing & Drainage**

10.29 Hot and cold pipework is believed to comprise copper and polybutylene. The pipe work able to be viewed should always monitored to ensure that there is no current leakage.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

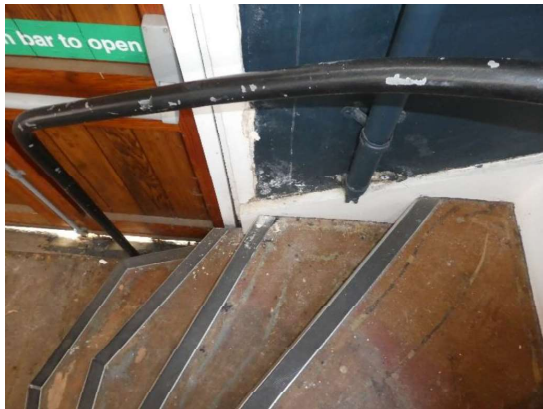
10.30 The original foul water drainage from the yard was believed to connect to the mains drainage on Moray Place to the east.

10.31 Some of the pipework and fixtures including header tank within the south western stairwell appeared to be redundant with one pipe connection observed to be disconnected.

10.32 The plumbing and drainage will require inspection and comment from a suitably qualified person.



Photograph 263 – Showing the foul water pipework in the south western stairwell. Noting disconnected pipework which requires further investigation.



Photograph 264 – Showing the uPVC pipework from the bar / kitchen area above penetrating the exterior wall on the south western corner.



Photograph 265 – Showing the uPVC pipework to external clay gully adjacent to the southern entrance door .

**Telecommunications / Security**

10.33 Telecommunication is believed to be via underground cables. The main alarm system control panel is located in the southern basement entrance with motion sensors strategically fitted to rooms and the sounder and siren is installed at high level on the north eastern buttress.

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

**11.0 SUMMARY & GENERAL MAINTENANCE**

- 11.1 The subject building comprises the former Trinity Methodist Church designed as an example of Gothic Revival Architecture by R. A. Lawson and was opened in 1870.
- 11.2 The building has been vacant for nearly six (6) years since the closure of the Fortune Theatre on 1<sup>st</sup> May 2018. The majority of soft furnishings have been removed and dehumidifiers run twenty-four seven within the basement rooms.
- 11.3 There are a number of areas with deferred maintenance which will need to be addressed to ensure the ongoing integrity of the building and its structure. The basement areas will require extensive works in terms of waterproofing along with design advice and installation of suitable heating and ventilation.
- 11.4 Whilst the waterproofing and weathertightness works will need to be given priority, ultimately the future usage of the building will govern the extent of any internal works which will likely involve extensive remodelling and upgrades in terms of disabled access and fire safety.
- 11.5 This report is based upon the findings of the visual inspection undertaken on Friday 8<sup>th</sup> September 2023. It is recommended that this report along with any maintenance plans for the building are reviewed and updated regularly with updates carried out bi-annually to ensure that maintenance plans remain current and reflect the current condition of the buildings.
- 11.6 Due to the age of the roof covering it should be regularly checked to ensure that all the coverings are in good order. Any cracked, slipped, damaged or deteriorating slates should be renewed, and care taken to ensure that no defects have arisen beneath the damaged areas. Ridge and hip tiles, valleys at the junction between roof slopes, should also be checked and maintained in sound condition. Flashings should be checked to ensure that they are sound and properly dressed in position. Any signs of defects should be rectified preferably with lead flashings.
- 11.7 Check that there are no cracks, splits, or other damage to the flat roof coverings and that the coverings remain properly secured.
- 11.8 Regularly inspect all areas of the external joinery for defects and repair as necessary. Replace all cracked or broken panes of glass and renew loose or missing glazing beads, sealants or putties as required.
- 11.9 Maintain external decorations in good and sound condition and renew on a regular basis every 5-8 years (or sooner if necessary), ensuring that all woodwork, metalwork, and other painted surfaces are properly prepared after maintenance ready to receive the new decorative finishes.
- 11.10 Ensure that all exposed pipework remains properly insulated. Check the hot water cylinders, HVAC equipment and associated valves for any signs of deterioration or leaks.
- 11.11 Ensure that all internal joinery is properly maintained. Ease and adjust doors as necessary and ensure that door furniture is in good condition.

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**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

- 11.12 The servicing, compliance and maintenance records should be kept up to date which will include but are not necessarily limited to the fire protection services, roof and storm water cleaning, HVAC servicing, maintenance and replacement schedules and any relevant hazard registers.
- 11.13 Where defects and other such issues have been identified throughout the body of this report, it is recommended that consideration be given to carrying out remedial works, along with undertaking regular maintenance work to ensure further deterioration does not occur.
- 11.14 In some instances, where defects have been identified, and are not showing significant signs of failure at the time of the inspection, these defects should be monitored regularly so as to ascertain if continued deterioration is taking place.
- 11.15 We are available should further clarification of the issues outlined in this report be required.



Prepared by  
Grant Parker MSc Arch MNZIBS  
DIRECTOR  
REGISTERED BUILDING SURVEYOR  
on behalf of  
**FLANDERS MARLOW LIMITED**

**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

**12.0 REPORT CONDITIONS & DISCLAIMERS**

- 12.1 This document has been prepared solely for the party to whom it is addressed with respect to the particular brief given to us. The advice and/or information contained in it may not be used or relied on in any other context or for any other purpose without our prior written agreement.
- 12.2 The term '*reasonable condition*' is used to indicate a condition relative to the age of the building or item to which reference is being made.
- 12.3 The Consultant will perform a visual inspection of the building specified in the Services section of the Agreement ("the building") and will provide the Client with a report of the inspection. The purpose of the inspection will be to assess the general condition of the building based on a limited visual examination. For residential buildings, the Consultant will not necessarily need to comply with all aspects of NZ Standard NZS 4306 2005 (Residential Property Inspection) in order to meet the performance standard stated in clause 1 of the Engagement Terms.
- 12.4 The Client will be responsible for identifying the building including identifying any accessory units where the building is part of a multi-unit complex. The Client will arrange for the Consultant to obtain reasonable access to the building including roof space and sub-floor space where reasonably and safely accessible. The Client will disclose to the Consultant any known defects which the Client is aware of, apparent or not, and any problem which may affect the integrity and use of the building or the facilitation of the inspection and reporting.
- 12.5 The inspection will be non-invasive and limited to those areas of the building which are readily and safely accessible and visible at the time of inspection. The inspection will not include any areas or items which are concealed behind finished surfaces (such as framing, plumbing, drainage, heating, ventilation or wiring) or any areas requiring the moving of anything which may impede access or limit visibility (such as moving floor coverings, insulation, furniture, appliances, personal property, vehicles, vegetation, debris or soil).
- 12.6 The inspection will focus on identifying significant apparent defects at the time of the inspection. The Client acknowledges and accepts:
- (a) the limited purpose and limited scope of the inspection, and that it may not identify all past, present, or potential future defects;
  - (b) the inspection will not be a compliance assessment against past or current requirements of the Building Code, including the Code's weathertightness requirements or any structural aspects, as this would require specific specialist advice;
  - (c) descriptions in the inspection report of systems or any appliances will relate to existence only and not condition, adequacy or life expectancy;
  - (d) the inspection report will not provide any guarantee or warranty (whether relating to merchantability, fitness for use or fitness for purpose) regarding the building or any item, system or component of the building and will not be relied on as such by the Client.
- 12.7 While the Consultant may use the visible presence of rot, decay or mould to aid in the assessment of the general condition of the building, the Client acknowledges and accepts that the inspection will not be a compliance assessment against the weathertightness requirements of the Building Code.

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**CONDITION REPORT – FORTUNE THEATRE, 231 STUART STREET, DUNEDIN**

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- 12.8 Where borer is identified as existing it is advisable to refer to the additional reference material that is available with the report. The extent of borer infestation should be verified through a recognised pest control company.
- 12.9 In addition to and without limiting anything stated in clauses 12.6 to 12.8 above, the following will be excluded from the scope of the inspection:
- (a) any area of the building or site or any item, system or component not specifically identified in the scope of the Services as needing to be inspected;
  - (b) engineering / structural, architectural, geotechnical, geological, hydrological, land surveying or soils examinations;
  - (c) dismantling of any system, structure or component or any invasive or destructive testing or analysis;
  - (d) systems including electrical, plumbing, air conditioning, heating (including fireplaces and chimneys), security, fire warning and control, sewerage, storm water, ducted vacuum systems;
  - (e) environmental hazards or conditions including the existence of asbestos, electromagnetic radiation, toxic or flammable chemicals, air or water contaminants, geological hazards or floods;
  - (f) sheds, outhouses, detached buildings, swimming pools, spa pools, saunas and associated equipment, or appliances including but not limited to kitchen, leisure and laundry appliances;
  - (g) common property or common areas, systems, structures or components where the building is part of a multi-unit complex unless specifically identified in the scope of the Services as needing to be inspected;
  - (h) acoustical or other nuisance characteristics of any system, service, structure or component of the building or building complex, adjoining properties or neighbourhood;
  - (i) any legal, resource consent or building consent or compliance aspects including title, boundaries, occupational rights, resource and planning consent, building consent, Building Code compliance, building warrant of fitness or heritage obligations.
- 12.10 Any repair recommendations or indicative repair costings included in the inspection report will be for general guidance only. The Client will not rely on such recommendations or indicative costings in making any decision involving legal or financial commitment or repair work but will obtain specific advice from appropriate specialists. The Client accepts the risk that if defects and/or damage are identified, damage may continue to occur, and/or new damage may occur to the building or its systems or components if any recommended repairs are not carried out properly and expeditiously by the Client.
- 12.11 The Client will give prompt written notice to the Consultant of the discovery of any material defect affecting the building not reported by the Consultant which the Client considers should have been identified. Except in an emergency situation, the Client will allow the Consultant 21 days from the Consultant's receipt of that notice to re-inspect the building prior to any repair work being undertaken. If the Client fails to give such notice and/or allow the re-inspection period, any liability of the Consultant in connection with the defect will be reduced (or extinguished) to the extent of any prejudice to the Consultant due to the Client's failure to comply with this clause.



# Land Disposal Investigation Report

## 231 Stuart Street, Dunedin

November 2025

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**Land Disposal Investigation Report**  
**Dunedin City Council**

4 November 2025  
Our Job No. 720535

Dunedin City Council  
50 The Octagon  
**Dunedin 9058**

**The Property Group Limited**  
Christchurch Office  
PO Box 7240 Christchurch 8240  
Level 2, Lion House  
169 Madras Street  
Christchurch 8011

**Attention:** David Arlidge, Property Services

**Land Disposal Investigation Report: 231 Stuart Street, Dunedin**

**Introduction**

Dunedin City Council (**Council**) has engaged The Property Group Limited (**TPG**) to provide property disposal advice pertaining to 231 Stuart Street, Dunedin (**Property**). The Property is more particularly described as Part Section 16 Block XIV Town of Dunedin, measuring 468 square metres more or less and held in Record of Title OT287/25 (limited as to parcels).

Investigations into the acquisition history, nature, and use of the Property will be required to determine Council's statutory obligations and any options for disposal. Neither the request for this report nor the report itself is intended to imply that the Council has declared the Property surplus or has decided to dispose of the Property.

This report is to enable Council to ascertain the extent of its statutory obligations when considering disposal of the Property and is based upon TPG's knowledge and expertise in the field. The arguments presented and conclusions drawn are based upon the working of relevant legislation and caselaw applicable as at the date of this report. This report also investigates additional items that Council should consider relating to the potential disposal of the Property.

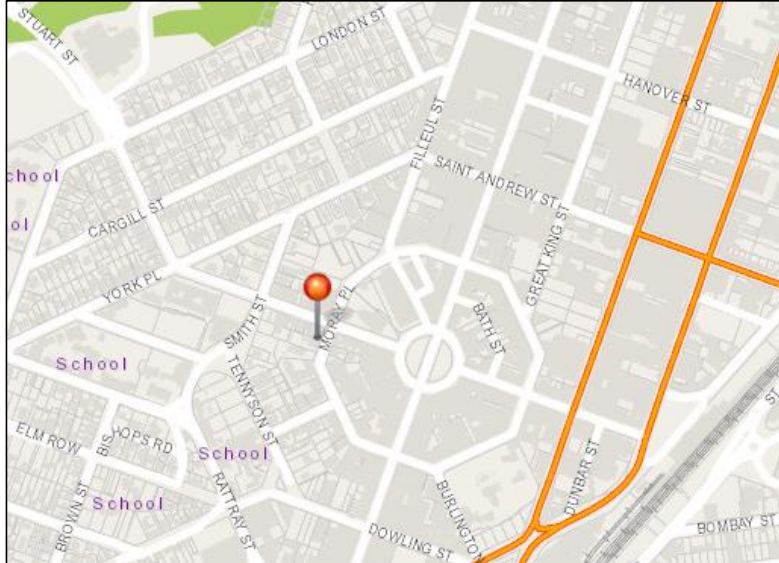
TPG's role in preparing this report is that of an advisor to Dunedin City Council, and TPG is unable to bind Dunedin City Council to any of the conclusions drawn in this report.

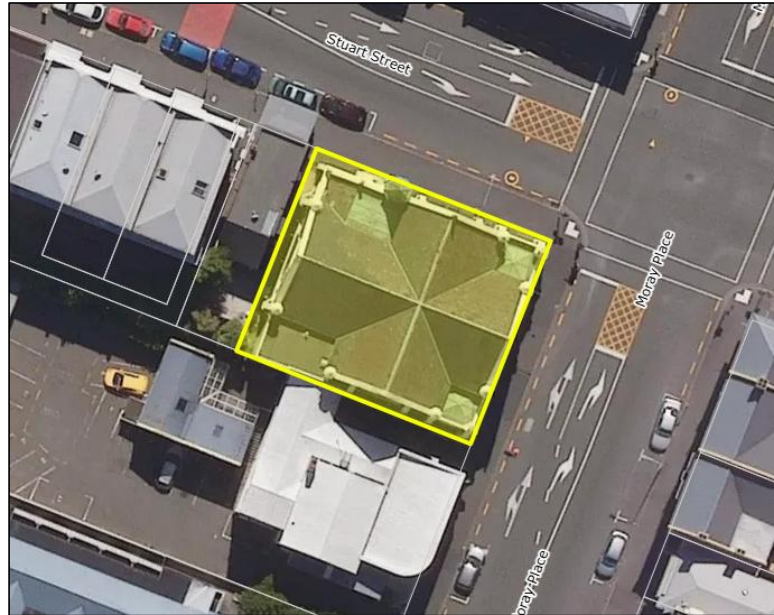
**Executive Summary**

No evidence has been obtained which indicates that the Property was acquired for a public work or is held for a public work. As such, Council is not required to address section 40 of the Public Works Act 1981 (**PWA**) (disposal to former owner) should it wish to dispose of the Property.

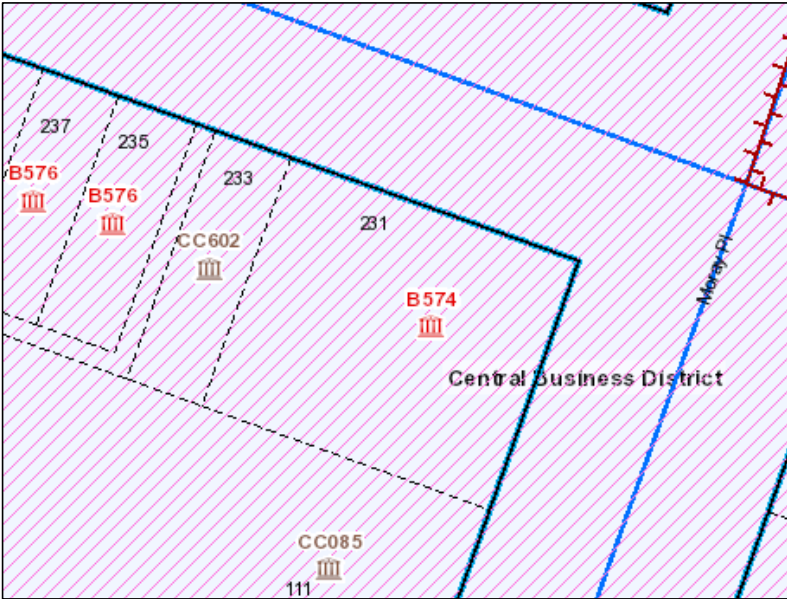
As Council holds the Property in fee simple, there are no legislative restrictions on disposal and Council can dispose of the Property in accordance with its internal policy for the disposal of land.

Further land status details are outlined below.

<b>TPG Job Number</b>	720535
<b>Client</b>	Dunedin City Council
<b>Authority to Act</b>	TPG were instructed on 28 October 2025 by David Alridge, Property Manager in Property Services at Council to investigate Council's disposal obligations.
<b>Location</b>	The Property is centrally located at 231 Stuart Street in Dunedin, on the southwest corner of Stuart Street and Moray Place. 



<b>Legal Description</b>	468 square metres more or less, being Part Section 16 Block XIV Town of Dunedin and comprised in RT OT287/25 (limited as to parcels).
<b>Encumbrances</b>	There are no registered memorials or encumbrances recorded on RT OT287/25.
<b>Status</b>	The Property is held in fee simple by Council.
<b>Mineral Status</b>	<p><b>Non-Statute</b></p> <p>There are no memorials registered on RT OT287/25 which suggest that the non-statute minerals have been removed from the title.</p> <p><b>Statute</b></p> <p>The statute minerals (gold, silver, uranium and petroleum in their existing natural condition) are already held by the Crown pursuant to Section 10 of the Crown Minerals Act 1991.</p>
<b>Contingent Liability</b>	<p>The Property is listed on the New Zealand Heritage List/Rārangi Kōrero as a Historic Place Category 1 by Heritage New Zealand. Therefore, the Property is protected under the Heritage New Zealand Pouhere Taonga Act 2014 (<b>HNZPTA</b>).</p> <p>Section 65 of the HNZPTA defines Category 1 as: <i>'places of special or outstanding historical or cultural heritage significance or value'</i>.</p> <p>Review of the listing on the Heritage New Zealand website identifies the building within the Property to have historic significance and physical significance, due to its quality architecture and landmark value.</p>

	<p>A report to Council in November 2024 recorded that the Property has been vacant for several years and its condition has suffered, with historic water damage to the interior. It is not able to be occupied in its current state, and would require significant investment to resolve these issues. Council has undertaken urgent repairs, where required, and managed the moisture levels with dehumidifiers.</p>
<p><b>Zoning Considerations</b></p>	<p>The Property is located in the Central Business District Zone under the Dunedin City Second Generation District Plan (<b>2GP</b>) and is within the Morary Place-Dowling Street commercial Heritage Precinct. This zoning allows various activities at the Property, including but not limited to commercial, recreational, retail and community use. Furthermore it is listed in the 2GP A1.1 Schedule of Protected Heritage Items and Sites as a Heritage Building, which requires protection of the entire external building envelope.</p> 
<p><b>Māori Interests</b></p>	<p>The Property is located in the Ngāi Tahu Claims Settlement Area. However, the Property is not 'relevant land' within the meaning of the Ngāi Tahu Claims Settlement Act 1998 and is not subject to the Ngāi Tahu Claims Settlement Act 1998. As per section 4 of the Local Government Act 2002, Council should consider consultation with local Rūnanga and or iwi with respect to any potential disposal of the Property if this is part of its internal disposal policy.</p>
<p><b>Physical Description</b></p>	<p>The Property is rectangular in shape and contains a historic Gothic-style building built in 1870 and was originally occupied for worship by the Trinity Methodist Church. Whilst the external structure has largely remained the same, the interior of the building was eventually remodelled in 1977 for use as a theatre. The theatre held two auditoriums able to seat 329 people in total.</p> <p>Other than general maintenance and repairs, no further works have taken place at the Property. The Property has been vacant since 2018.</p>

<b>Valuation</b>	<p>The current rateable value of the Property is;</p> <table border="0"> <tr> <td>Land value</td> <td>\$780,000</td> </tr> <tr> <td>Improvement value</td> <td>\$250,000</td> </tr> <tr> <td>Capital value</td> <td>\$1,030,000</td> </tr> </table> <p>We understand that Council has obtained a valuation report from Colliers.</p>	Land value	\$780,000	Improvement value	\$250,000	Capital value	\$1,030,000
Land value	\$780,000						
Improvement value	\$250,000						
Capital value	\$1,030,000						
<b>Survey and Title Requirements</b>	<p>The Property is held by Council as a standalone property in its own RT, no further survey or titling actions are required to dispose of the Property.</p>						
<b>Acquisition History</b>	<p><b>Background</b></p> <p>We understand from the documentation provided that the Property was purchased by Council in 2000. The purchase was made following an approach by the former landowner, the Fortune Theatre Trust (<b>Trust</b>). A purchase and leaseback to the Trust was negotiated to ensure the Theatre remained financially viable.</p> <p>Prior to the purchase, the Trust had been experiencing financial difficulties and was largely reliant upon funding and sponsorship to continue its operations.</p> <p>In a report to the Committee of the Whole on 7 February 2000, the Council considered the Trust's request and resolved to purchase the Theatre as a Property Investment with a commercial lease between the parties being entered into, with the rent being set at an 11% rate of return on equity invested. The Council also needed to be satisfied that the Trust could discharge its existing financial liabilities.</p> <p>Following formal Council approval of the proposal, an Agreement for Sale and Purchase was entered into between the Trust and Council for a purchase price of \$222,000 plus GST (if any), together with a commercial Deed of Lease. The purchase settled on 29 September 2000.</p> <p>The lease commenced on 29 September 2000 for an initial term of six years and three further terms of six years each. The annual rent was initially set at \$24,420 plus GST, with rental reviews to be undertaken.</p> <p>In May 2018, the Trust announced the immediate closure of the Theatre's operations as it was no longer financially sustainable. The Trust was wound-up, and the lease was surrendered on 20 July 2018. The Property has remained vacant ever since.</p> <p>The Deed of Lease did include a right of first refusal clause if, at any time during the term of the lease or any renewal thereof, the Council desired to sell the Property, the Council must first offer to sell the Property to the Trust. As the Lease ended several years ago and the Trust no longer exists, this clause is no longer applicable.</p> <p>From review of the evidence, it is clear that the purchase of the Property was initiated by the Trust and was considered a Property Investment, with a commercial lease being entered into simultaneously with the Trust.</p> <p>The Property has been vacant since 2018. In November 2024, the Council resolved to seek feedback on the removal of the Property from Schedule 2 of the Significance and Engagement Policy as part of the 9-year Plan 2025-2034 consultation process and to also seek feedback on a proposal to sell the Property.</p>						

In May 2025, following receipt of feedback, the Council resolved that the Property be removed from Schedule 2 of the Significance and Engagement Policy and that Council request a staff report of the options for the Property, including any options presented by submitters, and a possible sale.

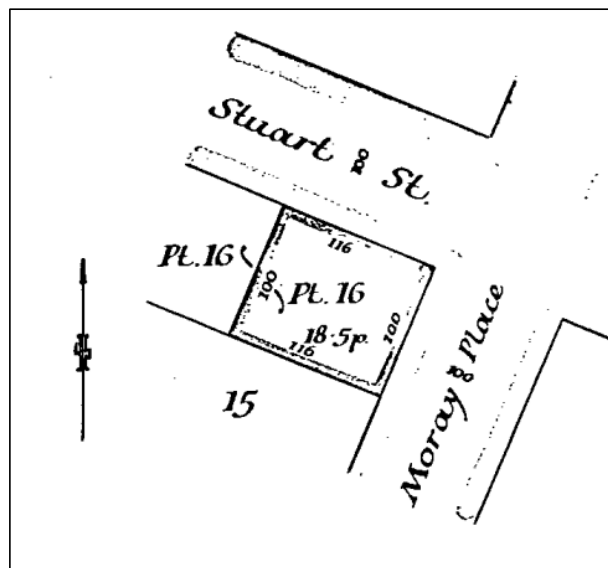
In July 2025, Council resolved to direct staff to prepare the Property for sale, noting that staff would provide a further report to Council and Council would not make a further decision regarding the potential sale until it had received the further report.

All documentation that we have viewed in preparing this report indicates that the acquisition of the Property was not in any way intended to be for a public work. We consider that the Property is not held for a public work and is thus not subject to the offer back provisions of section 40 Public Works Act 1981 (**PWA**). This is discussed further in the statutory consideration sections below.

### Chronology

The following section details a brief chronological acquisition history of the Property.

**January 1938** RT OT287/25 issued on 31 January 1938 to trustees under the Methodist Model Deed of New Zealand 1887 for part Section 16 Block XIV Town of Dunedin, measuring 18.5 perches more or less.



**April 1980** RT OT287/25 transferred to The Fortune Theatre Trust on 21 April 1980.

**October 2000** RT OT287/25 transferred to Dunedin City Council on 2 October 2000 after Council was approached by The Fortune Theatre Trust, regarding the purchase. The Property has remained in Council's ownership since this date.

**Statutory Interpretation**

**Public Works Act 1981**

In summary, section 40 PWA requires that any land acquired or held for a public work that is now surplus to the acquiring authority's requirements (in this case, Council) be offered-back to the former landowner, unless one of the exemptions apply.

For section 40 to apply, the land must have been acquired or held for a public work.

Section 2 of the PWA defines '**public work**' and '**work**' for the purposes of the PWA to mean:

(a) *every Government work or local work that the Crown or any local authority is authorised to construct, undertake, establish, manage, operate, or maintain, and every use of land for any Government work or local work which the Crown or any local authority is authorised to construct, undertake, establish, manage, operate, or maintain by or under this or any other Act; and include anything required directly or indirectly for any such Government work or local work or use:*

[subsections (b) and (c) omitted as considered irrelevant to this report]

Council records show the Property was acquired when the former owner, the Fortune Theatre Trust, approached Council to request that the Council purchase the Property and lease it back to the Trust so that the Theatre could remain financially viable. As discussed in the Acquisition History section of this report, this purchase was not for a public work of any kind and was rather considered to be an investment into the Property, as well as a commercial arrangement with the Trust.

All documentation that we have viewed in preparing this report indicates that the acquisition of the Property was not in any way intended to be for a public work. We consider that the Property was not acquired nor held for a public work and is thus not subject to the offer back provisions of section 40 PWA.

Having investigated the acquisition history and status of the land we are of the opinion that Council is able to dispose of the Property in accordance with its internal policy for the disposal of fee simple land. We have not identified any underlying legislation pertaining to the land.

**Local Government Act 2002 (LGA)**

Prior to disposal of the Property the Council must consider whether section 138 of the LGA is applicable to the Property, which states:

1. *A local authority proposing to sell or otherwise dispose of a park or part of a park must consult on the proposal before it sells or disposes of, or agrees to sell or dispose of, the park or part of the park.*

2. *In this section,—*

**dispose of**, *in relation to a park, includes the granting of a lease for more than 6 months that has the effect of excluding or substantially interfering with the public's access to the park*

**park—**

(a) *means land acquired or used principally for community, recreational, environmental, cultural, or spiritual purposes; but*

(b) *does not include land that is held as a reserve, or part of a reserve, under the [Reserves Act 1977](#).*

At present, the Property is vacant and not used by the public for any purpose. However, it has previously operated as a theatre until its sudden closure in 2018.

Notwithstanding this, on the information provided to us (commercial lease documentation for the Property and operation as a theatre, which infers payment for entry) it is evident that the Property has not been used principally for community, recreational, environmental, cultural, or spiritual purposes and on this basis, we consider that section 138 LGA does not apply.

#### **Additional Consideration**

Articles written about the Property purchase and potential disposal indicates a strong level of community support and interest, particularly given the Property's historic significance. Consultation and consideration of community views is also a requirement in sections 78 to 82 of the LGA. Recent Council resolutions indicate that is has been undertaken.

#### **Summary**

Council is able to dispose of the Property in accordance with its internal policy for the disposal of fee simple land.

#### **Conclusion**

We are of the opinion that 231 Stuart Street is not held for and was not originally acquired for a public work. As such, Council is not required to address section 40 PWA should it wish to dispose of the Property.

The property is held by Council in fee simple. There are no legislative restrictions on disposal and Council is able to dispose of the Property in accordance with its internal policy for the disposal of land.



**Amber Underwood**

Property Consultant

Date: 4 November 2025



**Chris Jones**

Principal Consultant

Peer Reviewer

Date: 4 November 2025

**Appendix: Historic Report of Title**



**RECORD OF TITLE  
UNDER LAND TRANSFER ACT 2017  
FREEHOLD  
Limited as to Parcels  
Historical Search Copy**



  
R. W. Muir  
Registrar-General  
of Land

Constituted as a Record of Title pursuant to Sections 7 and 12 of the Land Transfer Act 2017 - 12 November 2018

**Identifier** OT287/25  
**Land Registration District** Otago  
**Date Issued** 31 January 1938  
**Prior References**  
DI L588

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**Estate** Fee Simple  
**Area** 468 square metres more or less  
**Legal Description** Part Section 16 Block XIV Town of  
Dunedin


**Original Registered Owners**  
The Fortune Theatre Trust

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**Interests**  
674782 Mortgage to Bank of New Zealand - 17.3.1987 at 9:02 am  
5009773.1 Discharge of Mortgage 674782 - 2.10.2000 at 9:37 am  
5009773.2 Transfer to Dunedin City Council - 2.10.2000 at 9:37 am

Identifier **OT287/25**

**NEW ZEALAND.**



Register-book, Vol. 207 folio

**LAND & DEEDS**

Book-156  
Nature: *R/L*

Page: *1274*

**29 OCT 1974**

Time: *2:00*

Fees: \$ *3.45*

Abstract No. *410*

Reference: Land Transfer (Compulsory Registration of Titles) Act, 1924.  
Deeds Index. L-588  
Application No. C-11672


**CERTIFICATE OF TITLE UNDER LAND TRANSFER ACT.  
LIMITED AS TO PARCELS AND TITLES.**

This Certificate, dated the thirtyfirst day of January, one thousand nine hundred and thirtyeight, under the hand and seal of the District Land Registrar of the Land Registration District of O T A G O witnesses that

The persons named in the Schedule hereto as trustees under the Methodist Model Deed of New Zealand 1887 are

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as seized of an estate in fee-simple (subject to such reservations, restrictions, encumbrances, liens, and interests as are notified by memorial under written or endorsed hereon, subject also to any existing right of the Crown to take and lay off roads under the provisions of any Act of the General Assembly of New Zealand) in the land hereinafter described, as the same is delineated by the plan hereon bordered green, be the several admeasurements a little more or less, that is to say: All that parcel of land containing Eighteen decimal five (18.5) perches more or less situated in the City of Dunedin being part Section Sixteen (16) Block XIV TOWN OF DUNEDIN.

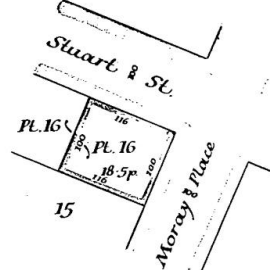


*K.M.W.*

**Asst District Land Registrar.**

EQUIVALENT METRIC AREA IS 4.68 m<sup>2</sup>

Pl. Sec. 16 BLK XIV  
Town of Dunedin



SCHEDULE	
Charles Frederick Roberts	Timber Agent
David Leonard Cumberbeach	Civil Servant
Jefferson Council Stephens	Solicitor
Ernest Alfred John Wilcocks Rosevear	Accountant
Charles Fitchett Oliver	Mercantile
Robert Newton Vesna	Company Manager
William Henry Ferens	Butcher
Thomas Smith	Butcher
John Kay	Butcher
Peter Christie	Draper
Samuel Leam	Draper
William John West	Clothing Inspector
David McPherson Miller	Crocer
James Dey	Driver
Robert Gordon	Storeman
James Chrystal Marton Mallac	Importer
John Scott	Retired
Alfred Hamish Reed	Bookseller
George William Davies	Engineer
Robert Boon Mason	Gas Inspector
Harold Roy Filson	Boat-maker
Vernon Smith	Ironmonger
Percy Herbert Martin	Grain Merchant
William Robert Henderson	Harbour Board Employee
Gordon William Havelock Adams	Asst. Newspaper Publisher
Allen McDougall	Carpenter
Arthur Sunderland	Taxi Proprietor
Charles Ernest Wheatley Lyth	Medical Practitioner
Albert Edward Heyman	Printer
Alexander Wycliffe Reed	Bookseller's Assistant
Douglas Tweedie Skene	Seedman
Tom Charles Sharp	Hospital Porter
Gordon Seywell Gapper	Civil Servant

THIS REPRODUCTION ON A REDUCED SCALE CERTIFIED TO BE A TRUE COPY OF THE ORIGINAL REGISTER FOR THE PURPOSES OF SECTION 215A LAND TRANSFER ACT 1952.

*J. M. ...*

*This Certificate of Title has ceased to be Limited as to Title*

*Extend 29.10.1974*

*J. L. R.*

Identifier **OT287/25**

c.t. 287/25

533257 Transfer to The Fortune Theatre Trust - 21.4.1980 at 1.34 pm

*[Handwritten signature]*  
A.L.R.

533258 Mortgage to The National Insurance Company of New Zealand Ltd - 21.4.1980 at 1.34 pm

**DISCHARGED**  
*[Handwritten signature]*  
A.L.R.

588212 Mortgage to The Dunedin City Council - 21.12.1982 at 12.12 pm

**DISCHARGED**  
*[Handwritten signature]*  
A.L.R.

673672 Mortgage to Bank of New Zealand - 2.3.1987 at 9.32 a.m.

**REJECTED**

A.L.R.

674782 Mortgage to Bank of New Zealand - 17.3.1987 at 9.02am

*[Handwritten signature]*  
A.L.R.



## **FEEDBACK ON DCHL GROUP DRAFT STATEMENTS OF INTENT 2026/27**

Department: Civic

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### **EXECUTIVE SUMMARY**

- 1 The draft 2026/27 Statements of Intent (draft Statements) of the Dunedin City Holdings Limited (DCHL) Group companies were presented to the Finance and Performance Committee at its meeting on 19 March 2026.
- 2 At that meeting, Committee Members were invited to provide feedback on the draft Statements.
- 3 A Councillor workshop was held on 15 April 2026 to discuss feedback received. This report provides a summary of elected member feedback for consideration. Approved feedback will be provided to DCHL for its consideration in preparing the final Statements of Intent.

### **RECOMMENDATIONS**

That the Council:

- a) **Approves** the proposed feedback on the draft Statements of Intent, with any amendment.
- b) **Notes** the approved feedback will be provided to Dunedin City Holdings Limited for its consideration in preparing the final DCHL Group Statements of Intent.

### **BACKGROUND**

- 4 Draft Statements were presented to the Finance and Performance Committee at its meeting on 19 March 2026. Committee Members were invited to provide feedback on the draft Statements.
- 5 Schedule 8 of the Local Government Act 2002 sets out the requirements and time frames for preparing and finalising Statements of Intent. Feedback on draft Statements must be provided to DCHL by 1 May 2026, and final Statements must be delivered to Council before 1 July 2026.

### **DISCUSSION**

- 6 The proposed feedback from elected members on the draft Statements is presented below.
- 7 Initial feedback was informally discussed at a non-public workshop held on 15 April 2026. The Chair and General Manager of DCHL attended the workshop.
- 8 The following feedback has been provided for consideration by Council. Approved feedback will be provided to DCHL for inclusion in the group companies Statements of Intent.

### **Dunedin City Holdings Limited**

- 9 Council requests that DCHL consider a significant increase in the dividend to DCC for the 2026/27 financial year, to help mitigate the proposed increase in rates, in light of the financial pressures

currently facing ratepayers. Council expects DCHL to work collaboratively with Council staff to determine the appropriate quantum of any proposed dividend increase.

- 10 DCHL to consider the diversity of applicants to the Intern Director programme, including diversity of demographics and professional backgrounds, alongside other selection criteria.
- 11 Council additionally requests that steps are taken to ensure both the University of Otago and Otago Polytechnic are aware of the Intern Director programme and are supported to promote it to potential applicants.
- 12 DCHL to include measures within the group companies Statements of Intent to identify, track, and report on co-investment opportunities that are investigated and developed in partnership with mana whenua.
- 13 Council requests greater consistency in the reporting of company sponsorships across the group. Sponsorship activity should be clearly articulated and demonstrate alignment with Council’s strategic framework.
- 14 Council encourages DCHL to support and enable increased collaboration across subsidiary companies. This may include opportunities for shared procurement, shared services, and/or the sharing of information and expertise, with the intent of improving efficiency, reducing duplication, and strengthening group-wide performance.
- 15 In addition to the feedback above, it is noted that Council staff will work with DCHL over the coming months to ensure that the Council’s Te Taki Haruru Māori Strategic Framework can be effectively integrated into group company Statements of Intent from the 2027/28 financial year.

**OPTIONS**

- 16 There are no options.

**NEXT STEPS**

- 17 Feedback approved by Council will be provided to DCHL.
- 18 Council staff will work with DCHL to determine the appropriate quantum of any proposed dividend increase in time for the Council budget meeting on 27 May 2026.
- 19 Final Statements of Intent will be presented to the 25 June 2026 Council meeting.

**Signatories**

Author:	Janet Fraser - Corporate Planner
Authoriser:	Carolyn Allan - Chief Financial Officer

**Attachments**

There are no attachments for this report.

**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision enables democratic local decision making and action by, and on behalf of communities, and promotes the social, economic, environmental, and cultural well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Economic Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Other strategic projects/policies/plans	<input type="checkbox"/>	<input type="checkbox"/>	✓

Draft Statements of Intent take into account Council’s Strategic Framework and Council’s Letter of Expectation to DCHL.

***Māori Impact Statement***

The DCHL Group draft Statements of Intent were first received by the Finance and Performance Committee; membership of the Committee includes mana whenua representation.

***Sustainability***

There are no known implications for sustainability.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

The financial forecasts within the DCHL draft Statement of Intent show annual levels of interest and dividend payable to DCC. Any updates will be reflected in the Annual Plan budget update report, which will be presented at the Council meeting on 27 May 2026.

***Financial considerations***

Financial projections in the draft Statements will be updated prior to their finalisation.

***Significance***

This decision is considered to be low in terms of the Council’s Significance and Engagement Policy.

***Engagement – external***

There has external engagement with the DCHL Group, and the mana whenua representative on the Finance and Performance Committee.

***Engagement - internal***

Feedback provided by Councillors is the subject of this report.

**SUMMARY OF CONSIDERATIONS**

***Risks: Legal / Health and Safety etc.***

There are no identified risks.

***Conflict of Interest***

There are no known conflicts of interest.

***Community Boards***

There are no known implications for Community Boards.

## LOCAL ALCOHOL POLICY - FURTHER CONSULTATION

Department: Civic

### EXECUTIVE SUMMARY

- 1 The purpose of this report is to update Council on the review of the DCC's Local Alcohol Policy (LAP), and to seek Council approval to engage with broader stakeholders and the public on possible LAP content options.
- 2 Council resolved in August 2025 to recommence the review of the LAP, approving a three-stage engagement process to build upon the work carried out during the 2024 review.
- 3 Stage One engagement with key stakeholders has taken place, along with research and information updates including those from whom Council must consult with when developing a draft LAP (Police, Health New Zealand/Te Whatu Ora, and Alcohol Licencing Inspectors).
- 4 A range of possible LAP content options have been identified to engage broader stakeholders and the public on to inform a draft LAP to formally consult on later this year.

### RECOMMENDATIONS

That the Council:

- a) **Approves** further stakeholder and public engagement on a range of possible LAP content options noted in this report.
- b) **Notes** that this engagement will inform the draft LAP to formally consult on later this year.

### BACKGROUND

- 5 The Sale and Supply of Alcohol Act 2012 (the Act) allows territorial authorities to develop local alcohol policies, following the object of the Act:
  - a) the sale, supply, and consumption of alcohol should be undertaken safely and responsibly; and
  - b) the harm caused by the excessive or inappropriate consumption of alcohol should be minimised.
- 6 LAPs are aimed at ensuring the accessibility and availability of alcohol is in line with the object of the Act. Licences can be refused if they are contrary to a LAP and conditions may be imposed on a licence if they are required by a LAP.

*The LAP must only include matters relating to licencing*



*Review of the DCC Local Alcohol Policy*

- 12 A review of the current LAP (adopted in 2019) was started in 2024, including formal consultation and submissions. The Hearings Committee made several recommendations in February 2025. Council resolved in August 2025 to approve them:

Moved (Cr Jim O'Malley/Cr Bill Acklin):

That the Council:

- a) **Approves** the Hearings Committee's recommendations of 27 February 2025 that:
  - i) A revised consultation document be prepared and consulted on.
  - ii) A revised consultation document be prepared and consulted on and that the tri-agencies (Medical Officer of Health, NZ Police and Licensing Inspectors) and other affected parties are actively engaged throughout the process including the preparation of the revised consultation document.
  - iii) Evidence presented to date be considered in the preparation of the revised consultation document.
- b) **Notes** that there would be a Council workshop later this year identifying options for the content of a revised draft Local Alcohol Policy.

**Motion carried (CNL/2025/249)**

- 13 A three-stage engagement process is being undertaken for this review:

- a) Stage One – compiling research under section 78 of the Act, reviewing evidence received during the 2024 review, engaging with key sector stakeholders, holding a Council workshop and identifying issues and possible LAP content options (completed).
- b) Stage Two – what this report is seeking Council approval to carry out: engaging with broader stakeholders and the public on possible LAP content options and analysing feedback to develop a draft LAP.
- c) Stage Three – Council approves consultation on the draft LAP via the Special Consultative Process, including submissions and hearings, for Council to then adopt the reviewed LAP.

## **DISCUSSION**

### **Research**

- 14 Research and information have been gathered in respect of the Act's seven section 78(2) considerations, with a full research report to accompany a report to Council on the formal draft LAP later this year. Selected key examples are as follows.

- a) The objectives and policies of the district plan – the relevant objectives and policies in relation to licensed premises in commercial and mixed-use zones are:

- Dunedin has a well-structured and economically and socially successful range of commercial and mixed-use environments.
  - The potential for conflict between activities within the commercial and mixed-use zones and in adjoining zones is minimised
- b) The number of licences of each kind held for premises in the district, and the location and opening hours of each of the premises:
- On-licences – 242
  - Off-licences – 77
  - Club licences – 79
- c) Any areas in which bylaws prohibiting alcohol in public places are in force
- For Dunedin’s Alcohol (Control of Alcohol in Public Places) Bylaw 2004 (approved for review in March 2026 by Council), the areas of George Street between The Octagon and Albany Street, Princes Street between The Octagon and Jetty Street, and the accompanying streets, lanes, public places, footpaths, carparks and reserves (including The Octagon, Exchange, Queens Gardens, Railway Station and the grounds of First Church).
- d) The demography of the district’s residents:
- 15-24 year olds make up 20.5% of the Dunedin population compared to 12.5% nationally.
- e) The demography of the people who visit the district as tourists or holiday makers:
- Port Otago had 151,609 passengers arriving in the 2024/25 cruise ship season.
  - A 2025 visitor profiles report found that 10% of New Zealanders who had travelled domestically in the last 12 months had visited Dunedin.
- f) The overall health indicators of the district’s residents:
- The NZ Quality of Life Survey in 2024 found that 70% of respondents perceive alcohol and drug problems, or antisocial behaviours caused by alcohol or drugs, to have been an issue in Dunedin in the last 12 months, up from 66% in 2020.
  - The New Zealand Health Survey 2023/24 indicates that the Otago region has high levels of heavy and hazardous drinking, and the highest monthly heavy episodic drinking compared to any other region in New Zealand/Aotearoa.
- g) The nature and severity of the alcohol-related problems arising in the district:
- Between 2012 and 2021 Dunedin had the highest age-standardised rate of hospitalisations wholly attributable to alcohol in the country (160 per 100,000 people).

- North Dunedin comprises 7-8% of the total Dunedin population yet has contributed between 23% and 27% of all alcohol-related ED visits during the 2018-2024 period (Figure One).
- For the 2022-2025 period in Dunedin, Police reported 2,002 drink-drive offences and 3,255 incidents of alcohol-related family harm.
- ACC identified that in 2023 there were 216 claims specifically for alcohol-related harm in Otago; the second highest rate in New Zealand.

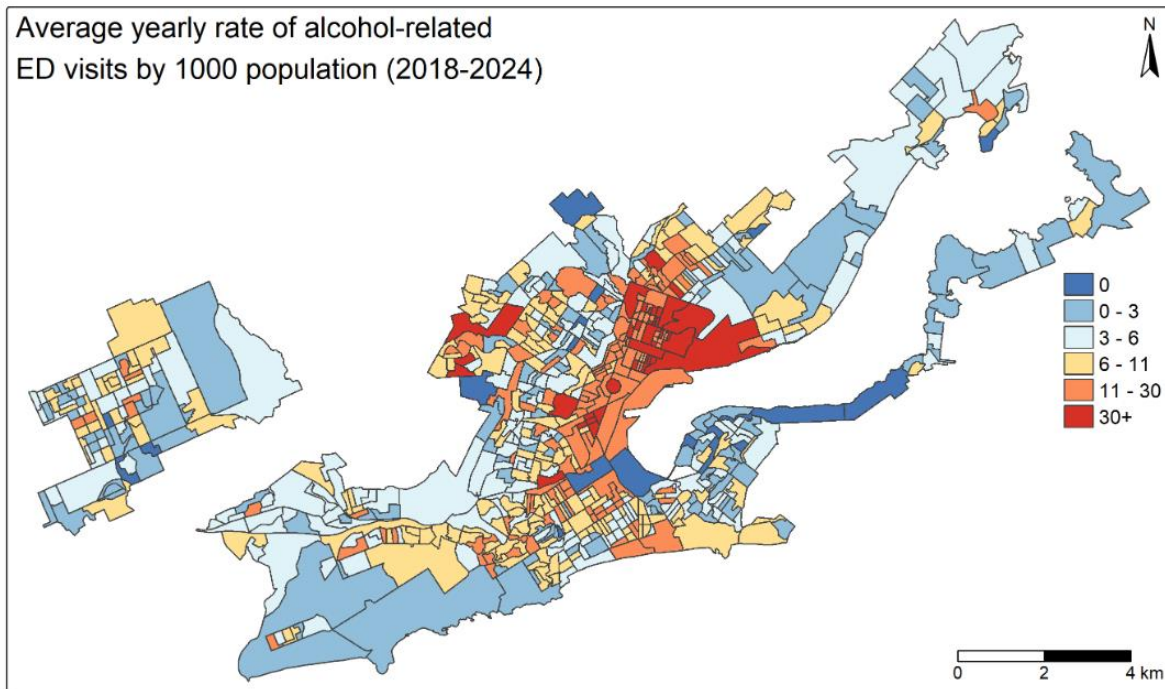


Figure One: Metropolitan Dunedin alcohol-related Emergency Department yearly visit rates by Statistical Area 1 level distribution, averaged over seven years (2018-2024, based on raw counts. Source: Te Whatu Ora, 2026).

- 15 It should be noted that the total volume of alcoholic beverages available for consumption was down 8.3% to 442 million litres in the twelve months to December 2025, having declined by 11.4% from a recent peak in 2021 (Stats NZ, 2026). On a per capita basis consumption, the number of standard drinks available has declined by 24.5% from 2011 to 2025 (i.e., 2.16 standard drinks to 1.63). LAPs are a means to build on these trends to reduce alcohol harm.

### Evidence

- 16 A selected summary of the evidence received during the 2024 review as well as current updates from the tri-agencies is as follows (in addition to the points noted in paragraphs 14(f) and 14(g)):
- a) Alcohol causes significant harms to individuals, whānau, and communities, conservatively costing New Zealand an estimated \$9.1 billion each year.<sup>1</sup>

<sup>1</sup> NZIER. Costs of alcohol-related harms in New Zealand: Updating the evidence with recent research. A report for the Ministry of Health. 2024.



Current LAP	Possible Options	Other LAP Examples
<b>Off-licence</b>		
No distance restrictions for new licences in relation to existing licences	200m distance for new licence to an existing one	1km for Waikato, other LAPs 100m or less
No location restrictions or limits outside District Plan limitations	No new off-licences in North Dunedin area from Moray Place to Botanic Gardens to University Oval	Auckland, Central Hawkes Bay, Hastings, Napier, Horowhenua, Tasman
	No new off-licences in NZ Deprivation Areas 7-10	Waikato, Porirua, Christchurch
Trading hours 7am-10am	Trading hours 9am-9pm (9am same as on-licences option)	Hastings, Porirua, Wairoa (10am-9pm); many others either 9am opening or 9pm closing
Sensitive area location restrictions – 100m from educational or recreational facilities for new licences	200m distance for new licences: to also apply to marae, places of worship, addiction services	150m: Gisborne, Horowhenua, Selwyn (Vapes comparison: 300m from schools, marae)
<b>On-licence</b>		
Opening time from 8am	Opening time 9am (same as off-licences)	Tauranga, Ōtorohanga, Horowhenua, Kawerau, Whakatane, Ōpōtiki
CBD entertainment venues close by 4am	CBD entertainment venues close by 3am (same as other CBD on-licences)	Gore, Southland, Invercargill
CBD One Way Door applies 2.30am	CBD One Way Door applies 2am	Waikato, Napier, Western Bay of Plenty, Mackenzie, Timaru
<b>Club</b>		
No duty manager (discretionary condition)	Duty manager must be present where higher numbers of people are expected when hosting tournaments or holding club functions	Carterton, Masterton, Wairarapa, Central Hawkes Bay
<b>Discretionary Conditions – Should Include: On-licence</b>		
Security requirements (CCTV, lighting, outside areas)	Include broader Crime Prevention Through Environmental Design principles	Twenty examples including Waikato, Gisborne, Rotorua Lakes, Tasman, Marlborough
Preventing intoxication	Maintain an incident register	Auckland, Thames-Coromandel, Waipa, Hutt, Horowhenua, Gore, Invercargill
Staffing requirements	Maintain staff training records	Ōtorohanga, Rotorua Lakes, Selwyn, Gore, Invercargill,
<b>Discretionary Conditions – Should Include: Off-licence</b>		
No restrictions on off-licence single sales	No single sales of beer 5% or greater strength (excluding craft beer) and RTDs, in 500ml container or less	Gore, Hastings, Hauraki, Porirua, Rotorua Lakes, Tauranga,
No restriction on 'Buy now, pay later'	Ban 'Buy now, pay later' schemes	Rotorua, Waikato, Waipa, Whanganui
No restrictions on external advertising	No external advertising/promotion of alcohol visible from the exterior of the premises	Waikato, Carterton, Gisborne, Marlborough, Ōtorohanga, Porirua, Thames-Coromandel, Waipa
<b>Discretionary Conditions – May Include</b>		

No time limit on serving rapid intoxication drinks	No sales of rapid intoxication drinks after 12am	Porirua, Timaru, Mackenzie, Waimate, Hastings
No restriction on alcohol delivery	No delivery of alcohol-only outside licencing hours	Porirua
Special licence requirements (several elements noted in current LAP)	In addition:	Central Hawkes Bay
	• Midnight cut-off time	
	• Limit of two drinks per serve for events over 1000 people	Whanganui, Carterton, Masterton, Wairarapa, Gisborne, Hurunui
	• Alcohol and event management plan for large events, i.e. over 400 people (Regulations definition)	Matamata, Hauraki, Waipa, Carterton, Masterton, Wairarapa, Kaikōura, Hurunui, Marlborough, Timaru
	• No sale of alcohol if high number of under 18 year olds	Carterton, Masterton, Wairarapa, Tasman

**Table One:** Possible LAP Content Options

### Stage Two Engagement

19 It is proposed to undertake broader engagement on the possible LAP content options to assist developing a draft for formal consultation. As well as the public, the following organisations have been identified as being likely to have a key interest in changes to alcohol licencing, or to be groups who may be disproportionately impacted by alcohol harm:

- Those stakeholders from Stage One
- ADL NZ
- Alcohol license holders (on-licence via Hospitality NZ)
- Āraiteuru Marae
- Disabled Persons Assembly NZ
- FENZ
- Health Coalition Aotearoa
- Hold On To Your Friends (HOTYF)
- Migrant and Former Refugee Women’s group
- Mirror Services
- Place-based groups (e.g. Brockville, Caversham, Corstorphine, Green Island, North East Valley, South Dunedin)
- Restaurant Association
- Salvation Army
- Sophia Charter
- Students for Sensible Drug Policy Ōtepoti (SSDP)
- Te Whare Tāwharau and Ōtepoti Communities Against Sexual Abuse
- Tūturu Ōtepoti
- Women’s Pasifika Network

20 The means of engagement will be to invite participation in a survey of the possible LAP content options, framed in terms of the objects of the Act, via the Council’s website.

21 Testing options to reduce alcohol harm is a key step to inform Council decisions on the content of the draft LAP that are reasonable, precautionary policy-based ones that take into account the preferences of the community.



- Stakeholder disappointment in not being given a second engagement opportunity as previously indicated.

### NEXT STEPS

28 Proposed next steps and approximate timeframes are:

- Stage Two engagement on possible LAP content options – May 2026
- Analysis of feedback and developing draft LAP content – June
- Report to Council to adopt a draft LAP for formal consultation – 22 July
- Consultation on draft LAP – August
- Hearings and deliberations – September
- Report to Council to approve new LAP – 15 October

### Signatories

Author:	Kevin Mechen - Alcohol, Psychoactive Substances and Gambling Advisor
Authoriser:	Bonnie Wright - Manager Compliance Solutions Paul Henderson - General Manager Corporate and Regulatory Services

### Attachments

	<b>Title</b>	<b>Page</b>
<a href="#">↓A</a>	DCC LAP Review Issues and Options Table 2026	157

**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision enables democratic local decision making and action by, and on behalf of communities.  
This decision promotes the social well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Other strategic projects/policies/plans	<input type="checkbox"/>	<input type="checkbox"/>	✓

The proposal to undertake further engagement on the LAP aligns with the Social Wellbeing strategic priorities of vibrant and cohesive communities, and safe and healthy people.

***Māori Impact Statement***

Local runaka will continue to have the opportunity to contribute to the LAP review.

***Sustainability***

There are no implications for sustainability.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

There are no identified implications for the LTP/Annual Plan, Financial or Infrastructure Strategies.

***Financial considerations***

There are no financial considerations identified.

***Significance***

Although the LAP is an important document that has a statutory basis, this decision to approve further engagement is considered to be low significance in terms of the Council’s Significance and Engagement Policy.

***Engagement – external***

The organisations that have been engaged with are noted in paragraph 16.

***Engagement - internal***

DCC’s regulatory team has been engaged with.

***Risks: Legal / Health and Safety etc.***

Legal risks are being managed through undergoing a three-step engagement process. DCC’s Legal Team or external lawyers will provide legal advice as required and as the review progresses.

***Conflict of Interest***

There are no known conflicts of interest.

***Community Boards***

There are no implications for Community Boards.

**Table One: Dunedin City Council Local Alcohol Policy Review - Issues and Options April 2026**

<b>Overarching issue</b>	<b>Examples and impacts of the issue</b>	<b>Issue raised by (via 2025/26 engagement, 2024 submissions)</b>	<b>Possible option(s) to mitigate issue</b>
<b>Alcohol harm in Dunedin is higher than other areas of the country</b>	Dunedin has the highest rate of hospitalisation wholly attributable from alcohol-related harm in the country	Medical Officer of Health Safe and Well Ōtepoti	Tighter restrictions for on- and off-licence trading hours
	Otago has higher hazardous drinking and heavy episodic drinking than the national average		
	Dunedin has the second highest rate for alcohol related harm ACC claims		
<b>The impacts of alcohol harm on Māori are disproportionate</b>	Māori are twice as likely to die from alcohol-related causes than non-Māori	Te Rūnanga o Ōtākou Medical Officer of Health	Expanding the definition of 'sensitive sites' to include marae
	Areas with a high Māori population have nearly 50% more outlets, with density linked to harm	Safe and Well Ōtepoti Health Coalition NZ	No external advertising/ promotion of alcohol visible at on or-off licenses from the exterior of the premises
<b>The impacts of alcohol harm on low-income communities/those living in high-deprivation areas are disproportionate</b>	In Dunedin, 71% of all alcohol-related Emergency Department visits were from people who resided in areas of high deprivation (North Dunedin are 23-27%, yet 7-8% of population)	Safe and Well Ōtepoti Medical Officer of Health	No new off-licences in NZ Deprivation 7-10 areas
	Harm is caused by the clustering of premises	Police	No new off-licences in North Dunedin area from Moray Place to Botanic Gardens to University Oval
	Proportionally more off-licenses in high-deprivation areas	Te Rūnanga o Ōtākou	Banning of buy-now-pay-later schemes for purchasing alcohol
	In New Zealand, adolescents who live in the most deprived areas typically consume the greatest quantity of alcohol	Salvation Army Health Coalition NZ	200m distance restriction of new off-licence to an existing one
	Buy-now-pay-later schemes encourage people (including students) to live beyond their immediate means, may lead to debts	Alcohol Healthwatch OUSA Kimberly Cousins (Dunedin alcohol)	

		researcher and academic – 2024 submission)	
<b>Alcohol harms are wide ranging, and impact wider families and communities</b>	Impact on wider family and communities, including through family violence (and the impact on children)	Te Rūnanga o Ōtākou Safe and Well Ōtepoti	Include broader Crime Prevention Through Environmental Design (CPTED) principles, e.g. monitoring of outdoor areas of on-license premises trading after 12am  200m distance restriction on new off-licences opening near sensitive areas (including community libraries and pools, schools, marae, addiction services)  Requirement for sports clubs to have a certified duty manager present when hosting tournaments or holding club functions
	Links of alcohol to sexual violence	Disabled Persons Assembly NZ	
	Micro-aggressions from intoxicated persons towards people with disabilities around George Street and the Octagon		
	Currently no safe zones for families and young people		
	Some clubs are at a higher risk of alcohol-related harm occurring		
<b>Alcohol advertising contributes to further consumption and harm</b>	Alcohol advertising impacts on youths and contributes to the normalisation of drinking	Chief Licensing Inspector (2024 submission)	No external advertising/ promotion of alcohol visible at on or-off licenses from the exterior of the premises
	Students are often specifically targeted by advertising	Medical Officer of Health (2024 submission)	
	Advertising can trigger people with alcohol use disorders, making it harder for them to stay sober or reduce their drinking	Police (2024 submission)	
	Children in New Zealand are exposed to substantial alcohol marketing	Te Rūnanga o Ōtākou	

		<p>Safe and Well Ōtepoti</p> <p>Wellsouth Primary Health Network</p> <p>Salvation Army</p> <p>OUSA</p> <p>Healthcare Coalition NZ</p> <p>Alcohol Healthwatch</p>	
<b>Alcohol harm occurs in and around on-licences</b>	District Plan definitions of residential and non-residential areas may have unintended consequences such as preventing the opening of a new on-licence in North Dunedin that could provide a safer space for students	Te Rūnanga o Ōtākou	Moving forward the one-way-door time to 2am for all on-licences in the CBD, to allow for gradual release of patrons into the community
	There are regular occurrences of alcohol-related assaults in and around the Octagon	Police	Reduction of on-licence hours
	People can be asked to leave one establishment due to being intoxicated, but may still be able to enter a different establishment and be served	Medical Officer of Health	Include broader Crime Prevention Through Environmental Design (CPTED) principles, e.g. monitoring of outdoor areas of on-license premises trading after 12am
	Incident registers are not currently mandatory meaning harm is not always recorded and/or tracked	Chief Licensing Inspector	Requirement that all premises keep an incident register to be made available to a Licencing Inspector or Police upon request
	Queues outside of CBD premises pose issues and potential risks to pedestrian and road user safety	OUSA	No sales of rapid intoxication drinks after 12am
	There is less evidence of host responsibility/harm minimisation in city venues	Kimberly Cousins	

<b>Alcohol harm causes a strain on emergency services</b>	Incidents of alcohol harm in and around the Octagon require limited emergency resources	Police Medical Officer of Health	Moving the one-way-door time to 2am in the CBD to stagger exits, which and can help reduce the number of incidents emergency services need to attend
	Wider secondary alcohol harm includes pre-emptive call outs for ambulances, which impact genuine call outs	Safe and Well Ōtepoti Emergency Department Senior Medical Officer	
	Resources at Dunedin’s emergency department are very stretched, compromising the quality and treatment of care provided to other patients	Kimberly Cousins	
	Presenters aged 15 – 24 consistently have the highest number of alcohol-related ED visits, accounting for over half of all alcohol-related visits		
<b>Harm associated with off-licence purchases</b>	72% of drinking occasions among high-risk drinkers occurs at locations outside of on-licenses	Medical Officer of Health Police	No new off-licenses in NZ Deprivation 7-10 areas
	Established relationship between trading hours, consumption and harm	Chief Licencing Inspector	No single sales of beer 5% or greater strength (excluding craft beer) and ready-to-drinks (RTDs), in 500ml container or less Restrictions on the trading hours of off-licenses
	There are issues of alcohol harm being caused by pre-loading before heading into town, often due to cheaper alcohol prices at off-licenses	Safe and Well Ōtepoti Te Rūnanga o Ōtākou	
	Single-sales can encourage impulsive drinking, particularly for people who struggle with alcohol dependence or have limited finances	Salvation Army	Requirement that all on-and-off licenses keep staff training records and make these available to a Licencing Inspector and Police upon request
	Alcohol harm often happens at home before going to on-licenses, with alcohol purchased from off-licenses	Healthcare Coalition NZ Alcohol Healthwatch	
<b>There is a city-and-country-wide cost of alcohol harm</b>	Across New Zealand, the cost of alcohol harm is estimated to be \$9.1 billion each year	Medical Officer of Health Safe and Well Ōtepoti	Restrictions on the trading hours of on-and-off-licenses
	There is a social and financial cost of alcohol harm to Dunedin, including from specific student events such as St Patrick’s Day, Hyde Street		
<b>Accessibility and access to alcohol can increase the likelihood of harm</b>	Alcohol accessibility and affordability impacts the ability of recovery for people who may struggle with alcohol dependence	Medical Officer of Health Chief Licencing Inspector	All off-licensed premises, unless endorsed under Section 40 of the Act as a remote seller, be prohibited from delivering alcohol (including via the use of a third-party delivery
	Hazardous drinking increases with longer hours of access and greater availability of alcohol	Police	
	Incidence of alcohol related harms linked to longer trading hours	Safe and Well Ōtepoti	

	Uber deliveries have been found to be 73% non-compliant for age requirement checks	Alcohol Healthwatch	company) outside of their allowable licensing hours
	Remote delivery services fail to provide drivers with the necessary training to assess customer intoxication or protect drivers from intoxicated customers	Salvation Army	Moving entertainment venue closing times forward from 4am to 3am to align late night closing times
	Higher alcohol density associated with higher rates of harm	OUSA	
		Healthcare Coalition NZ	
		Kimberly Cousins	
<b>Special licence terms can go further to minimising alcohol harm at events</b>	Currently non-licenced premises can run an event until 1am, but licenced premises can only run an event until midnight	Chief Licencing Inspector	All special licenses to have the same midnight cut-off time
	People at large events are not always assessed for signs of intoxication before purchasing additional units of alcohol	Police	Limit of two drinks per serve where 1000 patrons or more are expected at an event
	The current LAP defines large-scale events as being 150 people or more, which doesn't align with the special licence definition of '400 people or more'	Medical Officer of Health	Requirement of an alcohol and event management plan for any special licence event of 400 people or more
	The sale of alcohol at events where a significant proportion of the attendees are under the age of 18 normalise and expose children to alcohol use	Safe and Well Ōtepoti	Banning the sale of alcohol at events where a significant proportion of attendees are under the age of 18 (e.g., school events)
		Alcohol Healthwatch NZ	

## **HEARINGS COMMITTEE RECOMMENDATIONS - PROPOSED PARKING CHANGES MARCH 2026**

Department: Civic

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### **EXECUTIVE SUMMARY**

- 1 This report presents recommendations of the Hearings Committee meetings (Attachment A - Minutes of Hearings Committee meetings) held on 20 March 2026 and 14 April 2026 on proposed changes and corrections to parking restrictions.
- 2 This report also presents recommendations of the Hearings Committee meeting (Attachment B – Minutes of Hearings Committee meeting) held on 19 September 2023, for the Harbour Arterial Stage 3: Strathallan Street (Andersons Bay Road to Portsmouth Drive) parking changes.
- 3 The Hearings Committee (the Committee) recommends that the proposed parking restrictions presented in this report be approved.
- 4 If approved by Council, the changes recommended by the Hearings Committee will be included in the Council’s GIS bylaw database and become part of the Dunedin City Traffic and Parking Bylaw.
- 5 The proposed changes in parking and traffic restrictions are presented in four sections:
  - a) general minor parking changes that progressed to the 20 March and 14 April 2026 Hearings Committee meetings, which include requests from residents, changes made to enhance safety or amenity, and suggestions from officers. These changes include:
    - i. changes to time restrictions to encourage more parking turnover, supporting better access to new local businesses for communities,
    - ii. improvements to enable more efficient public transport services, and
    - iii. introduction of various no stopping areas following safety assessments;
  - b) general minor parking changes that progressed to the 19 September 2023 Hearings Committee meeting for Harbour Arterial Stage 3 parking changes along Strathallan Street (Andersons Bay Road to Portsmouth Drive) and around the new Orari Street/Otaki Street roundabout;
  - c) a change of turning restrictions set out in Schedule 2 of the Traffic and Parking Bylaw to introduce a “no right turn” from the eastern end of Strathallan Street onto Portsmouth Drive, as part of Harbour Arterial Stage 3 changes; and
  - d) corrections to the bylaw database to ensure it matches existing markings, parking restrictions, and signage.

- 6 Where required, consultation has been undertaken on the proposed changes discussed in this report.

## RECOMMENDATIONS

That the Council:

- a) **Adopts** the proposed changes to traffic and parking restrictions shown in the March 2026 update of the Dunedin City Council's traffic and parking restrictions database: [Dunedin Parking Controls - March 2026 \(TPC53, TPC54\)](#)
- b) **Adopts** the proposed changes to parking restrictions for the Harbour Arterial Stage 3 project shown in the September 2023 update of the Dunedin City Council's traffic and parking restrictions database for TPC38-13 and TPC38-14: [Dunedin Parking Controls - September 2023](#)
- c) **Adopts** the proposed change to Schedule 2 of the Traffic and Parking Bylaw to introduce a new turning restriction on the eastern end of Strathallan Street.
- d) **Notes** that Council officers will undertake follow-up actions for some of the proposed parking changes (with actions described in paragraphs 26, 31, 39, and 48).
- e) **Notes** that the Hearings Committee has considered feedback from consultation on the proposed changes relating to changes and restrictions.
- f) **Notes** that all parking restrictions previously approved by the Council remain unchanged.

## BACKGROUND

### Traffic and parking controls

- 7 Traffic and parking controls contribute to the objectives of the Dunedin Integrated Transport Strategy 2013 and the Dunedin City Council 9 Year Plan 2025-2034 by supporting a safe, efficient, and accessible transport network.
- 8 The Council maintains a Geographic Information System (GIS) map bylaw database of traffic and parking restrictions (the database) that reflects all on-street parking restrictions that are implemented with markings and/or signs.
- 9 Parking controls are made under the Dunedin City Council Traffic and Parking Bylaw. The Committee has the delegation to consider changes to parking controls and to make recommendations to Council, which subsequently approves traffic restrictions and parking controls.

### Context for proposed changes

- 10 The Council often receives requests from individuals and businesses to change parking restrictions. When considering these requests, officers assess a range of factors including safety concerns, commuters' needs, commercial users' needs, road width and topography, traffic flow, neighbouring on-street parking spaces, visibility concerns and crash statistics. If a proposed change is supported by officers, consultation is undertaken with affected residents, businesses, and property owners to determine level of support for the requested change.

- 11 The proposed changes in parking and traffic restrictions are presented in four sections:
- a) general minor parking changes that progressed to the 20 March 2026 Hearings Committee meeting and follow-up Hearings Committee meeting on 14 April 2026, which include requests from residents, changes made to enhance safety or amenity, and suggestions from officers. These changes include:
    - i. changes to time restrictions to encourage more parking turnover, supporting better access to new local businesses for communities,
    - ii. improvements to enable more efficient public transport services, and
    - iii. introduction of various no stopping areas following safety assessments;
  - b) general minor parking changes that progressed to the 19 September 2023 Hearings Committee meeting for Harbour Arterial Stage 3 parking changes along Strathallan Street (Andersons Bay Road to Portsmouth Drive) and around the new Orari Street/Otaki Street roundabout;
  - c) a change of turning restrictions set out in Schedule 2 of the Traffic and Parking Bylaw to introduce a “no right turn” from Strathallan Street onto Portsmouth Drive, as part of Harbour Arterial Stage 3 changes; and
  - d) corrections to the bylaw database to ensure it matches existing markings, parking restrictions, and signage.

## DISCUSSION

- 12 Proposed general parking changes reviewed in the 20 March and 14 April 2026 Hearings Committee meetings are shown in the Council bylaw database: [March 2026 Proposed Parking Changes](#) and are detailed in Attachment C – Minor General Parking Changes. The GIS layer includes a bookmark feature which links the numbered cases presented in the tables of Attachment C to their specific location.
- 13 The proposed general parking changes for the Harbour Arterial Stage 3 project were reviewed as part of the 19 September 2023 Hearings Committee meeting: [Minutes of Hearings Committee - Tuesday, 19 September 2023](#). The Hearings Committee recommended that the Council approve the proposed changes to parking and traffic restrictions shown in the September 2023 update of the Dunedin City Council's traffic and parking restrictions database: [Dunedin Parking Controls - September 2023](#), with Harbour Arterial Stage 3 changes shown in TPC38-13 and TPC38-14 with details set out in Attachment E.
- 14 In their meeting of 19 September 2023, the Hearings Committee also recommended that the Council approve the change of one turning restriction to introduce a “no right turn” from Strathallan Street onto Portsmouth Drive, requiring vehicles to turn left onto Wharf Street at the eastern end of Strathallan Street to assist with traffic flow efficiencies as part of the Harbour Arterial Route. This change is proposed in a highlighted update to Schedule 2 of the Traffic and Parking Bylaw in Attachment G.
- 15 To ensure that the regulatory changes for the Harbour Arterial Stage 3 project align with the construction and implementation of Stage 3 upgrades, the 12 December 2023 parking changes report to Council noted that: *“The parking changes discussed at the 19 September 2023 Hearings*

*Committee relating to the Harbour Arterial Project are not included as part of this report and will be brought before Council at a later date”: [Agenda of Council - Tuesday, 12 December 2023](#) (page 254).*

- 16 Proposed parking changes for TPC38-13 Strathallan St/Otaki St and TPC 38-14 Otaki St/Orari St and the proposed turning restriction from Strathallan Street onto Portsmouth Drive were therefore not presented for Council approval in December 2023, and are presented for Council approval in this report now that Harbour Arterial Stage 3 upgrades are underway.
- 17 For changes that progressed to the 20 March Hearings Committee, consultation on proposed minor general parking changes occurred from January–February 2026 (except where consultation had already been completed). The 48 submissions received related to 12 of the proposed parking changes, and four submitters spoke to the Hearings Committee meeting. The full set of feedback received from affected parties, including owners, residents and businesses, is presented in Attachment D – Feedback Received.
- 18 For the Harbour Arterial Stage 3 changes, consultation on the concept design for the project (including parking changes and the “no right turn” turning restriction) was conducted from 1 August–5 September 2022. The summary of feedback from the consultation is included in Attachment F – Concept Design and Feedback, and no submitters spoke to the 19 September 2023 Hearings Committee meeting.

### **Minor General Parking Changes – TPC-53 (Attachment C)**

#### ***Changes to support public transport efficiency***

- 19 The existing bus stop at 138 Princes Street is one of the most popular stops on the bus network, serviced by 11 bus routes (with an average of 8,069 people disembarking at this stop per month, making it the busiest stop Dunedin-wide for people disembarking). The stop currently only has capacity for one bus, meaning that buses queue across the Dowling Street intersection and in the lane behind when more than one bus arrives. When buses stop across the intersection to let passengers off, accessibility is reduced as the bus ramp cannot be lowered, and safety for other road users is compromised.
- 20 To address these issues, a parking change is proposed to increase the length of the bus stop to be a double capacity stop, by removing the existing kerb buildout and tree and relocating the existing bench and rubbish bins.
- 21 Feedback received through public consultation was largely in favour of the change, with one opposing submission reflecting a desire for additional amenity improvements to be provided along the Princes Street corridor before prioritising this particular improvement.
- 22 In the meeting of 20 March, Hearings Committee members raised concerns about the loss of amenity that would result from extending the bus stop, removing the tree, lamppost, and kerb buildout, and relocating the rubbish bins and bench, and asked officers to consider alternative proposals for the bus stop, including the potential to trial a bus stop further up Princes Street (closer to the Octagon). Committee members also asked for more information about the scheduling of bus services using that stop.
- 23 The Hearings Committee reconvened on 14 April to discuss these alternatives and hear the recommendations from Council staff and the Otago Regional Council (more information of the discussion is set out in Attachment A). As a result of this meeting, two Hearings Committee

members voted in favour of recommending the original proposal, and one Committee member voted against. The original proposal is retained in Attachment C.

- 24 Other proposed parking changes that contribute to improved public transport efficiency are:
- a) Installing two sections of no stopping lines around an existing bus stop at the Stuart Street and London Street intersection to provide buses sufficient space to enter and exit in line with New Zealand Transport Agency best practice guidelines,
  - b) Installing no stopping lines (which removes two unrestricted parks) at the exit tapers of the inbound and outbound bus stops near 115 Taieri Road (Kaikorai) to ensure parked cars are not obstructing buses from exiting those stops, and
  - c) Making the bus stop at 428 North Road (North East Valley) (which has been a temporary bus stop since 2023 after the previous stop was decommissioned for safety and accessibility concerns) permanent, which will formally remove two unrestricted car parks and ensure that clear signage and markings are in place to prevent people from parking in the bus stop.
- 25 One submission received through public consultation opposed the removal of the two car parks at 115 Taieri Road, citing adequate space for buses to exit the stops. However, the submission provided by Otago Regional Council highlights that exit tapers are important to improve bus access to the kerb and make it safer and more efficient for buses to manoeuvre in and out of the stops (see Attachment D for both submissions).
- 26 For the proposal to make the temporary bus stop at 428 North Road permanent, Otago Regional Council (ORC) conducted their own consultation in mid-2025, including the installation of a bus shelter and road markings at the location. The ORC received one submission in opposition to this change (see Attachment D) from two residents who spoke to the Hearings Committee on 20 March. The residents noted that buses continued to idle at the stop, and officers agreed to request that ORC remind drivers not to use that stop as a layover.

***Changes to better serve local businesses and communities***

- 27 A number of proposed parking changes involve reducing time restrictions on parks to better serve new and existing businesses in the area. Reducing the time restriction on a park supports more parking turnover, meaning more people can use the park to access local businesses. Changes to parking time restrictions are proposed near 8 Sturdee Street, 335A Kaikorai Valley Road, 194B Main South Road, 27 North Road, 15 Gladstone Road South, and 165 Eglinton Road. Feedback on these proposed changes appears in full in Attachment D.
- 28 One submission was received in support of the proposed parking changes near 15 Gladstone Road South (Mosgiel), and one submission was received in support of changes near 8 Sturdee Street (Central Dunedin).
- 29 In reviewing the feedback on the proposed changes to 335A Kaikorai Valley Road (Bradford) and 194B Main South Road (Green Island), officers have updated the original proposed parking changes:
- a) For 335A Kaikorai Valley Road, the original proposed change would convert one unrestricted car parks to one P5 park to support parking turnover for a new café. Four of the six submitters supported the conversion of two unrestricted car parks to two P5s, instead of one. Given the availability of other unrestricted parking in the area, officers

revised the proposed parking change to propose converting two unrestricted parks to two P5 parks (shown in Attachment C).

- b) For 194B Main South Road, the original proposed change would convert seven P15 car parks and one P10 car park to eight P5 car parks, to support parking turnover for the local Night 'n Day. Nine of the ten submitters opposed this change, citing negative effects on a local takeaway business and too much time pressure on customers queuing at the Night 'n Day. To better balance the needs of the Night 'n Day, customers, and other businesses in the area, officers revised the proposed parking change to propose converting five P15 car parks to P5s and converting two P15 car parks to P10 parks to make the time restriction of these two parks consistent with the adjacent existing P10 park (shown in Attachment C).
- 30 Officers considered the submissions on 27 North Road and 165 Eglinton Road and did not propose updates to the original proposed parking changes:
- a) For 27 North Road, the proposed parking change would convert two P30 car parks to two P5 parks to support parking turnover for a new Night 'n Day at 27 North Road. Six of the nine submitters opposed this change, citing the impact on other local businesses in the area. Officers reviewed the parking restrictions along North Road. Given the availability of other P30 parks and the limited availability of other shorter-term parks that would enable people to quickly access the Night 'n Day, the original proposal has not been amended. Officers also conducted a parking survey near 27 North Road over the course of eight days and identified that the current P30 parks outside of 27-31 North Road and outside of Dunedin North Intermediate are used often by the same vehicles across days and times of day, which suggested that there may be an issue with vehicles overstaying the existing time restrictions. As a result, officers have requested that the Parking Services team monitor this area of parks to ensure that vehicles are not overstaying.
- b) For 165 Eglinton Road, the proposed parking change would convert one pick-up drop-off park to a P5 park and convert two P10 parks to two P5 parks to support better parking turnover for the Night 'n Day. Submissions from Night 'n Day Foodstores Limited and Anchorage Properties Limited proposed also converting three sixty-minute (P60) parks to short-term parking and converting some of the P60 car parks on the other side of the road to short-term parking (see Attachment D). Given that these were the only submissions received on the proposed changes at 165 Eglinton Road, officers considered that not enough of a basis was provided to proceed with changes further to those already proposed, and more consultation would be needed before making further changes. The original proposal in Attachment C has therefore not been amended.
- 31 For 165 Eglinton Road, one submitter spoke to the Hearings Committee on 20 March on behalf of the Night 'n Day and emphasised the need for more short-term parking in the area. The Hearings Committee requested that officers review the parking restrictions near 165 Eglinton Road and investigate what further adjustments to time restrictions in the area could be brought as a proposal to the June Hearings Committee.
- 32 Two proposed parking changes would free up access to unused residents'-only parking spaces by converting them to unrestricted parks. The proposed parking changes would replace one residents'-only park at 71 Duncan Street (Central Dunedin) with an unrestricted park, and one residents'-only park at 32 Dundas Street (Dunedin North) with an unrestricted park. No submissions were received on these proposals.

- 33 The proposed parking change at 2 Broadway (adjacent to Broadway Medical Centre) would make permanent the temporary parking arrangements that were in place during the construction of the new Otago Regional Council building. The change would convert one paid P60 car park to one 2-hour (P120) mobility park and convert one paid P60 car park to one P5 park. The three submissions received supported the change, citing the value of having a mobility park available, as well as a P5 to support drop-offs and pick-ups at the medical centre.
- 34 From August to October 2025, the Council ran a trial that provided 30 additional unrestricted car parks along Main South Road (Fairfield), in the wide shoulder area beside the Sunnyvale Community Centre. This trial responded to community concerns about congestion, limited parking availability, and safety concerns, particularly during weekends with busy sporting events. Council Parks & Recreation officers led the consultation on the trial with nearby residents and property owners, accepting feedback from August to the end of October. Based on the positive feedback received and positive benefits for the community, the proposed parking change would make permanent the removal of no stopping lines and the installation of 30 unrestricted car parks along Main South Road.
- 35 Across from 2 Christie Street (Abbotsford), the proposed parking change would remove redundant no stopping lines where a new parking bay has been constructed. Four unrestricted car parks are available in the parking bay, providing more parking amenity adjacent to the busy sports fields at Miller Park. No submissions were received on this proposal.
- 36 Construction of the Changing Places bathroom has started in the parking lot near the Railway Station at 20 Anzac Avenue. To ensure the right parking restrictions are in place, the proposed parking change converts four private car parks near the bathroom site to one time-restricted mobility park and converts the two unrestricted parks to two P5 parks to support more parking turnover. No stopping lines will be installed across from the parks and a no parking zone installed in front of the bathroom site to prevent vehicles from obstructing access to the site.

The four private car parks were formerly used by nearby tenants but are not part of the tenants' lease agreement. Council officers contacted the tenants directly to inform them of proposed changes, and consultation on the Changing Places bathroom installation was conducted through the 9 Year Plan consultation.

***Changes as a result of safety assessments***

- 37 Safety assessments have resulted in proposed parking changes near Fern Road, Fox Street, Toitū Otago Settlers Museum, Rattray Street, Stuart Street, Moray Place, Musselburgh Rise, and Otaki Street.
- 38 At 1 Fern Road (Maia) and 20 Fox Street (South Dunedin), proposed parking changes would add no stopping lines to sections of road that are not long enough to accommodate a standard car park (which is 6 metres as the Council standard). Vehicles parking in these areas have raised community concerns about safety and obstructed sight lines pulling in and out of driveways at the Dunedin Rudolf Steiner School & Kindergarten (Fern Road) and at the business located at 20 Fox Street. Two submissions were received on proposed changes at 1 Fern Road, and two submissions were received on 20 Fox Street.
- 39 For Fern Road, one submitter spoke to the 20 March Hearings Committee meeting, highlighting repeated issues with obstructive and unsafe parking during school drop-off and pick-up times. Council officers agreed to follow up on these issues by:

- a) Working with the school to discuss communications for parents and staff with reminders about safe and appropriate parking, as well as parking options in the area
  - b) With a safety engineer, assessing options for traffic and parking changes near the school and bringing a proposal to the June Hearings Committee, and
  - c) Putting in a request for Parking Services to increase monitoring in the area around pick-up and drop-off times at the school.
- 40 At Toitū Otago Settlers Museum, the proposed parking change on High Street would relocate the pick-up drop-off park further down the road so that the existing coach stop in front of the museum could be extended. This would provide better access for coaches and shuttles to pull directly into the stop instead of manoeuvring around a vehicle in the current pick-up drop-off park, and provides more room for coaches and shuttles to queue.
- 41 At 164-170 Rattray Street (Central Dunedin), four unrestricted car parks were obstructing two new vehicle crossings at a commercial lot, which creates safety risks for vehicles attempting to enter or exit the lot. The proposed parking change would formalise the removal of these four unrestricted car parks and regulate three sections of no stopping lines around the vehicle crossings. One unrestricted car park adjacent to the lot would also be converted to one paid P60 park, to align with the surrounding parking restrictions. Installing two vehicle crossings was a condition of the resource consent for the lot, where a leased car parking facility is now operational and makes 52 car parking spaces available to lease, including two mobility car parks.
- 42 The Hearings Committee queried whether an additional car park could be included in the approximately 9-metre-wide gap between the two vehicle crossings, and asked officers to review and provide a safety assessment of the viability of an additional park at the follow-up meeting of the Committee on 14 April.
- 43 On 14 April, officers presented the results of this assessment and noted that installing a car park between the vehicle crossings would obstruct the required sight lines as stated in the resource consent for the lot and as required under Rule 6.6.3.2 of the Dunedin City Council 2<sup>nd</sup> Generation District Plan. The rule requires a sight distance of 69 metres from a new vehicle access onto any road other than a state highway where the speed limit is 50 kilometres per hour. As a result, the original proposed parking change for 164-170 Rattray Street remains unchanged. The Hearings Committee requested that officers separately provide more information about the 2<sup>nd</sup> Generation District Plan and how the sightline requirements for new vehicle crossings were established.
- 44 At 233 Stuart Street (Central Dunedin), vehicles pulling into two existing P5 parks have caused damage to the historic veranda. To reduce risk of further damage to the building, the proposed parking change would replace the two P5 parks with 12 metres of no stopping lines. On reviewing feedback, officers proposed an adjustment to the original proposed change:
- a) The original proposed change would also shift the existing section of six four-hour (P240) car parks further up Stuart Street by approximately 1 metre, with no change proposed to the P5 park in front of 245 Stuart Street.
  - b) The three submissions received highlighted the need for P5s to still be available near 233 Stuart Street for couriers. Two submissions suggested converting two P240 parks to P5 parks in front of 235 Stuart Street, rather than moving them altogether (see Attachment D for the full submissions). One submission suggested relocating the existing P5 park in front of 245 Stuart Street to be adjacent to where the previous P5 parks were.

- c) Officers have updated the proposed parking change to propose converting one P240 park to a P5 park in front of 235 Stuart Street, and converting the existing P5 car park in front of 245 Stuart Street to a P240 car park. This would ensure that the balance of time restrictions remains the same between the original and the updated proposed change (six P240 car parks, one P5 park), but changes the location of the P5 park so that couriers can easily pull into the park (see Attachment C for the updated proposal). One submission highlighted that the P240 parks are frequently used as if they are all-day parks. As a result, a request has been put through to Parking Services to have the P240 parks monitored to ensure that vehicles are not overstaying in those parks.
- 45 On Moray Place near St Paul's Cathedral, the proposed parking change would remove 40 metres of faded no stopping lines from within the existing coach stop, to reflect that the coach stop is permanent and to have the proper signage installed.
- 46 At the Musselburgh Rise and Shore Street intersection (Andersons Bay), the roundabout installed as a trial in 2019 to address speed, congestion, and pedestrian safety issues will be made permanent. The proposed parking change would remove one unrestricted car park and install 9 metres of no stopping lines near 170 Musselburgh Rise to ensure vehicles can safely exit the roundabout without obstruction from parked vehicles, and to enable pedestrian crossings to be installed. Transport Safety officers led the consultation on the roundabout works in December, and no feedback was received about the installation of the no stopping lines.
- 47 On Otaki Street adjacent to the Bunnings car park (South Dunedin), a section of eight unrestricted parks is frequently used by trucks and trailers that are left in that location for long periods of time. Trucks blocking off those parking spaces reduce sight lines and create safety risks for vehicles entering and exiting the Bunnings car park. The proposed parking change would convert the eight unrestricted parks to eight P120 parks to address these safety risks and support more parking turnover.
- 48 On Macandrew Road near Port Otago (Port Chalmers), a section of approximately ten unrestricted parks creates potential safety risks with the volume of large trucks and other vehicles travelling up and down the road and using those parks, which can push other drivers across the centre line to go around them. The proposed parking change would replace the 10 unrestricted parks with no stopping lines to improve safety for all road users on that section of road. Given the impact of the proposed change on parking availability in the area, the Hearings Committee recommended that this proposal be withdrawn and brought back to the June Hearings Committee if necessary, with a more detailed safety assessment provided. The removal of this parking change proposal from the March changes is noted in Attachment C, and this proposal has been removed from the online map of proposed changes.

### **Parking counts for minor general parking changes**

- 49 Proposed minor general parking changes presented in this report for March 2026 will result in the loss of one restricted car park, the gain of 18 unrestricted car parks, the loss of three motorcycle parks, and time restriction changes for 34 car parks. The detailed breakdown is presented in Table A.

<b>TABLE A - PARKING COUNTS</b>				
<b>MINOR GENERAL PARKING CHANGES – TPC 53</b>				
	<b>Restricted car parks</b>	<b>Unrestricted car parks</b>	<b>Motorcycle parks</b>	<b>Restriction change only</b>
Changes to support public transport efficiency	N/A	-7	-3	N/A
Changes to better serve local businesses & communities	1	30	N/A	23
Changes as a result of safety assessments	-2	-5	N/A	11
<b>TOTAL</b>	<b>-1</b>	<b>18</b>	<b>-3</b>	<b>34</b>

### **Minor General Parking Changes for Harbour Arterial Stage 3 – TPC 38-13 and 38-14 (Attachment E)**

- 50 The purpose of the Harbour Arterial project is to mitigate travel disruption related to the new Dunedin hospital construction. It includes a package of improvements to enable the Harbour Arterial (Strathallan, Wharf, Thomas Burns and Ward Streets) to be a planned alternative route to/from the Port and the central city (especially for heavy vehicle traffic). Improvements include real-time signage and enhancements to traffic signals to make travel more efficient, and the reallocation of road space to allow additional capacity for large vehicles.
- 51 The budget for these Harbour Arterial improvements was re-confirmed through the Dunedin City Council 9 Year Plan 2025-34, which was adopted by the Council in their meeting of 30 June 2025:: [Minutes of Council - Monday, 30 June 2025](#).
- 52 The project is split into four stages, covering the following locations:
- Stage 1: Wharf Street (Kitchener Street to Birch Street) (completed)
  - Stage 2: Thomas Burns Street (Fryatt Street to Bombay Street) (completed)
  - Stage 3: Strathallan Street (Andersons Bay Road to Portsmouth Drive) (consulted, brought to the September 2023 Hearings Committee, and included in this report for consideration now that road and intersection upgrades are underway)
  - Stage 4: Ward Street (Thomas Burns Pedestrian Overbridge to SH88) and St Andrew Street extension (Ward Street to the railway line) (future work programme)
- 53 Changes proposed as part of Stage 3 include the removal of all parking along Strathallan Street, and the implementation of broken yellow lines at the new Orari Street/Otaki Street roundabout and Strathallan Street/Otaki Street intersection.
- 54 Consultation was run between 1 August – 5 September 2022 for the community to give feedback on the concept designs. The concept design and feedback summary from this consultation is included as Attachment F. No submitters spoke to the Hearings Committee meeting on 19 September 2023.

- 55 The parking changes are detailed below. While the number of parks lost is calculated using the measurement of a standard car park (6 metres), in reality, unrestricted parks on Strathallan Street were frequently used by heavy trucks occupying multiple spaces.

<b>TABLE B - HARBOUR ARTERIAL STAGE THREE PARKING CHANGES</b>		
	<b>Restricted parks lost</b>	<b>Unrestricted parks lost</b>
Strathallan Street	2	48
Otaki Street	0	15
Orari Street	4	5
<b>HARBOUR ARTERIAL TOTAL</b>		<b>74</b>

- 56 In their meeting of 19 September 2023, the Hearings Committee considered the regulatory changes needed for Stage 3. The Hearings Committee recommended that the Council approve the proposed changes to parking and traffic restrictions for the Stage 3 project, and the proposed parking changes as shown in TPC 38-13 and TPC 38-14 in the September 2023 update of the Council’s bylaw database: [Dunedin Parking Controls - September 2023](#) (see Attachment B for the meeting minutes).

**Change of Turning Restrictions (Attachment G)**

- 57 As part of the Traffic and Parking Bylaw, the Council may impose turning restrictions and install signage to prohibit:
- a) vehicles on a roadway that are turning from facing or travelling in one direction, from facing or travelling in the opposite direction (no ‘U-turns’)
  - b) vehicles or specified classes of vehicles from turning to the right or to the left, or from proceeding in any other direction.
- 58 This report includes one change to turning restrictions as part of the Harbour Arterial Stage 3 project and proposes to introduce a “no right turn” restriction from the eastern end of Strathallan Street onto Portsmouth Drive, requiring vehicles to turn left onto Wharf Street at the eastern end of Strathallan Street to assist with traffic flow efficiencies as part of the Harbour Arterial Route.
- 59 In their meeting of 19 September 2023, the Hearings Committee recommended that the Council approve the proposal to amend Schedule 2: Turning Restrictions of the Traffic and Parking Bylaw to introduce the new “no right turn” restriction as part of the Harbour Arterial Stage 3 project.
- 60 The proposed addition of this turning restriction to Schedule 2 of the Traffic and Parking Bylaw is highlighted in Attachment G.

**Corrections – TPC-54 (Attachment H)**

- 61 Corrections of parking restrictions to the bylaw database are detailed in Attachment H – Corrections. They do not change current parking restrictions, but make corrections to the database to accurately reflect the parking restrictions and markings which are on the ground.

## OPTIONS

- 62 Two options are proposed for general parking changes. The recommended option (Option One) is to proceed with some or all of the proposed changes to the bylaw database, and Option Two is maintaining the status quo.

### Option One – Recommended Option

- 63 That the Council approves some or all the proposed changes to the traffic and parking bylaw database.

#### *Advantages*

- Improves the efficiency of public transport services and infrastructure by enabling enough capacity for buses and ensuring that they can safely enter and exit bus stops.
- Contributes to achieving an integrated, affordable, responsive, and effective transport network, by ensuring the right parking time restrictions and parking amenity are in place to support local communities and businesses.
- Improves the safety of the transport network by preventing obstructive parking, increasing visibility, and making existing parking controls clearer.

#### *Disadvantages*

- Costs of implementing the parking changes (e.g. installation of markings and signage).

### Option Two – Status Quo

- 64 Retain current parking restrictions without amendment.

#### *Advantages*

- The costs of implementing parking changes can be allocated elsewhere.

#### *Disadvantages*

- Does not improve the efficiency of public transport services and infrastructure.
- Does not meet the expressed parking needs of local communities and businesses, nor contribute to the goals of the Integrated Transport Strategy.
- Does not improve the safety of the network, nor reduce conflict points.

## NEXT STEPS

- 65 If Council adopts the recommended changes to parking restrictions, these will be implemented through appropriate signs and road markings and restrictions will be enforced under the Traffic and Parking Bylaw.
- 66 The traffic and parking bylaw database and Schedule 2 of the Traffic and Parking Bylaw will be updated following the Council meeting, and signs and markings will be scheduled for installation.

**Signatories**

Author:	Cr Cherry Lucas
Authoriser:	

**Attachments**

	<b>Title</b>	<b>Page</b>
<a href="#">↴A</a>	Attachment A - Minutes of 20 March 2026 Hearings Committee Meeting	177
<a href="#">↴B</a>	Attachment B - Minutes of 19 September 2023 Hearings Committee Meeting	182
<a href="#">↴C</a>	Attachment C - Minor General Parking Changes March 2026	185
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**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision enables democratic local decision making and action by, and on behalf of communities.  
This decision promotes the social well-being of communities in the present and for the future.  
This decision promotes the economic well-being of communities in the present and for the future.  
This decision promotes the environmental well-being of communities in the present and for the future.  
This decision promotes the cultural well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Parks and Recreation Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Other strategic projects/policies/plans	✓	<input type="checkbox"/>	<input type="checkbox"/>

Improvements to traffic and parking restrictions supports a safe, efficient and accessible transport network, and supports the social and economic wellbeing of Dunedin communities.

***Māori Impact Statement***

Mana whenua have expressed support for a safe and efficient transport network.

***Sustainability***

Parking control changes improve efficiency and access to the transport network, which contribute to sustainability goals.

***Zero carbon***

Parking changes proposed in St Clair will help to marginally reduce city emissions and vehicle circling in the area. DCC emissions would not be affected.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

There are no implications.

***Financial considerations***

Costs for implementing the proposed changes are covered by existing budgets.

***Significance***

The report is considered of low significance in terms of the Council’s Significance and Engagement Policy.

***Engagement – external***

There has been engagement with affected parties, including residents, landowners and business owners adjacent to changes. There has been engagement with Otago Regional Council with regard to the public transport network.

**SUMMARY OF CONSIDERATIONS**

***Engagement - internal***

There has been engagement with Transport, Property and Parks and Recreation Services staff. Consultation was undertaken with the i-Site from a local tourism perspective for the George Street changes. Transport staff will work with i-Site on implementing appropriate restrictions based on the time of year should the variable zone be approved. Parks and Recreation staff consider the changes for St Clair to be appropriate and beneficial for the area.

***Risks: Legal / Health and Safety etc.***

Many of the proposed changes aim to improve safety of vulnerable users of the transport network.

***Conflict of Interest***

There are no known conflicts of interest.

***Community Boards***

There are no implications for Community Boards as part of this report.

**Hearings Committee**  
**Proposed Parking Changes**  
**MINUTES**

Minutes of an ordinary meeting of the Hearings Committee held in the Council Chambers, Dunedin Public Art Gallery, The Octagon, Dunedin, on Friday 20 March 2026, commencing at 9.30 am

**PRESENT**

**Chairperson** Cr Cherry Lucas  
Cr Mandy Mayhem Cr Lee Vandervis

**IN ATTENDANCE**

Scott MacLean (General Manager City Services), Jeanine Benson (Group Manager Transport), Simon Spiers (Team Leader Regulation Management – Transport), Abbey Chamberlain (Senior Transport Planner – Transport), Ian Martin (Principal Advisor Road Safety – Transport), Rose Wunrow (Policy Analyst – Transport Regulation), Sacha Hatton (Transport Regulation Coordinator) and Jack Cowie (Otago Regional Council Senior Network Planner – Transport)

**Governance Support Officer** Rebecca Murray

**1 DECLARATIONS OF INTEREST**

Members were reminded of the need to stand aside from decision-making when a conflict arose between their role as an elected representative and any private or other external interest they might have.

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Committee:

- a) **Notes** the Elected Members' Interest Register; and
- b) **Confirms** the proposed management plan for Elected Members' Interests.

**Motion carried (HEAR/2026/001)**

## 2 SPEAKING SCHEDULE

A report from Governance provided a schedule of submitters who wished to present to the Committee.

The following submitters presented to the Committee:

### **Matthew Lane, Night 'n Day Foodstores Limited**

Matthew Lane raised concerns about a lack of short-term parking in the North Road and Eglinton Road areas and the impact this had on local businesses that rely on quick customer turnover. They supported increasing the availability of short-stay (P5) parking, noting that the current layout contributed to congestion and was difficult to enforce. Mr Lane considered the proposed changes would better support local users and businesses and responded to questions.

### **Lucy Gray**

Lucy Gray raised significant safety concerns related to illegal parking on the roadway, footpaths, and across driveways, which restricted visibility, pedestrian safety, property access, and could impede emergency vehicles in Fern Road. She supported yellow lines, signage, and stronger enforcement, noting ongoing issues associated with school-related parking and expressing willingness to participate in a community-led response involving the school and responded to questions.

### **Bronwyn Macgregor and Ben Wolken**

Bronwyn Macgregor and Ben Wolken raised concerns about the location of a new bus stop outside their property on North Road, advising it had created safety, visibility, noise, and privacy issues when exiting their garage due to buses, a cycle lane, and traffic movements. They noted ongoing bus idling despite assurances, early morning and late-night noise impacts, and loss of amenity, and questioned future compliance and consultation. Concerns were also raised about cyclist speeds, lack of warning signage, and the potential for a bus shelter to further obstruct sightlines. Ms Macgregor and Mr Wolken indicated they would be more comfortable if operational issues, particularly idling, were addressed in coordination with ORC, and supported exploring enforcement, signage, and community-based solutions and responded to questions.

## 3 PROPOSED PARKING CHANGES - MARCH 2026

A report from Transport presented information on the proposed changes and corrections to parking restrictions regulated by the Dunedin City Council.

The Group Manager Transport (Jeanine Benson), Team Leader Regulation Management – Transport (Simon Spiers) and Policy Analyst Transport Regulations (Rose Wunrow) spoke to the report and responded to questions.

Discussion was held on the proposed parking changes, noting:

### 1 Fern Road:

The Committee discussed traffic and parking issues in the Fern Road in relation to school activity. Members noted the need for staff to engage with the school to investigate the effectiveness of the current parking bay and the surrounding area. The possible introduction of a P5 drop-off area was discussed as a way to assist with school drop-off. Members also discussed directing school staff parking to Isabel Street and raised questions regarding a caravan potentially associated with the school, including whether it could be located on the school site.

164 Rattray Street:

The Committee discussed concerns about vehicle access arrangements at 164 Rattray Street. Members sought clarification on the process used to determine the appropriate number of entrances to a car park and expressed concern about the retrospective approval of such matters. It was suggested that this issue may be more appropriately considered through a DCC or Parking Hearings Committee process.

Macandrew Road near Port Otago:

The Committee discussed the proposal relating to Macandrew Road near Port Otago and agreed that further consultation was required before a decision could be made. Members indicated that the matter should return to the Committee following additional engagement.

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Committee:

**Adjourns** the meeting for 10 minutes.

**Motion carried**

The meeting adjourned at 11.39am and reconvened at 11.46am.

Discussion continued, noting:

164 Rattray Street:

Members discussed concerns that the current situation appeared to be retrospective in nature. It was noted that, prior to the Committee forming a view, it would be helpful to receive the engineer's report, particularly from a safety perspective. The Committee discussed requesting that an engineer undertake a further assessment of the site. Members noted that, subject to the outcome of that assessment, the removal of the yellow lines could be considered if a car park could be accommodated safely; if not, the existing restrictions would remain.

138 Princes Street:

Members discussed the possibility of trialling a bus stop layout in the area currently used for motorcycle parking and general parking spaces rather than removing the built out section. Questions were raised about how this could be implemented in practice and what impacts it may have on existing users.

Adjournment:

The Committee agreed that further information was required to inform its decision and adjourned the hearing, with the intention of reconvening within two weeks to allow staff time to carry out the necessary research and assessments.

Moved (Cr Cherry Lucas/Cr Mandy Mayhem):

That the Committee:

**Adjourns** the meeting.

**Motion carried**

The meeting adjourned on Friday, 20 March 2026 at 12.11pm and reconvened on Tuesday, 14 April 2026 at 9am.

DCC Transport and ORC Transport staff were present and responded to questions to assist the Committee's consideration.

Members discussed relevant provisions of the 2GP, including clause 6.6.3.2, and raised concerns about aspects of implementation, process awareness, and impacts on access and parking. Possible amendments through the 2GP to improve driveway access were also discussed.

Information was provided by ORC Transport staff regarding bus operations on Princes Street, including service frequency, peak-hour volumes, scheduling constraints, and the importance of the stop for passenger access to the Octagon. Discussion also included bus queuing, and observed congestion, as well as the limited ability to address these issues through timetable changes alone.

Potential options were discussed, including adjustments to the bus stop layout, retaining or modifying the existing build-out, relocation impacts, trialling alternative arrangements, and associated implications for safety, accessibility, funding, consultation requirements, and amenity. Staff outlined constraints, costs, and operational considerations.

Moved (Cr Mandy Mayhem/Cr Cherry Lucas):  
That the Committee:

- a) **Recommends** Council approve the proposed changes to parking and traffic restrictions shown in the March 2026 update of the Dunedin City Council's traffic and parking restrictions database found here: [March 2026 Proposed Parking Changes](#), which includes the proposed parking changes that have been updated by officers after consultation for 335A Kaikorai Valley Road, 194B Main South Road, and 233 Stuart Street, with the following amendments:

1 Fern Road

- Work with the school to investigate traffic and parking arrangements in Fern Road, including the existing parking bay, and to liaise with Parking Enforcement as required.
- Consider the installation of a P5 drop-off zone to assist with school drop-off activity.
- Request that school staff parking be directed to Isabel Street where practicable.
- Request that the caravan if associated with the school be parked within the school grounds.

164 Rattray Street

- Temporary approval of the removal of the car park space located between the car park entrance and exit, pending a proposed amendment to the 2GP, Rule 6.6.3.2, relating to the required lengths of clear view spaces, as requested by the Committee.

Macandrew Road near Port Otago

- That further consultation be undertaken and that the matter be reconsidered as part of a future parking changes hearing process.
- b) **Notes** that all parking restrictions previously approved by the Council remain unchanged.

**Motion carried (HEAR/2026/002)** with Cr Vandervis recording his vote against the 138 Princes Street change.

The meeting concluded at 9.56am.

CHAIRPERSON

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**Hearings Committee**

**MINUTES**

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**Minutes of an ordinary meeting of the Hearings Committee held in the Council Chambers, Dunedin Public Art Gallery, The Octagon, Dunedin, on Tuesday 19 September 2023, commencing at 1.34 pm - Proposed Parking Changes**

**PRESENT**

**Chairperson** Cr Jim O'Malley  
Cr Cherry Lucas

**IN ATTENDANCE** Jeanine Benson (Group Manager Transport).

**Governance Support Officer** Jennifer Lapham

---

**1 APOLOGY**

AN APOLOGY WAS RECEIVED FROM CR MAYHEM.

MOVED (CR JIM O'MALLEY/CR CHERRY LUCAS):

THAT THE COMMITTEE:

**ACCEPTS THE APOLOGY FROM CR MAYHEM**

**MOTION CARRIED**

**2 DECLARATIONS OF INTEREST**

Members were reminded of the need to stand aside from decision-making when a conflict arose between their role as an elected representative and any private or other external interest they might have.

Moved (Cr Jim O'Malley/Cr Cherry Lucas):

That the Committee:

- a) **Notes** the Elected Members' Interest Register; and

b) **Confirms** the proposed management plan for Elected Members' Interests.

**Motion carried (HEAR/2023/005)**

## **2 PROPOSED PARKING CHANGES**

A report from Transport presented information on proposed changes and corrections to parking and turning restriction for the Committee's consideration. It noted that consultation had been undertaken for the proposed changes.

The Group Manager, Transport (Jeanine Benson) spoke to the report and responded to questions.

Moved (Cr Jim O'Malley/Cr Cherry Lucas):

That the Committee:

**Recommends** Council to approve the proposed changes to parking and traffic restrictions shown in the September 2023 update of the Dunedin City Council's traffic and parking restrictions database, <https://tinyurl.com/ParkingSeptember2023>

**Motion carried (HEAR/2023/006)**

The meeting closed at 1.48 pm

## **CHAIRPERSON**



ATTACHMENT A – MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee   23 April 2026 Council Meeting					
#	Location	General Description	Change Proposed	Consultation	Map of Changes
1	8 Sturdee Street	Change of time restrictions following request from business to improve parking turnover	2x unrestricted car parks changed to P30 parks	Consultation 16 January - 15 February.	
2	335A Kaikorai Valley Road	Change of time restrictions following request from business to improve parking turnover	Convert two unrestricted car parks to P5 parks.	Consultation 16 January - 15 February. <b>Based on feedback received, officers have updated the proposed parking changes (reflected in map at right).</b> See report for more detail.	
3	27 North Road	Change of time restrictions following request from business to improve parking turnover	Convert two P30 car parks to P5 parks.	Consultation 16 January - 15 February.	

ATTACHMENT A – MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee   23 April 2026 Council Meeting					
#	Location	General Description	Change Proposed	Consultation	Map of Changes
4	1 Fern Road	Safety assessment and public enquiries revealed need for sections of no stopping lines	Install approximately 5 metres and 3 metres of no stopping lines.	Consultation 16 January - 15 February.	
5	194B Main South Road	Change of time restrictions following request from business to improve parking turnover	Convert five P15 car parks to P5 parks, and two P15 parks to two P10 parks.	Consultation 16 January - 15 February. <b>Based on feedback received, officers have updated the proposed parking changes (reflected in map at right).</b> See report for more detail.	

ATTACHMENT A – MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee   23 April 2026 Council Meeting					
#	Location	General Description	Change Proposed	Consultation	Map of Changes
6	31 Queens Gardens / High Street	Improvements to coach stop in front of Toitū Otago Settlers Museum to increase capacity of the stop	1x Pick Up Drop Off car park relocated further down High Street and coach stop extended over former Pick Up Drop Off location.	Consultation 16 January - 15 February.	
7	164 Rattray Street	Install NSL and add 1 paid P60 park	Install three sections of no stopping lines (removing four unrestricted parks) and convert one unrestricted park to one paid P60 park to align with surrounding restrictions.	Inform letter provided to surrounding businesses (no feedback received).	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
8	2 Broadway	Make temporary parking arrangements at 2 Broadway (which were in place during Otago Regional Council building construction) permanent changes, in light of positive support from surrounding area.	Change 2x paid P60 parks to one 2-hour Mobility car park and one P5 park.	Consultation 16 January - 15 February.	
9	20 Fox Street	Prevent vehicles from parking in two short sections (less than a standard 6 metre car park) adjacent to a business driveway, so driveway remains unobstructed.	Install two 4 metre sections of no stopping lines around 20 Fox Street driveway.	Consultation 16 January - 15 February.	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
10	71 Duncan Street	Removal of underutilised residents'-only car park	One residents'-only car park changed to an unrestricted car park.	Consultation 16 January - 15 February.	
11	32 Dundas Street	Removal of underutilised residents'-only car park	One residents'-only car park changed to an unrestricted car park.	Consultation 16 January - 15 February.	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
12	Opposite 2 Christie Street (near Miller Park)	Redundant no stopping lines are running along a newly installed parking bay.	Remove no stopping lines for new parking bay that provides four unrestricted car parks.	Consultation 16 January - 15 February.	<p>Remove redundant no stopping lines</p> <p>New parking bay (4x unrestricted car parks)</p>
13	138 Princes Street	Improvements to high-traffic bus stop to prevent buses from queuing out into the Dowling Street intersection and risking passenger safety.	Removal of kerb and motorcycle parking to extend bus stop, allowing more than one bus to pull into the stop.	Consultation 13 January - 10 February.	<p>Kerb buildout and motorcycle parking removed, and bus stop extended.</p>
14	15 Gladstone Road South Mosgiel	Change of time restrictions following request from business to improve parking turnover	Convert one unrestricted car park to one P5 car park.	Consultation 16 January - 15 February.	<p>1x unrestricted car park becomes 1x P5 car park</p> <p>No change to 1x existing P5 car park</p>

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
15	165 Eglinton Road	Change of time restrictions following request from business to improve parking turnover	Convert one Pick Up Drop Off park to a P5 park, and convert two P10 car parks to two P5 parks.	Consultation 16 January - 15 February.	
16	428 North Road	Permanently install the bus stop which has been in place temporarily since mid-2023	15 metres of unrestricted car parking changed to 1x bus stop.	Consultation completed by Otago Regional Council.	
17	Stuart Street and London Street intersection	Installation of no stopping lines to provide buses with safe and sufficient space to enter and exit the bus stop	<p>Approximately 6m unrestricted car parking changed to no stopping lines.</p> <p>Approximately 15m unrestricted car parking change to no stopping lines.</p>	No consultation required due to large road reserve as border.	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
18	115 Taieri Road	Installation of 6m no stopping lines at two bus stops to provide sufficient space for the bus to manoeuvre	<p>6m unrestricted car parking changed to no stopping lines for inbound bus stop exit taper.</p> <p>6m unrestricted car parking changed to no stopping lines for outbound bus stop exit taper.</p>	Consultation 16 January - 15 February.	
19	Between Sunnyvale Community Centre and sports field	Provide better parking access for visitors to the sports fields near the Sunnyvale Community Centre by making the unrestricted parking trial permanent	Remove no stopping lines and install 30x permanent unrestricted parks.	No further consultation required (consultation already completed through October 2025 by Parks & Recreation).	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
20	233 Stuart Street	Introduce no stopping lines to prevent further damage to historic veranda at 233 Stuart Street	Extend approx 12m NSL over 2x P5 parks, convert 1x P240 park to a P5, and shift 5x P40 spaces further up Stuart St (by approx 1 metre). Convert 1x P5 at the top of the parks to 1x P240.	Consultation 30 Jan - 22 Feb. <b>Based on feedback received, officers have updated the proposed parking changes (reflected in map at right).</b> See report for more detail.	
21	Musselburgh Rise and Shore Street intersection	With the trial roundabout becoming permanent, install no stopping lines to allow safe exit of the roundabout on Musselburgh Rise	Install 9 metres of no stopping lines (removing 1 unrestricted car park).	No further consultation required (feedback already received by Transport Safety officers in December 2025, no feedback provided on no stopping lines).	
22	228 Stuart Street	Removal of no stopping lines to make temporary coach stop permanent	Remove 40m of no stopping lines from within coach stop.	Inform letter provided to 228 Stuart Street (St. Paul's Cathedral) (no feedback received).	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
23	Across from 7 Otaki Street	Change of time restrictions based on safety risks of trucks and trailers parking on Otaki Street	Convert section of unrestricted parks (approx. 49.3 metres / 8 parks) to eight P120 parks based on safety concerns.	Inform letters provided to Mainfreight, Bunnings, Commercial Vehicle Centre, and Carters (no feedback received).	
24	20 Anzac Avenue (Railway Station)	Change of parking restrictions and markings to install mobility park near Changing Places bathroom	Instal 80 metres of no stopping lines, replace 4x private car parks with 1x mobility park, change 2x unrestricted parks to 2x P5 parks, and add a no marking section adjacent to the mobility park.	Broader consultation on the Changing Places project to install the bathroom completed as part of the 9-Year Plan, and inform letters provided to tenants previously using the private parks (noting that those 4x parks are no longer part of the tenants' lease).	

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#	Location	General Description	Change Proposed	Consultation	Map of Changes
25	<p>Macandrew Road near Port Otago</p> <p><b>[NOTE: This change has been deferred to the June Hearings Committee, and is no longer part of the changes recommended for Council approval at this meeting]</b></p>	Introduction of no stopping lines as a result of safety assessment.	Install 2x sections of no stopping lines (approximately 60 metres) (replacing 10x unrestricted car parks).	Inform letters provided to nearby properties.	



ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
<b>8 Sturdee Street</b>					
1276681	Lawrie Forbes	Zeal Steel Ltd.	Support	No	Good move to free up access to the trade supplier tenant. Lawrie Forbes Zeal Steel 14 Devon St Next door
<b>335A Kaikorai Valley Road</b>					
1277180	Justin Hobbs	Pedersen Read	Support	No	I work in an near by business and believe that there is ample unrestricted parking in the area and have no objection to a new 5 min carpark to help support this new business.
1278908	Amanda and Gareth Lea	Salmac Insulation	Support	No	We support the proposed parking changes at 335A Kaikorai Valley Road. These changes will help support a great local business and make access easier for customers.
1279814	Julie-Ann Ross	Design Arc Limited	Support	No	design arc Limited supports the proposed changes. However, we feel that there should be 2 P5 parks on either side of the Kai Mocha driveway - see attached file. Having a park on the other side of the driveway would make it easier for drivers to pull into the park & not slow down the traffic. The Kai Mocha cafe is a great asset to the local businesses around it and with plenty of unrestricted parking in the area there is no real impact to the community by having 2 P5 carparks outside the cafe. <b>[See attachment at the end of this document]</b>
1279815	Tony Ross	Pedersen Read	Support	No	Pedersen Read supports the proposed changes. It would be great to have 2 P5 parks as the drivers tend to be wanting to stop at the same time & if there isn't a park they won't stop & the cafe misses out on custom. There is plenty of unrestricted parking in the area there is no real impact to the community by having 2 P5 carparks outside the cafe.
1281029	Julie-Ann Ross	Jethro Developments Ltd.	Support	No	I support this proposed change & would also be happy with an additional P5 park on the other side of the Kai Mocha driveway so that there would be 2 P5 parks. Thanks
<b>27 North Road</b>					
1275406	Nicky Gibbs	Night 'n Day Foodstores Ltd.	Support	No	I am providing feedback as a new business coming into the area. Having short term parking will give us the ability to provide a new convenience option for locals, with parking right outside the property turning over at a much quicker rate than the current situation. Parking is at a premium at the Gardens, often seen outside the likes of Beam Me Up Bagels. Customers are in and out, usually with a very quick turnaround. To be able to firm up parking outside 27 North Road so that it works the same way would be wonderful.

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
1275458	Susie Gardyne	Orphans Aid International	Oppose	No	We would like it left 'as is' for the benefit of our customers and other shops in this area. I am the manager at Orphans Aid Op Shop, 21 North Road. We want people to be able to shop for longer than 5 minutes. I feel this will have a flow on effect to our shop if it changed to 5 minute parking.
1275462	Paul Lowe	Night 'n Day Foodstores Ltd.	Support	No	As a small business owner, I support the proposal to change the parking from 30 minute parking to 5 minute parking. The majority of customers stop briefly to make quick purchases, and shorter term parking encourages better turnover of spaces and improves access for these customers. I believe the proposed change will reduce long stay parking, make parking more efficient, and better reflect how the area is used on a day-to-day basis. Improved vehicle turnover will support local businesses and create a more accessible and convenient environment for customers. Introducing five minute parking will benefit the wider area by improving space turnover and making short stay parking more readily available. This change supports all local business businesses that rely on quick customer visits, reduces long-term vehicle occupation, and makes it easier for locals to stop in for essential errands. Faster parking turnover will improve access, reduce congestion caused by drivers searching for spaces, and contribute to a more efficient, welcoming, and pedestrian friendly shopping environment for everyone in the area.
1276048	Raewyn Alexander	Cat Rescue Charity Shop	Oppose	No	Cat Rescue Charity Shop will be adversely affected by the proposed change to two 5 minute parks. Customers stop in the current 30 minute ones to drop off donations, which always takes more than 5 minutes. We also have a number of disabled customers and customers with small children , who rely on using the parks outside 31/33 North Road to access the shop. If any change was made one of the parks should be made a Mobility Park. Because one new, as yet unestablished business, should dictate a change is unreasonable and unfair to the two businesses whose customers use these parks - Cat Rescue and the Fish and Chip Shop, both of which require longer parking spaces. Local businesses, such as ours, are not better accessed by a 5 minute park. As Manager/ Co-ordinator of the Cat Rescue Charity Shop this submission is to state very clearly that we oppose this proposed parking change as it would not work for us. We would lose customers, which as a Charity which receives no government or DCC funding, we cannot afford to have happen. Last year we had to raise funds towards over 640 rescued cats and kittens, as well as 900 desexing operations. Much of this money was raised through our shop customers.

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
1276723	Janine Ingham		Oppose	No	This area has a number of shops that require more than 5 minutes of parking eg second hand stores, coffee and bagels etc. Perhaps if there were more 30 minute parks available further up north road instead of the the street being full of vans from the electrical it would be ok to have a few 5 minute parks but unless that happens I do not support this.
1277388	Francisco Javier Araya		Oppose	No	To the Dunedin City Council, I am writing to formally voice my strong opposition to the proposed parking changes at 27 North Road. Replacing two 30-minute (P30) car parks with 5-minute (P5) spaces is a counterproductive move that ignores the practical realities of urban mobility and local commerce. A five-minute limit is effectively useless for anything beyond a literal "drop-and-run" scenario; it does not provide sufficient time for a driver to park, exit the vehicle, conduct even the simplest transaction, and return before risking a fine. By the time a parent unbuckles a child or an elderly resident safely exits their car, the clock has practically run out. Furthermore, these ultra-short-term parks create unnecessary traffic friction. Constant turnover in such a tight window increases the frequency of cars pulling in and out, which raises the risk of accidents for cyclists and pedestrians in an already busy area. It also discriminates against those with limited mobility who require more than five minutes simply to navigate the distance between their car and a shop entrance. While a new business may desire rapid turnover, the Council must prioritize the accessibility and safety of all road users over the convenience of a single commercial entity. I urge the Council to maintain the P30 designation, which strikes a far more reasonable balance between supporting business turnover and providing citizens enough time to actually use the services on North Road.
1277504	Gina Love		Oppose	No	I would support one park being changed to a 10 minute spot. Keeping the longer parks encourages shoppers to visit other shops in close proximity. 5 minutes would not allow this, just one quick pick up for which there are no shops you could visit in 5 minutes except perhaps the butcher. It seems like a dramatic response to one business owners request.
1277875	Pam Robertson		Oppose	No	I am disabled and it takes me more than 5 minutes to get out of my car let alone get to the fish and chip shop to get my order. I park there very regularly and I am getting rather angry over these decisions being made without thinking of less abled people. If this new business feels that people only need 5 minutes to pick up from them they obviously don't want disabled or elderly to be buying from them and there are a lot of elderly living down there as we won't be able to.

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
1280155	Anchorage Properties Ltd.	Anchorage Properties Ltd.	Support	No	Support these changes.
<b>1 Fern Road</b>					
1281040	Jason Gray		Support	No	Whilst in support, without adequate enforcement the proposed changes may prove futile. This is because of recurring observations of the disregard for parking laws in the vicinity, particularly around school drop off/pick up times. Unfortunately the risk to public safety increases when sight lines, safe manoeuvring space and access for traffic are all negatively affected by behaviours such as ignoring yellow lines and straddling the footpaths with vehicles. To mitigate this, the Transport Department might consider (in consultation with the school) restricted parking zones targeting drop off/pick up times of the day, or other measures to help everyone using the streets of this part of Maia safely (both pedestrian and road users). For residents, it is a source of frustration that parking frequently occurs on footpaths (with or without yellow lines) for protracted periods of time, blocking pedestrian right of way (especially those with wheelchairs, prams or walkers) and damaging kerbs and underlying pipes. This forces folk to walk on the road, an even more dangerous practice than usual at these busy times of the day.
1281044	Lucy Gray		Support	Yes	I support the changes as I am in favour of improved safety. Illegal parking in Fern Road is a significant source of frustration for me, and I am pleased to provide feedback. I seriously doubt the efficacy of the proposed action as there are already yellow dotted lines along parts of Fern road which are ignored daily. I have observed motorists parking by straddling the footpath and dotted yellow lines outside my house (sometimes facing the wrong way!) as well as either side of my driveway, and over the yellow dotted lines and footpath between my property at 2 Fern Road and 4 Fern Road. I have never seen any enforcement. I have left notes on windscreens and been ignored. I am not comfortable approaching drivers. When the lines were repainted at the end of last year, the next day I observed cars parking over these lines. At peak times cars can be parked both sides of my driveway (where there are existing dotted yellow lines) and I am unable to safely exit my property as I do not have clear line of sight up and down the street. Due to the parking on the footpath, the road is significantly narrowed which is a safety issue in the event of emergency services trying to get through, and there is no footpath for pedestrians, such as myself, to safely walk around the neighbourhood. Cars are often parked on the footpath on the Manapouri Street on my property boundary, resulting in damage to the footpath, and the water

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					main beneath it which has resulted in water entering our property. My husband has submitted a number of Fix-a-grams relating to this matter. Due to the footpath being obstructed, children and caregivers have to use the road, or the berm outside the 139 Manapouri Street, which is not suitable or designed for this purpose. For these reasons, while I am in favour of improved safety, I would like to make the following suggestions: Teachers park their vehicles in Isabel road. I rarely see cars parked in Isabel Road during the day, but there are always cars parked in Fern Road, outside the school. This would free up spaces for cars to park for pick-up/drop off Rudolph Steiner school, the DCC and the community work collaboratively on improvement to parking for the safe enjoyment of Fern Road during term time, and also when there is a school fair. Implementation of a time limit, like at other schools eg 5 minute drop off/pick-up. I regularly see cars parked illegally for more than half an hour indicating this is more than a simple pickup/drop off An enforcement programme implemented by the DCC to ensure that parking is monitored, and infringements are issued, and residents are able to safely enter and exit their properties. Improved signage about the legality and etiquette of parking in this area. <b>[See attachment at the end of this document]</b>
<b>194B Main South Road</b>					
1276070	Anna Yuen	Jackie's Takeaways	Oppose	No	To whom it may concern, I am writing as the daughter of the owners of Jackie's Takeaways and am making this submission on their behalf, as they have limited English proficiency. We wish to formally express our opposition to the proposed parking changes on Main South Road near 194B Main South Road. In particular, we are concerned about the proposal to change the existing P10 car park located directly outside our business to a P5 car park. This car park is essential to our operation and to our customers. Our business requires the cooking of raw ingredients, which means customers often need to wait more than five minutes for their orders. Unlike businesses that sell pre-prepared food, a five minute parking limit is not practical for our customers and does not reflect the nature of our service. We have already received multiple complaints from customers regarding this proposed change, many of whom have stated that five minutes is insufficient and would discourage them from stopping at our business. This is particularly concerning as this is the only car park directly accessible to Jackie's Takeaways. We also do not support the proposed change of the remaining seven car parks on the street from P15 to P5. These parking spaces are regularly used by our customers and reducing the time limit will further limit accessibility and negatively impact our small business. Retaining these parks as

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					P10 or P15 would be far more appropriate and would better support local businesses and their customers. Overall, we believe the proposed changes will have a detrimental effect on Jackie’s Takeaways and do not adequately consider the operational needs of small, family-owned food businesses. We respectfully request that the current parking time limits be retained. Thank you for considering our submission. Kind regards, Anna On behalf of the owners of Jackie’s Takeaways
1277125	Chris Willett		Oppose	Yes	<p>I have some questions regarding this parking change proposal.</p> <p>1/ how will it better meet the needs of the community and local businesses?</p> <p>2/ Who thought of this idea? Was it council staff? Or do you employ a parking infringement company who gave you the idea? 3/ how much is this little exercise, costing the rate payer? Me....</p> <p>4/ your letter of 16th January was unsigned.....but , I see was attributed to the whole of the Regulation Management Team? Who is the Boss of this Team?</p> <p>There are three businesses on that side of the road, not counting the police station, which has a car park. The park Outside the excellent Jackies Takeaway is one park, there is room for two if you allow the police, who are seldom in attendance, to only have one road access. Takeaways have fast food, but, 10 minutes is even faster!</p> <p>The Dairy has 99% car traffic. They are self service and takeaways, at certain times, usually associated with high traffic hours, there will be a line of people at the counter waiting to be served.</p> <p>Five minutes would allow a driver just enough time, to : wait until it is safe to open his door on a blind curve, get out of his car, walk to the shop door, walk to the counter, turn around and go back to his car without being able to purchase anything!</p> <p>Q/ How will that help local business?</p> <p>The other business is The Hairdresser next to the police station.</p> <p>It will be interesting to receive your answers to all of my questions.</p>

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					<p>I am only too pleased for you to supply my name and the unaltered contents of this email to: media and the public.</p> <p>On the face of your proposal, given the fact that, you have not given any tangible reason how it " Will better meet the needs of the community and local businesses" I can only assume that someone in your outfit had an idea how to make more revenue from parking infringements!</p> <p>Yours, very sincerely</p> <p>Chris Willett</p>
1277522	Antony Duthie	Night 'n Day Green Island	Support	Yes	The P15 definitely need's reduced to P5 to keep vehicles moving, which will help business. The P10 outside the fish and chip doesn't need replacing. The parking over the road needs reduced in limits to please to P5 and P30 to aid business and the bowling club with pick up and drop off of players.
1278242	Katie Little		Oppose	No	There is no way that I could be served in 5 mins at the Night n Day. Whenever I am there there are at least 3 other people I. There being served so there is a wait. Its ridiculous. There is no benefit to the community for proposed changes to the 15 min parks. Its actually detrimental. You would be punishing the businesses as well as if I got a ticket outside the night n day just because it took longer than 5 mins to get served i would stop going there. The only benefit i can see is to you. Doesn't help the community or the businesses. Being able to give tickets to more people helps you.
1278250	Sue Cunningham		Oppose	No	It takes longer than 5 minutes to get an ice cream for one person , let alone 3 people. First you have to get to the front of the queue of people waiting. Fish and chips takes 20 mins at the minimum . A hair cut can take 30 minutes minimum. Your organisation is out of touch with real living.
1278251	Gemma Todd		Oppose	No	Takes longer than 5 minutes to do anything at any of the shops here. Night and day as an example, if there are a couple people in line in front of you, you can be waiting for more than 5 minutes if they are getting ice creams or milkshakes
1278367	Karina Sidaway		Oppose	No	The current parking time of 15 mins outside the night'n'day and Jackie's Takeaways is a sensible amount of time. The proposed new parking time of only 5 minutes doesn't allow for there being more than one customer in the shop at a time. Ive had to wait more than 5 minutes to be served at the Night'n'Day most times Ive been there,

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					especially if we are ordering ice creams for our family for example, as these take time to make.
1279472	Trudy Cunningham		Oppose	No	Dont change the parking timezones in green island at all. Supporting our local community shops is important but you need the time to be able to do so.
1278169	Anonymous		Oppose	No	[REDACTED] why? How about you stop wasting rate payer money for a change? You realise there's almost always a car parked in one of those spots 24/7, and that's not going to change even with an adjacent police station (and [REDACTED] with bringing parking wardens into the equation) so this is a pointless waste of money.
1281235	Troy		Oppose	No	Notes from phone consultation: Calling on behalf of the owners of Jackie Takeaways. There is a request in at the moment from the night and day to change the P10 to a P5 outside their shops. The owner has spoken to Troy and asked him to ask us to not change it to the 5minutes as he cannot get the food cooked fast enough for that.
<b>2 Broadway</b>					
1276456	Paulien Leijnse	Broadway Medical Centre	Support	No	This change is absolutely in the best interests of the users/patients who visit the health services available both on Broadway and within 200m of Broadway. It enables drop-offs and pick-ups and ensures we have a mobility park available that does not require the user to cross any roads. We strongly agree with this change.
1276483	Paul Bennet	Toitu Investments	Support	No	Agree with proposed changes
1279211	Anita Rae	Physio Performance	Support	No	We are a business located at 8 Broadway, Physio Performance, and have been happy with the temporary changes and agree with the proposal for them to become permanent.
<b>20 Fox Street</b>					
1278253	Ian Hobson	Kido Hobson Ltd.	Oppose	No	Thanks for you letter 16.1.26. I disagree with the information supplied and having owned 18 Fox street for around twenty years without these two parks being of any issues over this period. To my knowledge my tenant nor the owners of 20 Fox street have not complained that thier is an issue with these two parks. Fox Street is an industrial area with bussiness clients and staff parking throughout the street, removal will result in 2 parks less (not correctly stated in your letter). I have attached a photo taken 27.1.26 with two larger vehicles parked in these parking areas and you can clearly see that they are not obstructing either 18 or 20 Fox St driveways. You mention that the areas concerned do not meet a standard DCC on-street parking space. Not knowing what your DCC specifications are, one would presume that

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Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					common sence should preveil and the parking space should remain as it has been for many years without change. <b>[See attachment at the end of this document]</b>
1278443	Clint Jeffery		Support	No	Great idea as the vehicles parked there obstruct the entry and exit into the building making it dangerous for customers coming in and out. Also the street at the back of this building (19 Reid road) is also very dangerous with similar issues around cars parking on under sized parks obstructing the driveway.
<b>138 Princes Street</b>					
1276257	Charlotte Peters		Support	No	I think it would be good to extend the bus stop area. Though it is sad to lose a tree in the area. Could potentially more motorcycle biking be put on the corner of Dowling St (by the steps) to offset the loss of motorcycle parks on Princes Street.
1276811	Kaila Dobson	Otago Regional Council	Support	No	<p>The Otago Regional Council (ORC) welcomes the opportunity to provide feedback on the proposed changes to the bus stop located at Princes Street, 138. ORC manages and operates the Otago public transport network and has a particular interest in ensuring that bus infrastructure supports safe, efficient, and reliable services.</p> <p>1. Health &amp; Safety Considerations ORC notes that the current configuration of the bus stop at Princes Street, 138, presents potential health and safety risks for drivers and passengers, including: • Insufficient space for buses to safely stop without impeding traffic flow. • Limited visibility for drivers merging in and out of the stop, particularly during peak periods. • Passengers boarding and alighting in close proximity to moving traffic, which may increase the risk of incidents. We strongly encourage that any redesign addresses these risks to improve driver and passenger safety. This is consistent with ORC’s commitment to the Health and Safety at Work Act 2015, and our broader operational safety obligations.</p> <p>2. Bus Stop Extension To support the efficiency and reliability of services on the Princes Street corridor, ORC recommends that the bus stop be extended to accommodate multiple buses at peak times. This extension would: • Allow buses to stop without blocking the main traffic lane, improving flow and reducing congestion. • Reduce dwell-time pressures on drivers, lowering stress and fatigue. • Enhance reliability for the network, particularly for services connecting with the central city and key interchanges.</p> <p>3. Alignment with Network Improvements Extending and upgrading the Princes</p>

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ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					<p>Street, 138 bus stop is critical for maintaining network efficiency, particularly given increased patronage on key routes. This stop is currently ranked 25th for busiest boardings in Dunedin, with approximately 30,000 boardings and 92,000 passenger disembarks per year, highlighting its importance within the public transport network. ORC supports infrastructure that enables smooth operations, timely services, and safe passenger access at high-use locations such as this.</p> <p>Conclusion ORC strongly supports measures that improve driver and passenger safety and enhance bus stop infrastructure to support the network, including the proposed extension of the Princes Street, 138 stop. We request that DCC consider these points in the final design and implementation of the project.</p> <p>Please do not hesitate to contact ORC’s Transport Operations Team if you require further information or clarification on our submission.</p>
1277960	Michelle Anderson	Pact	Support	No	Is often difficult using pedestrian crossing currently as buses in and out. Very busy space and will make things much better :)
1278491	Anna Leslie		Support	No	will definitely help with buses sitting in middle of dowling st princes st intersection or sitting over the pedestrian crossing areas while they wait for the bus in front of them to move off the stop. It happens daily.
1278763	Jeremy Lemmens	Jeremy Lemmens Peripheral Services	Oppose	No	<p>The proposed alteration pertains to a critical section of pedestrian infrastructure, specifically a footpath that significantly enhances pedestrian safety by creating a secure buffer from vehicular traffic. This location serves as a designated meeting point for myself and my associates, as well as for my child when we rendezvous in the city after their bus commute. It facilitates pedestrians in safely observing approaching traffic without encroaching upon the roadway.</p> <p>Furthermore, this vicinity hosts several essential public services, including a medical center and a dental practice, in addition to various dining establishments. A substantial number of individuals employed in the lower Princess Street and south of the exchange areas frequently utilize this location for lunch and dinner following their workdays, as well as for accessing the medical center. The area also accommodates motorcycle parking, addressing a notable deficiency in secure motorcycle parking options within the immediate vicinity.</p>

ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					A comprehensive upgrade for Princes Street, similar to the successful George Street project, is essential before allocating ratepayer funds to this area. Unlike international cities, urban traffic congestion is not a significant issue here, and the brief delay a bus experiences at a green light is a minor inconvenience, as traffic can readily flow around it. This was the rationale behind the original design of our city's intersections, featuring double lanes. The more recent implementation of single lanes, which do not effectively manage traffic volume, leads to extensive queues. This shift may be the primary cause of traffic congestion, as the removal of double through-lanes in favor of single through-lanes, often with an underutilized turning lane, forces vehicles into intersections before the light changes, thereby creating genuine traffic jams. Consequently, I cannot endorse these proposed changes without a complete upgrade of Princes Street, as they do not adequately address the needs of all road users, particularly pedestrians and those requiring access to critical services.
1280055	Dawn Stanton		Support	No	My partner and I use this BUs Stop every day to get to work and there is frequently another bus witing at the stop or behind out bus, these changes will make it safe for people getting on and off. My partner also has mobility issues and it will be helpful to him to know he has a safe space to get off the bus without feeling stressed about how far out from the pavement he is going to be.
<b>15 Gladstone Road South</b>					
1275467	Chris Henderson	Midway Motors	Support	No	I would add that this area is now fully utilised as a retail space, McGrath Benchtops, Spelt Bakery and ourselves Midway Motors. P 60 is needed for the remainder of this area. The parks are generally taken by all day parkes when there is plenty of parks available on the other side of the road. P60 for the remaining would allow the longer visitors to these retail business's to do their thing and move on without fear of overstaying. We also have many deliveries by truck who currently have to double park whilst unloading which is also Dangerous
<b>165 Eglinton Road</b>					
1277875	Matthew Lane	Night 'n Day Foodstores Ltd.	Support	Yes	Please see attached, suggested amendments to the proposed parking in the Mornington area. <b>[See attachment at the end of this document]</b>
1280162	Anchorage Properties Ltd.	Anchorage Properties Ltd.	Support	No	As per the attached submission should go further with adding more P-5 parks. <b>[See attachment at the end of this document]</b>

ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
<b>428 North Road</b>					
1283512 [feedback provided to ORC 12 June 2025]	Bronwyn Macgregor and Ben Wolken		Oppose	Yes	Dear Sumit Saini ( Implementation Advisor- Transport), This is our feedback regarding the proposed change of the bus stop at 428 North Road from Temporary to Permanent. We are adamantly opposed to this change for the following reasons. 1. Placing the bus stop next to our garage on the street will now mean it is dangerous for us to back out of our garage onto North Road. A bus stopped at 428 North Road blocks our vision of cyclists coming down North Road in the bike lane and also blocks our view of traffic turning left out of Birchfield Avenue onto North Road. If a car parks on the other side of our garage as well as the bus parked at 428 means we have to drive straight out, across North Road and into the lane of traffic coming North along the road as well. If we hit another car or cyclist this now becomes our liability. 2. There was no consultation prior to placing the temporary bus stop at 428 North Road with us or the owner of 428 North Road. We feel this was a rushed decision with no consideration for the people that would be affected the MOST by the change. 3. Since the buses have been parking outside 428 North Road our quality of life has been significantly affected. The noise of the bus is continuous and we are often woken by buses starting up their engines or idling for long periods before leaving the stop. The buses run from early morning 6.30am -11:30 pm at night every 15 minutes!! There are also often many more buses parked than are needed according to the timetable. 4. We have been told that the buses are supposed to park up at the Normanby terminus, and when it is time to leave, turn around and pick up people waiting at the bus stop at 428 North Road, then leave immediately. This clearly does not happen and we have had to continually ring Transport services to complain about buses waiting for long periods of time. Especially at evening time when they often sit at the bus stop for up to half an hour. 5. The bus stopping at 428 North Road, also infringes our privacy. Now everytime we walk out our front door, or even look out of it, there is a bus waiting in clear view. The bus driver and people on the bus can look straight up at us. We value our privacy and this has now been taken. Not to mention we have our garage used as a bus shelter for anyone who is standing at the bus stop. It is not their property NOR a bus shelter for the ORC. 6. I have lived at our property at 424 North for nearly all my life since I was six. The bus stop was at 440 North Road and there had never been a problem with it. A shelter is there for people getting on the bus. Why the change now? Nobody informed us! If it is because of the cyclists coming down North Road, you have now shifted liability to us. Is that fair??? We certainly do not think so!! If the

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ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					bus stop is no longer to be at 440 North Road, then it could be on the other side of the road at the Terminus. People could board there, and the next stop would be the one closest to Dalkeith Street. This would solve a lot of problems. The buses have always stopped at the Terminus, as they have to, to turn around and come back down North Road. There would be no need for another stop OR bus shelter outside 428 North Road. People from Mount Mera and Norwood Street could still board the bus at a convenient spot for them, ( the Terminus) and those from Birchfield Avenue could choose either the stop at the Terminus or the one near Dalkeith Street. Please consider our points of view. I think they are all very valid. Before you go any further, I think there would have to be a submission process where we can formally put forward our objections. We would like to be kept informed about this ( not at the last minute but in plenty of time to submit!). Yours sincerely Bronwyn Macgregor and Ben Wolken ( Home owners for 20 years at 424 North Road). <b>[See response from Otago Regional Council at the end of this document]</b>
<b>115 Taieri Road</b>					
1281108	Kaila Dobson	Otago Regional Council	Support	No	The Otago Regional Council (ORC) welcomes the opportunity to provide feedback on the Dunedin City Council’s proposal to install two sections of no stopping lines (approximately 6 metres each) in front of the inbound and outbound bus stops near 115 Taieri Road. ORC is responsible for planning, funding, and operating the Otago public transport network, and therefore has a direct interest in ensuring that bus stop infrastructure supports safe, efficient, and reliable services for passengers and drivers. <b>Support for the Proposal:</b> ORC supports the proposed installation of no stopping lines at both bus stops near 115 Taieri Road. From an operational perspective, the proposed changes will improve bus access to the kerb by providing sufficient clear space for buses to safely manoeuvre, pull in parallel to the kerb, and depart without obstruction. <b>Operational and Safety Benefits:</b> Clear kerbside space at bus stops is critical to the safe operation of the public transport network. Vehicles parking or stopping too close to bus stops can result in buses being unable to fully access the stop, requiring drivers to stop in the traffic lane or at an angle. This can create safety risks for: <ul style="list-style-type: none"> <li>• Bus drivers, due to constrained manoeuvring space and reduced sightlines</li> <li>• Passengers, particularly those boarding or alighting, including people with mobility impairments, prams, or limited vision</li> <li>• Other road users, due to buses stopping partially within the traffic lane</li> </ul> The proposed no stopping lines will help ensure buses can align correctly with the kerb, improving accessibility, reducing dwell times, and

ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					supporting more consistent and reliable services along Taieri Road. <b>Network Efficiency and Accessibility:</b> Reliable access to bus stops is essential for maintaining service efficiency across the wider network. Delays or operational difficulties at individual stops can have flow-on effects to service punctuality and passenger experience. ORC considers that the proposed changes will contribute positively to network performance while also improving the customer experience at these stops. <b>Conclusion:</b> ORC supports the proposed parking changes near 115 Taieri Road and considers them an appropriate and proportionate response to address current operational and safety issues at these bus stops. The installation of no stopping lines will enhance safety, accessibility, and efficiency for public transport operations and aligns with best practice for bus stop design. ORC appreciates the opportunity to provide feedback and looks forward to continued collaboration with Dunedin City Council to support a safe and effective public transport network.
1275769	Riana Da Silva		Oppose	No	The bus stop directly in front of 115 -117 Taieri Road, with the green bus shelter, is not at all affected by parked cars at 115-117 Taieri Road. There is sufficient room for any bus to stop BEFORE it reaches the green bus shelter. Parking is required for at least 3 vehicles at 115-117 Taieri Road, for the following reasons: Tenants / Ambulance / Taxis, family and friends picking up elderly getting around with walking aids.
<b>233 Stuart Street</b>					
1278894	Kacey Smith	Halo Hair Boutique	Support	No	We do support the changes it will be a shame to lose the 5min parking for the couriers outside our building Etc However if it protects the building than that is more important.
1281238	Property Owner		Support	No	Alternative recommendation: need somewhere for the couriers to drop off something so can there be a P5 on the uphill section to allow couriers to drop off things. <b>[See attachment at the end of this document]</b>
1282292	Fiona McCrimmon	McCrimmon Law	Oppose	No	1. We agree with the removal of the 2 P5 parks outside the historic verandah . 2. However if these 2P5 parks are removed we need two replacement P5 parks to cope with daily reasonable demand and use. 3. We propose that in addition to retaining the existing P5 park further up Stuart St that two current P240 parks are redesignated as P5 parks to retain the existing number of P5 parks. Ideally there should be one redesignated P240 park on each side of the road. It would work best if the P5 on our side of the road ( on LHS if travelling uphill) was located immediately uphill from the two P5 parks that are to be removed. 4. The current P240 parks are grossly abused on

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ATTACHMENT B – FEEDBACK RECEIVED ON MINOR GENERAL PARKING CHANGES (TPC-53)   20 March 2026 Hearings Committee					
Reference	Name	Org. (if applicable)	Support / Oppose	Speaking?	Comments
					<p>a daily basis by local residents parking their cars almost permanently on this stretch of road. This has persisted for a year or more. If that parking wasn't abused there would be less pressure on the current number of P240 parks, as these residents are taking those parks out of circulation almost permanently. 5. We need to retain a total of 3 x P5 parks in this stretch of road ( i.e retain the existing P5 and relocate , not lose, the two P5s that are to be taken away). This would provide for:- frequent courier vans for deliveries to businesses - including our law practice - on both sides of the road - people dropping in to our business who will often pop in briefly to arrange things and sign documents, it being rare that a P240 would be available to facilitate that - there are no disability parks in this block and the P5 enables elderly clients to be dropped off and picked up -due to the gradient of this stretch of road the P5s are well utilised by those with mobility issues. The P5 further up the hill is not well positioned for that use -due to the removal of DCC rubbish collections to businesses and residents in this stretch of road, and many separate rubbish collection agents, these trucks need to be able to stop to collect the various bins on various days -I also operate a flower business from my premises for locally grown flowers for which people need to be able to stop on a P5 near the historic verandah . -the P5 that is further up this stretch of road is not well positioned for any of the above purposes due to the 3 lanes of traffic at that intersection - and specifically is not well positioned for the elderly or those with mobility issues -To this end my business and other businesses and services need to retain the uphill P5 and have two current P240 parks immediately uphill from the current verandah parks redesignated as P5s. The only people who would be disadvantaged by that solution would be those currently abusing the P240 parks who are treating them as resident parking.</p>

**ATTACHMENTS TO SUBMISSIONS**

**335A Kaikorai Valley Road**

Attachment to Submission 1279814	Julie-Ann Ross

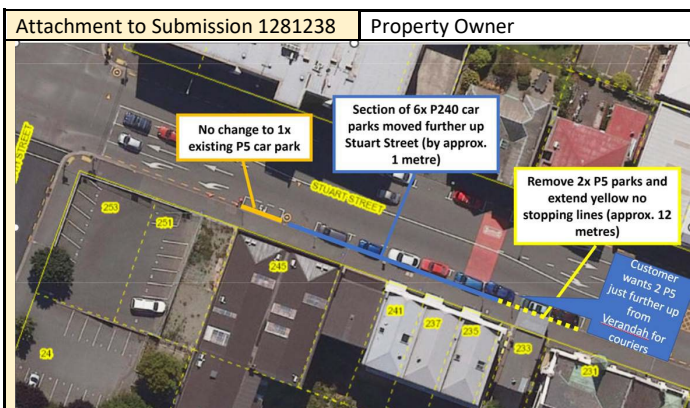
**1 Fern Road**

Attachment to Submission 1281044 [licence plate numbers have been removed for privacy reasons]	Lucy Gray

**20 Fox Street**



**233 Stuart Street**

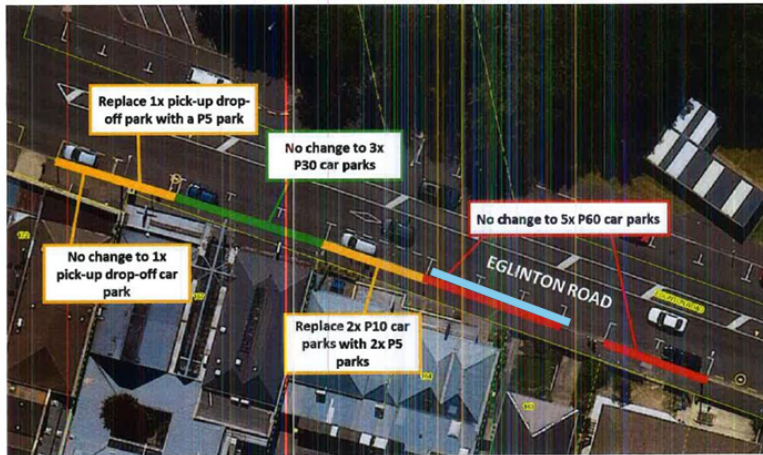


**165 Eglinton Road**

Attachment to Submission 1277875	Matthew Lane
Attachment to Submission 1280155	Anchorage Properties Limited
<i>See PDF file on next page – the two submissions listed above attached the same PDF file.</i>	

**Submission – changes of parking restrictions on Eglinton Road**

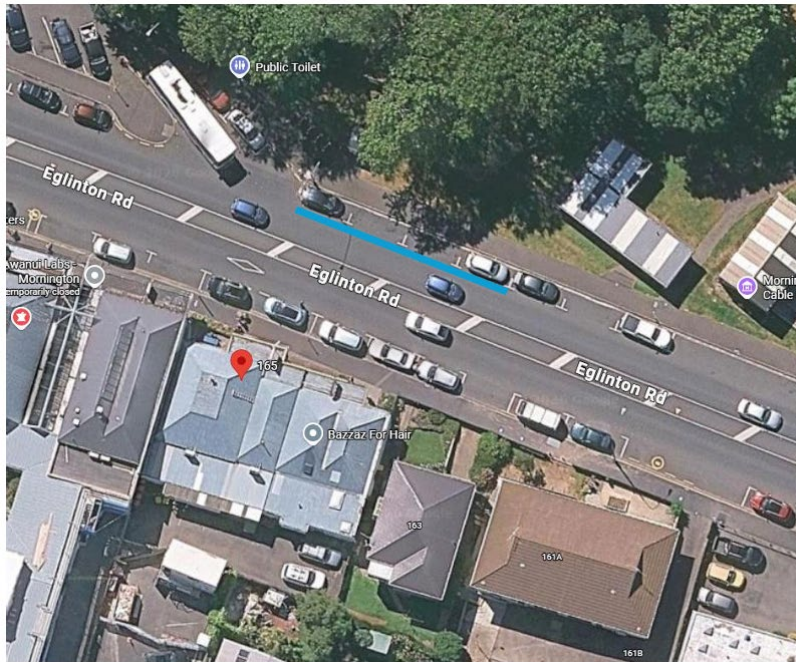
We support the proposed changes at 165 Eglinton Road. However, we believe this review could be extended to better support local businesses and, more importantly, the general public.



Consideration should be given to converting the newly added blue line to short-term parking. This change would significantly improve both safety and traffic flow on Eglinton Road. Under the current proposal, the 5-minute parks are positioned between 60-minute and 30-minute parking. When these longer-stay parks are occupied—often for extended periods—motorists are left with little option other than parallel parking.

By designating the blue line as short-term parking, drivers would be able to pull directly into available spaces. This would generally result in front car parks being utilised first, reducing uncertainty for following traffic regarding a vehicle’s intentions. In contrast, parallel parking often requires motorists to pull alongside an occupied space, and if the following vehicle does not allow sufficient room, the manoeuvre is interrupted. Traffic is then brought to a standstill while multiple attempts may be required to complete the park.

Furthermore, while changes are being considered in this area, strong consideration should be given to providing short term parking on the other side of the road. The following parks highlighted in blue (or part there of) would significantly improve traffic flow and public demand.



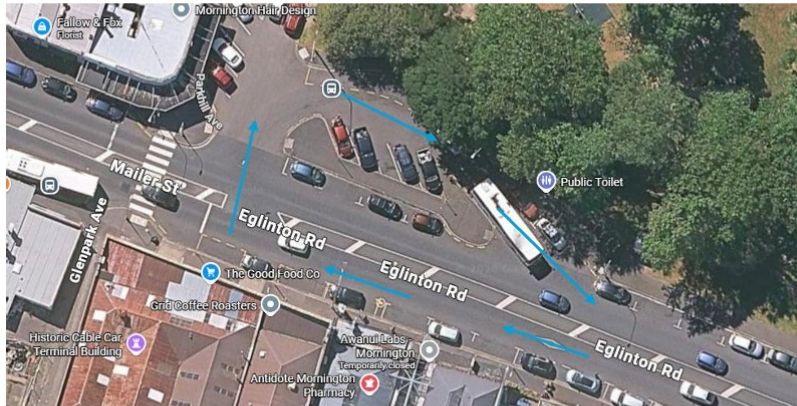
As with the suggestion above, this approach allows for a direct pull-in, pull-out parking arrangement. At present, there is no short-term parking on this side of the road, once again leaving motorists reliant on both available space and the need to parallel park.

The frequent lack of available parks—compounded by comparatively reduced parking enforcement in Mornington—means these spaces currently serve a limited number of motorists, often for extended periods, when they could instead be used by a much larger number of users on a more regular basis.

The current reliance on short-term parking located on the opposite side of the road creates negative traffic effects for vehicles travelling downhill on Eglinton Road. An example of typical motorist movements toward South Dunedin is outlined below:



Compounding this additional impact on traffic movement, to continue their journey the motorist then must turn right across the traffic, and then rejoin the road as per the following. Alternatively a U-turn is undertaken



Traffic can also be delayed when a bus is stopped at the bus stop. When adjacent car parks are occupied, it is not possible for vehicles to pass through, as the remaining gap is too narrow to safely accommodate an average-sized car. This results in traffic being held until the bus departs, further contributing to congestion and impeded traffic flow along Eglinton Road.

Mornington is a well-established commercial hub that has continued to grow over time. In recent years, new businesses such as Grid Coffee and Patties Ice Cream have opened, and alongside existing services including Night 'n Day and the local pharmacy, the area is characterised by predominantly short-term visit destinations. However, the current proportion of short-term car parks relative to longer-stay parking is unbalanced and does not align with the actual demand of the area.

The Mornington Night 'n Day alone catered to an average of 61 customers per hour during standard business hours in the week ending 25/01/2025, with the majority of customers arriving by car. When considered alongside other short-term businesses, the proposed parking changes do not adequately accommodate the public's need for short-term parking. This shortfall contributes to unnecessary congestion, increased safety risks, and overall dissatisfaction for motorists and pedestrians alike.

**EMAIL RESPONSE FROM OTAGO REGIONAL COUNCIL TO SUBMISSION ON 428 NORTH ROAD (1283512)**

**From:** Sumit Saini <Sumit.Saini@orc.govt.nz>  
**Sent:** Monday, October 20, 2025 8:06:52 AM  
**To:** [EMAIL REDACTED]  
**Subject:** Feedback regarding 428 North Road Bus stop- Bronwyn Macgregor, Ben Wolken

Good Afternoon Bronwyn and Ben,

I hope you had a good weekend and are keeping warm.

Thank you for taking the time to provide feedback regarding the proposed change at 428 North Road. I sincerely apologise for the delay in responding. We have been short-staffed, and I was also monitoring the Route 8 bus movement since the recent timetable adjustments. These changes mean drivers are no longer waiting for extended periods at North Road, and breaks are now being moved to St Clair Terminus.

I really appreciate your feedback and please see the response below to your concerns:-

1. Safety when exiting your property-  
Your safety is important to us. I will be discussing this matter further with DCC's road engineering team to ensure the shelter is positioned in a way that maintains visibility when you exit your garage. Installing a glass shelter closer to the intersection is one option we will be considering, and similar setups elsewhere in the city have worked successfully.
2. Consultation process-  
I apologise that the original installation of the temporary stop at 428 North Road was not informed, but I do not hold any information on what measures were taken at that time. The stop was introduced as an urgent interim measure because the previous stop at 440 North Road posed safety concerns for passengers, bus drivers and other road users.
3. Bus noise and idling-  
I acknowledge the disruption you experienced. Since 30 July, with the updated timetable, buses are no longer waiting or idling at the 428 North Road stop. They now stop only briefly to pick up passengers before continuing toward the city. Looking longer-term, we are working with operators to transition the fleet from diesel to electric by the end of 2026, which will further reduce noise impacts.
4. Buses waiting for extended periods-  
As above, this issue has been resolved with the new timetable. Drivers are no longer allowed to wait to take breaks at this location.
5. Privacy and passengers using your garage-  
I completely understand your concerns. A key reason we want to install a bus shelter is to provide passengers with a safe, designated waiting area, so they are not tempted to stand on or near your property. This will help preserve your privacy and ensure passengers remain in an appropriate space.
6. Alternative locations-  
While the former 440 North Road stop had historically been used, the current road layout makes it unsafe. Visibility is poor, and buses making U-turns after leaving the terminus cannot service the stop safely. The terminus also cannot accommodate a shelter due to space limitations, and the stop near Dalkeith Street is already at capacity, with passengers waiting outside the existing shelter. For these reasons, 428 North Road is the most viable and safe option.  
I do understand that there may occasionally be a short wait of a minute or two when a bus is at the stop as you exit your driveway. However, this is minimal compared to the broader safety and accessibility benefits for the community.

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Addition to all above, we have received strong support for making this stop permanent, particularly from older residents in the local area who rely on Route 8. The addition of a shelter at 428 North Road will provide them with safety, comfort, and accessibility that other nearby stops cannot currently offer.

I completely understand that this change has an impact on you, and I want to reassure you that your concerns are being taken into account. At the same time, providing safe and accessible public transport for the wider community is a responsibility we must balance.

Thank you again for providing us your feedback.

Have a Great Day!

Kind Regards,  
Sumit Saini



**Sumit Saini**

**Implementation Advisor - Transport**  
Transport

[MOBILE PHONE REDACTED] [Sumit.Saini@orc.govt.nz](mailto:Sumit.Saini@orc.govt.nz)

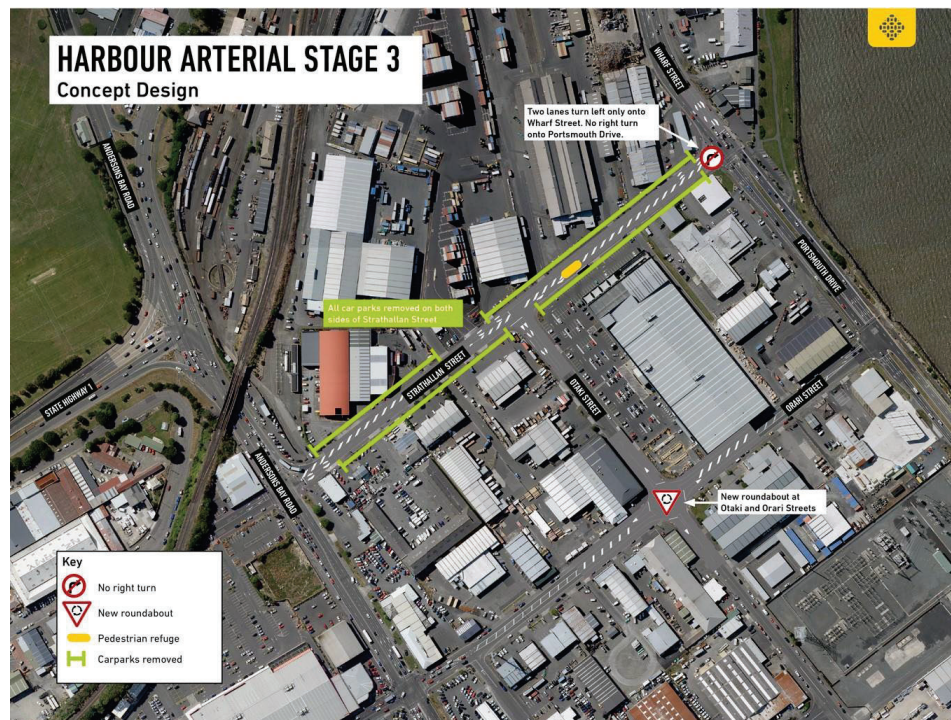
ATTACHMENT E – HARBOUR ARTERIAL STAGE 3 MINOR GENERAL PARKING CHANGES (TPC-38)   19 September 2023 Hearings Committee   23 April 2026 Council Meeting							
N		Cases			Parking counts		
		Location	Description	Current	Proposed	Consultation	
13	Harbour Arterial	Infrastructure & Layout change	Strathallan Street / Otaki Street intersection	<p>Change in road layout to facilitate Harbour Arterial Stage 3 (Strathallan Street).</p> <p>Removal of parking to support vehicle throughput.</p>	<p>Strathallan Street 2x P60 parks 296m of unrestricted space (48 parks).</p> <p>Otaki Street 27m of unrestricted space (5 parks)</p>	<p>Strathallan Street No stopping lines for the length of the street.</p> <p>Otaki Street No stopping lines at the intersection with Strathallan Street (~27m)</p>	<p>Consultation undertaken as part of Harbour Arterial business case for all stages.</p> <p>Consultation period: 1 August 2022 – 5 September 2022</p> <p>The consultation was undertaken using the following methods:</p> <ul style="list-style-type: none"> <li>• Social Pinpoint, where participants can provide comments on a map of the concept designs</li> <li>• Responding with written submissions via email or post</li> </ul>
14	Harbour Arterial	Infrastructure & Layout change	Otaki St/Orari St Roundabout	<p>Change in intersection layout from priority intersection to roundabout, to facilitate Harbour Arterial Stage 3 (Strathallan Street).</p>	<p>Unrestricted parking allowed up to intersection.</p> <p>6x P120 parks on southbound Orari St.</p>	<p>No stopping lines on all four legs of the Otaki/Orari intersection – loss of 15 unrestricted spaces.</p> <p>4x P120 parks replaced with broken yellow lines 1x P120 parks remain</p>	<p>Consultation undertaken as part of Harbour Arterial business case for all stages.</p> <p>Consultation period: 1 August 2022 – 5 September 2022</p> <p>The consultation was undertaken using the following methods:</p> <ul style="list-style-type: none"> <li>• Social Pinpoint, where participants can provide comments on a map of the concept designs</li> <li>• Responding with written submissions via email or post</li> </ul>

NOTE: The content in this attachment is a copy of information presented in the 19 September 2023 Hearings Committee order paper: [Agenda of Hearings Committee - Tuesday, 19 September 2023](#).





## Harbour Arterial Stage 3 Concept Design



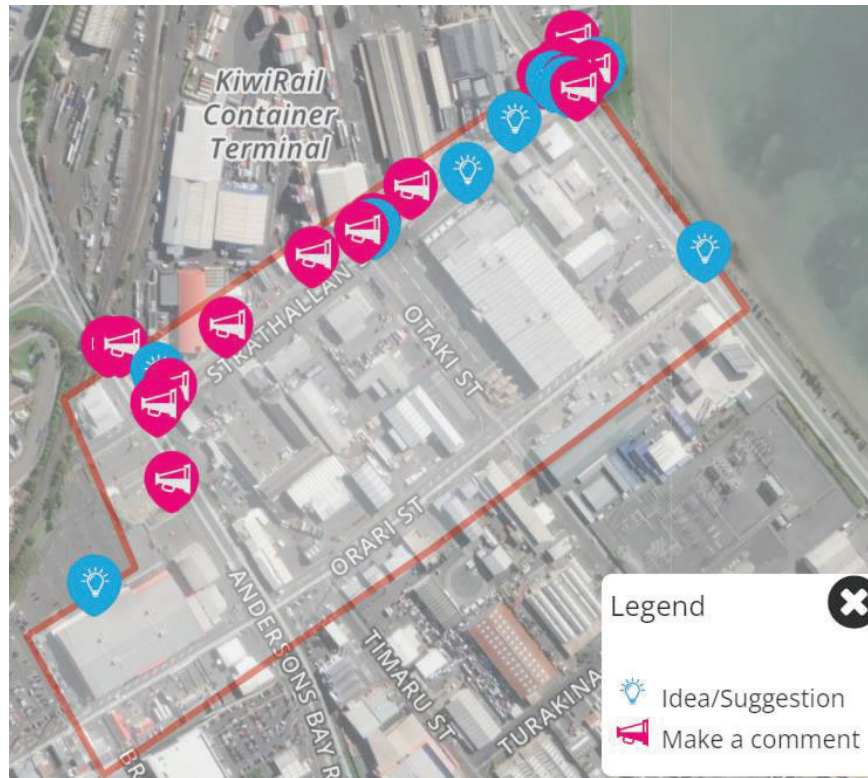
### Proposed measures

- Remove all on-road carparks on Strathallan Street to improve efficiency by allowing for longer approach lanes to signals.
- At the eastern end of Strathallan Street vehicles will be required to turn left onto Wharf Street. Turning right onto Portsmouth Drive will be prohibited.
- Construct a new roundabout at the Otaki/Orari Streets intersection.





## Harbour Arterial Stage 3 Social Pinpoint Results



- 177 total visits
- 17 different individuals in total provided comments
- 25 comments with a mixture of support/opposition for the proposed measures, suggestions for improvement or general comments

*Note: individuals can provide multiple comments.*

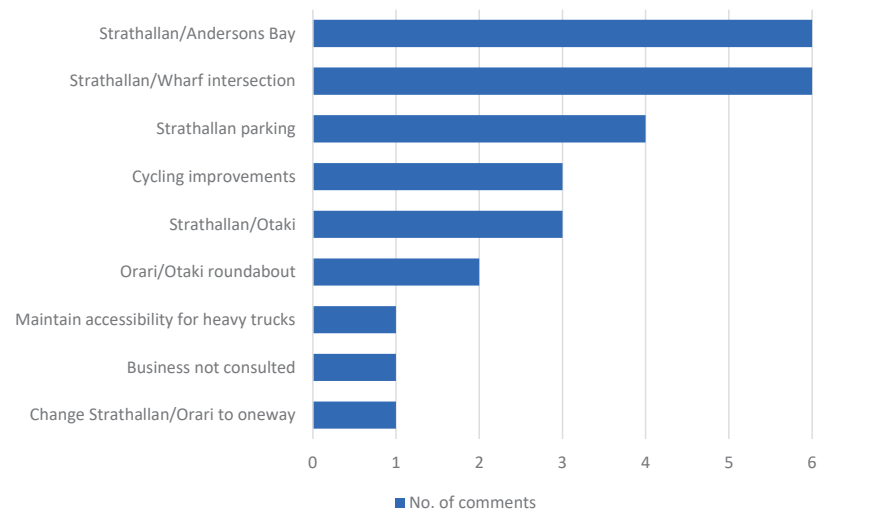




## Harbour Arterial Stage 3 Social Pinpoint Results

Out of the 25 comments provided, the top three topics raised were the Strathallan Street/Andersons Bay Road intersection, the Strathallan Street/Wharf Street intersection, and the parking on Strathallan Street. Other topics raised also included improvements for cycling, the Strathallan Street/Otaki Street intersection, and the Otaki Street/Orari Street roundabout.

Stage 3 Social Pinpoint Comment Themes





## Key observations from Social Pinpoint comments

Comments regarding the Strathallan Street/Andersons Bay Road intersection suggested:

- Additional right turn lane into Andersons Bay Road from Strathallan Street
- Provide priority to traffic turning into Andersons Bay Road from Strathallan Street
- Improving the signal phasing

Three respondents directly opposed the change to the Strathallan Street/Wharf Street intersection, citing negative impacts to local traffic and businesses who rely on this route. Additional improvements that were suggested include:

- Putting the traffic lights not too close to avoid damage from turning traffic
- Ensure cornering isn't too tight
- Pedestrian and wheelchair/scooter friendly
- Better lighting in the Kitchener Reserve

Four comments were received regarding the Strathallan Street carpark removal, with all opposing the change due to:

- Negative impacts for local workers and their need for accessible parking
- Limited alternative travel options to this area

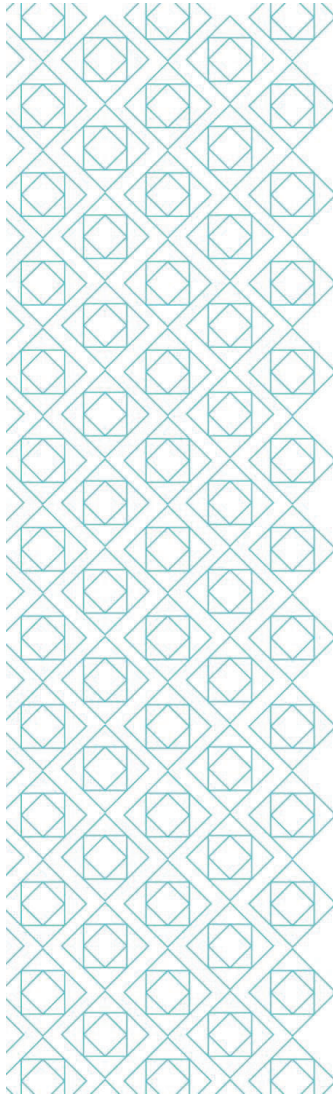
Comments regarding cycling improvements suggested:

- Creating separate cycleway to avoid traffic mingling
- Extend cycle lane from rail bridge to Orari Street/Portsmouth Drive
- Bike racks near the Warehouse

Comments regarding Orari Street and Otaki Street noted:

- Improvements needed at Strathallan Street/Otaki Street intersection to increase pedestrian safety
- Ensure safe turning for trucks at Strathallan Street/Otaki Street
- Roundabout usually not ideal for large trucks and trailers
- Rerouting HGVs into Orari Street/Otaki Street not ideal due to size





## Feedback from businesses and organisations

Feedback was received from five businesses on the Stage 3 concept design.

Two businesses expressed support for the roundabout at Otaki Street/Orari Street if it's made appropriate for heavy/oversize transport.

Three businesses expressed concern with removing the carparks on Strathallan Street if alternative parking is not provided. They noted their workers dependence on accessible parking.

Two businesses did not support removing RH turn at Strathallan Street/Wharf Street.

Other comments included:

- Noting the need for wide corners and traffic lights for truck movements, many commercial businesses needing the use of these roads in this area
- Maintain accessibility for businesses at the Strathallan Street/Wharf Street intersection
- Loss of Strathallan Street parking negatively impacts local businesses whose workers use these spaces to commute, need to have alternative local options

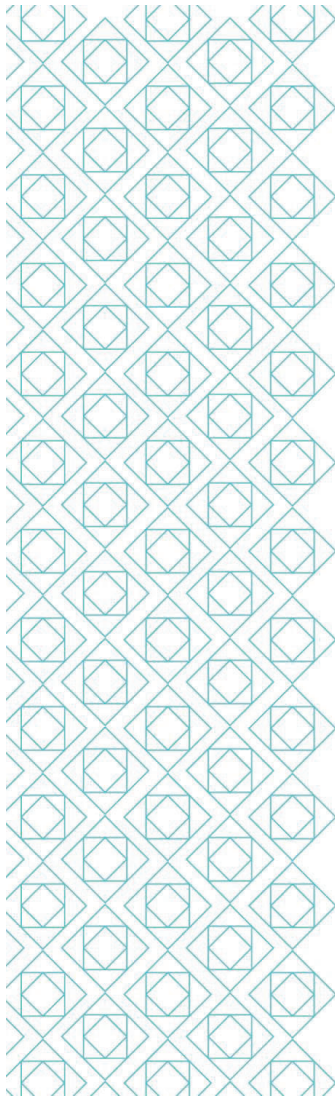
Feedback was also received from three organisations on the Stage 3 concept design.

The feedback was generally supportive of:

- The project overall
- Pedestrian refuge on Strathallan Street/Otaki Street if it complies with accessibility standards

Other comments included:

- Recommending accessibility audits for Strathallan Street/Wharf Street intersection and Strathallan Street/Andersons Bay Road intersection, with appropriate improvements made
- Installing audible traffic crossing for pedestrian refuge to allow safe crossing from mid-point
- Stronger vision for improving public transport and active travel, the current upgrades do not seem to contribute to this vision effectively
- Involving DPA and Access for All Group in all future planning prior to construction



## Other feedback received

Feedback was also received via email from three individuals. The feedback noted the following issues:

- Removal of Strathallan Street parking negatively impacts local workers, must be remediated with alternative parking
- Cycle lanes could be wider from car overbridge to Strathallan Street
- Connect cycle lanes via a new one on Strathallan Street
- Improve cycle safety on Andersons Bay Road, particularly under the railway bridge

SCHEDULE 2: TURNING RESTRICTIONS				
TURNING RESTRICTIONS				
* Turning restrictions that prohibit entry to a one way street do not show in this schedule as the restriction in those cases is already recorded under the one-way road restrictions' schedule.				
ROAD	SIGN TYPE	COMING FROM	NO TURN INTO	REASON
ANDERSONS BAY RD RIGHT (EAST)	No Right Turn	Andersons Bay Rd- Direction to the North	No entrance to Andersons Bay Rd opposite direction lane	Safety Measure-Turning Restriction
ANDERSONS BAY RD RIGHT (EAST)	No Right Turn	Andersons Bay Rd-Direction to the North	No entrance to Andersons Bay Rd opposite direction lane	Safety Measure-Turning Restriction
ANDERSONS BAY RD RIGHT (EAST)	No Right Turn	Andersons Bay Rd-Direction to the North	No entrance to Andersons Bay Rd opposite direction lane	Safety Measure-Turning Restriction
CANONGATE - LOWER (CEN)	No Right Turn	Canongate- Direction to Rattray	Rattray St	Safety Measure-Turning Restriction
CUMBERLAND ST O/B - MAIN SPAN (CEN)	No Right Turn	Cumberland Main Ramp-Direction to Cumberland St	Ramp up from SH1/Cumberland St	Safety Measure-Turning Restriction
CUMBERLAND ST O/B - MAIN SPAN (CEN)	No Right Turn	Cumberland Main Ramp-Direction to Wharf St	Ramp down to SH1/Cumberland St	Safety Measure-Turning Restriction
CUMBERLAND ST O/B - WHARF STH RAMP (CEN)	No Right Turn	Cumberland/Wharf South ramp-Coming Up from Wharf St-Direction to the North	Wharf North ramp Coming down to Wharf St. North	Safety Measure-Turning Restriction
FILLEUL ST (CEN)	No Left Turn	Filleul St-Direction London St	London St	Safety Measure-Turning Restriction
FITZROY ST (EAST)	No Right Turn	Fitzroy-Direction to Hillside Rd	Hillside Rd	Safety Measure-Turning Restriction
GEORGE ST (NORTH/CEN)	No Right Turn	George St-Direction to the Octagon	Upper Octagon	Safety Measure-Turning Restriction
GEORGE ST (NORTH/CEN)	No Right Turn	George St-Direction to Princes St	Lower part of to the Octagon	Safety Measure-Turning Restriction
GEORGE ST (NORTH/CEN)-CORNER BATH STREET	No Right Turn	George St Direction to Princes St	From Octagon to Bath St	Safety Measure-Turning Restriction
GREAT KING ST (CEN/NORTH)	No Right Turn	Coming out from car park	Opposite lane of Great King Street	Safety Measure-Turning Restriction
GREIG ST (EAST/R)	No Left Turn	Greig St-Direction to Portobello Rd	Portobello Rd	Safety Measure-Turning Restriction
HARROP ST (CEN)	No Right Turn	Harop St- Direction to The Octagon	Upper Octagon (Stuart St)	Safety Measure-Turning Restriction
HIGHGATE (WEST/CEN/NORTH)	No Right Turn	Hightgate-Direction to Stuart St	City Rd	Safety Measure-Turning Restriction
HILLSIDE RD (EAST)	No Right Turn	Hillside Rd-Direction to Andersons Bay Rd	Rankeilor St	Safety Measure-Turning Restriction
KAIKORAI VALLEY RD LEFT (CEN/WEST/GI)	No Right Turn	Kaikorai Valley Rd-Direction to Main South Rd	Main South Rd	Safety Measure-Turning Restriction
KOREMATA ST (GI)	No Right Turn	Koremata St-Direction to Main St Rd	Main South Rd	Safety Measure-Turning Restriction
MARION ST (EAST/R)	No Right Turn	Marion St-Direction to Portobello Rd	Portobello Rd	Safety Measure-Turning Restriction

SCHEDULE 2: TURNING RESTRICTIONS				
TURNING RESTRICTIONS				
* Turning restrictions that prohibit entry to a one way street do not show in this schedule as the restriction in those cases is already recorded under the one-way road restrictions' schedule.				
ROAD	SIGN TYPE	COMING FROM	NO TURN INTO	REASON
MELROSE ST (CEN)	No Left Turn	Melrose St-Direction to Littlebourne Rd/Queens Dr	Queens Dr	Safety Measure-Turning Restriction
MORAY PL (CEN)	No Right Turn	Coming out from Library car park	Moray Place-Direction to George St	Safety Measure-Turning Restriction
MORAY PL (CEN)	No Right Turn	Moray Pl or Burlington St-Direction to Princes St	Princes St-Direction to The Octagon	Safety Measure-Turning Restriction
MORAY PL (CEN)	No Left Turn	Moray Place-Direction to Filleul St	Library's garage Exit ramp	Safety Measure-Turning Restriction
OPOHO RD - LOWER (NORTH)	No Left Turn	Opoho Rd-Direction to Arden Street	Opoho Loop Rd	Safety Measure-Turning Restriction
PORTOBELLO RD (EAST-EAST/R)	No Right Turn	Portobello Rd- Direction to Peninsula	Mc Taggart St	Safety Measure-Turning Restriction
PRINCES ST (CEN/EAST)	No Right Turn	Moray Pl or Burlington St-Direction to Princes St	Princes St-Direction to The Octagon	Safety Measure-Turning Restriction
PRINCES ST (CEN/EAST)	No Right Turn	Princes St-Direction to Stuart St	Upper part of The Octagon	Safety Measure-Turning Restriction
QUARRY RD (GI)	No Right Turn	Quarry Rd-Direction to Main South Rd	Main South Rd	Safety Measure-Turning Restriction
QUEEN ST (NORTH)	No Left Turn	Queen St-Direction to Warrender St	Lachlan Ave/Warrender St	Safety Measure-Turning Restriction
QUEENS DR (CEN/NORTH)	No Left Turn	Queens Dr-Direction to Stuart St	Stuart St	Safety Measure-Turning Restriction
QUEENS DR (CEN/NORTH)	No Right Turn	Queens Dr-Direction Stuart St	Stuart St	Safety Measure-Turning Restriction
QUEENS DR (STK)	No Right Turn	Queens Dr-Direction to Andersons Bay Rd	Andersons Bay Rd	Safety Measure-Turning Restriction
QUEENS GARDENS CONNECTOR SH1 (CEN)	No Right Turn	Dowling St-Direction to SH1/Crawford St	SH1/Crawford St	Safety Measure-Turning Restriction
RANKEILOR ST (EAST)	No Right Turn	Rainkelor St-Direction to Hillside Rd	Hillside Rd	Safety Measure-Turning Restriction
RATTRAY ST (Corner with Dowling St)	No Left Turn	Rattray Street-Direction to Dowling St	Canongate	Safety Measure-Turning Restriction
ROYAL CRES (STK)	No Left Turn	Royal Cres-Direction to Rugby St	Rugby St	Safety Measure-Turning Restriction
ROYAL CRES (STK)	No Right Turn	Royal Cres-Direction to Rugby St	Rugby St	Safety Measure-Turning Restriction
STUART ST RIGHT (CEN)	No Right Turn	Stuart St-Direction Queens Dr	Queens Dr (Right side of Queens Dr)	Safety Measure-Turning Restriction
STRATHALLAN ST (EAST)	No Right Turn	Andersons Bay Road – Direction to Wharf Street	Portsmouth Drive	Safety Measure-Turning Restriction
THE OCTAGON (CEN) (On Princes Street)	No Right Turn	Princes St-Direction to the North	Lower part of the Octagon	Safety Measure-Turning Restriction

SCHEDULE 2: TURNING RESTRICTIONS				
TURNING RESTRICTIONS				
* Turning restrictions that prohibit entry to a one way street do not show in this schedule as the restriction in those cases is already recorded under the one-way road restrictions' schedule.				
ROAD	SIGN TYPE	COMING FROM	NO TURN INTO	REASON
THE OCTAGON (CEN) (On George Street)	No Right Turn	Princes St-Direction to the North	Lower part of The Octagon	Safety Measure-Turning Restriction
THE OCTAGON (CEN) (On George Street)	No Right Turn	George St-Direction to the South	Upper part of The Octagon	Safety Measure-Turning Restriction
THE OCTAGON (CEN) (On Princes Street)	No Right Turn	George St-Direction to the South	Upper part of The Octagon	Safety Measure-Turning Restriction
YORK PL (CEN)	No Right Turn	York Pl-Direction to Rattray St	Rattray St	Safety Measure-Turning Restriction

SCHEDULE 3: WEIGHT RESTRICTIONS, HEAVY TRAFFIC PROHIBITIONS AND TURNING MOVEMENTS BY SPECIFIED CLASSES OF VEHICLES			
*For Heavy Traffic categories please refer to Land Transport Rule: Vehicle Dimensions and Mass 2016			
WEIGHT RESTRICTIONS			
BRIDGE ON	RESTRICTION/PROHIBITION	APPROX. LOCATION (kms) from the start of the road	APPROX. LOCATION (m) from the start of the road
DONALDS HILL RD (WC)	Bridge shall not be crossed by 50Max or any vehicle that exceeds the posted axle limit of 8400kg (unless the vehicle holds a current overweight permit)	0.264	264
FARM RD EAST (TAI)	Bridge shall not be crossed by 50Max or any vehicle that exceeds the posted axle limit of 3000kg (unless the vehicle holds a current overweight permit)	0.655	650
HARRAWAY RD (GI)	Heavy Vehicle Speed shall not exceed 10km/hr when crossing bridge	0.08	80
HENLEY FERRY - HENLEY RD (TAI)	Bridge shall not be crossed by 50Max or any vehicle that exceeds the posted axle limit of 3000kg (unless the vehicle holds a current overweight permit)	6.075	6705
HINDON ROAD/RAIL	Heavy vehicles shall not exceed 15km/hr when crossing bridge	13.64	13640
HORSEBURN - HORSEBURN RD (ST)	Bridge shall not be crossed by 50Max or any vehicle that exceeds class 1 (Unless the vehicle holds a current overweight permit). Speed shall be limited to 15km/hr when crossing bridge	0.1	100
MCINTOSH ROAD- MCINTOSH RD (WC)	Bridge shall not be crossed by 50Max or any vehicle that exceeds the posted axle limit of 3000kg. (Unless the vehicle holds a current overweight permit). Speed shall be limited to 15km/hr when crossing bridge	0.323	323
PUKERANGI - PUKERANGI RD (ST)	Bridge shall not be crossed by 50Max vehicle or any vehicle exceeding class 1 unless the vehicle holds a current overweight permit	3.773	3773
ROSLYN OVERBRIDGE - HIGHGATE (WEST/CEN/NORTH)	Bridge shall not be crossed by 50Max or any vehicle exceeding class 1 unless the vehicle holds a current overweight permit	1.152	1152
WAIPORI VLY/WAIPORI-WAIPORI FALLS RD (TAI)	Bridge shall not be crossed by 50Max or any vehicle that exceeds class 1 (unless the vehicle holds a current overweight permit). Speed shall not exceed 10km/hr when crossing bridge	6.052	6052
HEAVY TRAFFIC PROHIBITIONS-Size Restrictions (Low Clearance)			
BRIDGE ON	RESTRICTION/PROHIBITION	APPROX. LOCATION (kms) from the start of the road	APPROX. LOCATION (m) from the start of the road
ANDERSONS BAY ROAD-RAIL OVERBRIDGE	Bridge clearance of 5.2m note must be checked by the operator prior to travel	0.085	85
CARNFORTH ST-RAIL OVERBRIDGE	Bridge clearance of 3.7m note must be checked by the operator prior to travel	0.17	170
CARNFORTH ST-SH 1 SOUTHERN M/WAY OVERBRIDGE, GREENSLAND	Bridge clearance of 3.6m note must be checked by the operator prior to travel	0.14	140
COAST ROAD-RAIL OVERBRIDGE EVANSDALE	Bridge clearance of 3.6m note must be checked by the operator prior to travel	0.398	398

SCHEDULE 3: WEIGHT RESTRICTIONS, HEAVY TRAFFIC PROHIBITIONS AND TURNING MOVEMENTS BY SPECIFIED CLASSES OF VEHICLES			
*For Heavy Traffic categories please refer to Land Transport Rule: Vehicle Dimensions and Mass 2016			
HEAVY TRAFFIC PROHIBITIONS-Size Restrictions (Low Clearance)			
BRIDGE ON	RESTRICTION/PROHIBITION	APPROX. LOCATION (kms) from the start of the road	APPROX. LOCATION (m) from the start of the road
GEORGE ST-PINEHILL RD / SH1	Bridge clearance of 5.2m note must be checked by the operator prior to travel	2.293	2293
GREAT KING ST-HOSPITAL OVERBRIDGE	Bridge clearance of 6.22m note must be checked by the operator prior to travel	0.548	548
KAIKORAI VALLEY RD-SH 1 SOUTHERN M/WAY OVERBRIDGE, CONCORD	Bridge clearance of 5.2m note must be checked by the operator prior to travel	4.864	4862
KAIKORAI VALLEY RD-STOCK YARD OVERBRIDGE	Bridge clearance of 5.1m note must be checked by the operator prior to travel	4.614	4614
KING EDWARD ST-RAIL OVERBRIDGE	Bridge clearance of 5.05m note must be checked by the operator prior to travel	0.287	287
MAIN SOUTH RD-SH 1 SOUTHERN M/WAY OVERBRIDGE, ABBOTSFORD	Bridge clearance of 5m note must be checked by the operator prior to travel	0.119	119
NEVILLE ST	Bridge clearance of 4.2m note must be checked by the operator prior to travel	0.012	12
PATERSON RD-RAIL OVERBRIDGE, WINGATUI	Bridge clearance of 3.8m note must be checked by the operator prior to travel	0.063	63
SADDLE HILL- SH1 FAIRFIELD M/WAY OVERBRIDGE	Bridge clearance of 5.38m note must be checked by the operator prior to travel	0.056	56
SOUTH RD-SH 1 SOUTHERN M/WAY OVERBRIDGE, GLEN	Bridge clearance of 6.4m note must be checked by the operator prior to travel	1.014	1014
STUART ST-HIGHGATE OVERBRIDGE	Bridge clearance of 5.2m note must be checked by the operator prior to travel	1.282	1282
VOGEL STREET--CUMBERLAND ST OVERBRIDGE JETTY ST	Bridge clearance of 3.0m note must be checked by the operator prior to travel	0.232	232
WHARF ST-CUMBERLAND ST OVERBRIDGE BIRCH ST NORTH BOUND LANE	Bridge clearance of 6m note must be checked by the operator prior to travel	0.703	703
WHARF ST-CUMBERLAND ST OVERBRIDGE BIRCH ST SOUTH BOUND LANE	Bridge clearance of 5m note must be checked by the operator prior to travel	0.705	705
WILKIE RD-RAIL OVERBRIDGE	Bridge clearance of 4.2m note must be checked by the operator prior to travel	0.005	5

<b>ATTACHMENT H – CORRECTIONS</b>		
<b>20 March 2026 Hearings Committee   23 April 2026 Council Meeting</b>		
<b>#</b>	<b>Location</b>	<b>Detail</b>
1	19 Timaru Street	Update bylaw layer to remove redundant no stopping lines
2	Dunedin City Library car park (230 Moray Place)	Update bylaw layer to reflect metered parking onsite
3	Dunbar Street	Update bylaw layer to remove redundant P240 restriction
4	95 Hanover Street	Update bylaw layer to reflect onsite P60 parks and remove redundant no stopping lines
5	103 York Place	Update bylaw layer to remove redundant Authorised Vehicles Only park
6	292 York Place	Update bylaw layer to remove redundant P30 restriction
7	670 Cumberland Street	Update bylaw layer to reflect P5 parks onsite
8	346 Great King Street	Update bylaw layer to correct no stopping line location
9	45 Brighton Road	Update bylaw layer to reflect correct bus stop location
10	Main South Road roundabouts	Update bylaw layer to reflect historic no stopping lines
11	South Road (184 Mornington Road)	Update bylaw layer to reflect historic no stopping lines
12	Henderson Street	Update bylaw layer to reflect onsite no stopping lines
13	129 Tomahawk Road	Update bylaw layer to reflect historic bus stop
14	165 Stuart Street	Update bylaw layer to reflect paid P60 parks
15	62 George Street	Update bylaw layer to reflect P5 hours of operation (At All Times)
16	1 Princes Street	Update bylaw layer to reflect P5 hours of operation (At All Times)
17	8 The Octagon	Update bylaw layer to reflect mobility park hours of operation (At All Times)
18	255 Great King Street	Update bylaw layer to reflect P30 hours of operation (At All Times)
19	110 Princes Street	Update bylaw layer to reflect P30 hours of operation (At All Times)
20	109-123 Princes Street	Update bylaw layer to reflect P30 hours of operation (At All Times)
21	168-192 Princes Street	Update bylaw layer to reflect P30 hours of operation (At All Times)
22	169 Princes Street	Update bylaw layer to reflect P30 hours of operation (At All Times)
23	82 Filleul Street	Update bylaw layer to reflect P5 hours of operation (At All Times)
24	Dalmore Loop (ORC)	Update bylaw layer to remove historic bus stops on Orbell, Falkirk, and Gladstone
25	Railway Station north car park	Update bylaw layer to reflect onsite no stopping lines
26	Liberton Loop (ORC)	Update bylaw layer to remove historic bus stops on Hislop, Pryde, and Wilkinson
27	229 Crawford Street	Update bylaw layer to remove motorcycle park and reflect correct onsite no stopping lines
28	41 Tennyson Street	Update bylaw layer to replace no stopping lines with 1x P240 to reflect correct onsite restriction

## **HEARINGS COMMITTEE RECOMMENDATIONS ON DANGEROUS, INSANITARY AND AFFECTED BUILDINGS POLICY REVIEW**

Department: Civic

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### **EXECUTIVE SUMMARY**

- 1 On 12 February 2026, the Council approved an amended Dangerous, Insanitary and Affected Buildings Policy (the Policy) and statement of proposal for consultation.
- 2 This report presents the recommendations of the Hearings Committee (the Committee) on the Policy following the review. It recommends minor changes that aim to update language and make the Policy easier to understand. See Attachment A for the amended Policy.

### **THE COMMITTEE CONSIDERED FOUR SUBMISSIONS AND HEARD FROM ONE SUBMITTER FOLLOWING CONSULTATION.**

### **RECOMMENDATIONS**

That the Council:

- a) **Notes** that the Hearings Committee has heard and considered submissions on the proposed Dangerous, Insanitary and Affected Buildings Policy
- b) **Notes** the minutes of the Hearings Committee (Attachment B)
- c) **Adopts** the amended Dangerous, Insanitary and Affected Buildings Policy (Attachment A)

### **BACKGROUND**

#### **Building Act 2004**

- 3 The Council is required under section 131 of the Building Act 2004 (the Act) to adopt a policy on dangerous and insanitary buildings. The policy must take into account affected buildings and it must state:
  - the approach that Dunedin City Council will take in performing its functions under the Act,
  - its priorities in performing these functions, and
  - how the policy will apply to heritage buildings.
- 4 Policies must be reviewed within five years of the policy being adopted and then at intervals of not more than five years. A policy does not cease to have effect because it is under review or

being reviewed. The special consultative procedure must be used if the policy is amended or replaced.

### **Dunedin's Policy**

- 5 The Council adopted its first Dangerous and Insanitary Building Policy in 2007. The most recent review in 2021 included affected buildings, and then it became the Dangerous, Insanitary and Affected Buildings Policy.
- 6 The Policy was developed in accordance with the purpose and principles of the Act which seek to ensure that:
  - people who use buildings can do so safely and without endangering their health,
  - buildings have attributes that contribute appropriately to the health, physical independence and wellbeing of people who use them, and
  - buildings are designed, constructed and able to be used in ways that promote sustainable development.

## **DISCUSSION**

### **Consultation**

- 7 Consultation was carried out from 18 February until 24 March 2026 using the special consultative procedure as required by the Act. Consultation was open via the DCC website and was advertised in the Otago Daily Times.

### **Results of consultation**

- 8 There were four submissions on this review with one submitter wishing to be heard.

#### *Overall agreement with proposed Policy*

- 9 Two submitters agreed overall with the proposed Policy, one did not agree, and one did not answer this question. All submitters made comments or suggestions on the proposed Policy.

#### *Specific feedback*

- 10 One submitter would like greater Council scrutiny over residential premises, citing the Health Act 1956 as well as other legislation. This submitter was heard at the hearing.
- 11 A submission from Fire and Emergency New Zealand (FENZ) commented that the Policy is simple and clear but noted that the word "ordinary" is missing from the definition of Dangerous buildings.
- 12 Another submitter asked for penalties for owners of derelict and insanitary buildings.
- 13 Heritage New Zealand Pouhere Taonga (HNZPT) commented that it is generally supportive of the Council's commitment in the proposed Policy to take into account the heritage values of the building in determining possible courses of action. It recommended including additional information and clarification on certain points relating to heritage buildings and archaeological sites.

**Committee recommendations**

- 14 The Committee met on 30 March 2026 to consider submissions, hear from one submitter, and deliberate. Deliberations included matters both within the scope of this Policy and outside its scope.
- 15 Following deliberations, the Committee recommends that the Council adopts all of the proposed changes to the Policy that were consulted on, as well as two other amendments.

*Summary of recommended changes*

- 16 The Committee recommends that the following proposed changes are adopted:

Section	Proposed change	Reason
<b>Policy history</b>		
Policy history	Adding the history of the policy including dates of adoption and review.	Greater clarity and in response to audit.
<b>Definitions</b>		
Heritage definition	Updating this definition.	To align with updated terminology.
<b>Section 1: Taking action on dangerous, insanitary and affected buildings</b>		
Section 1.3	Additional section on how Council may respond to a dangerous, insanitary or affected building including examples.	To provide greater clarity.
<b>Sections 2 and 3: Heritage buildings</b>		
Sections 2 & 3	Providing additional explanatory information for buildings that are also heritage buildings.	To provide greater clarity and information for owners of these buildings.
Section 2.6	Adding that the Council will notify New Zealand Heritage Pouhere Taonga (NZHPT) of any notice requiring building work or restricting entry to a building that is included on the NZHPT list.	To be clear about this requirement of the Building Act 2004.
<b>Section 6: Review</b>		
Review	Adding reference to the special consultative procedure that is required when this policy is amended or replaced.	To be clear about this requirement of the Building Act 2004.

- 17 Following consultation and deliberations, the Committee also recommends:
  - adding the word “ordinary” to the definition of *Dangerous buildings* to accurately reflect this definition in the Act as it had been inadvertently omitted. The FENZ submission pointed out this omission.

- adding the following words to 2.4 for greater clarification: “Any proposed work affecting buildings or areas subject to heritage orders or encumbrances (e.g. a heritage covenant), may require additional approvals”.

## OPTIONS

- 18 Options are to accept the Committee’s recommendations or to refer the Policy back to the Committee for further consideration.

### Option One – Accept recommendations of the Committee (Recommended)

- 19 This option is to approve the amended Policy, as described.

#### Advantages

- Clarifies and simplifies the Policy.
- Meets requirements of the Act.

#### Disadvantages

- No identified disadvantages.

### Option Two – Refer the Policy back to the Committee for consideration

- 20 If the Council does not accept the recommendations of the Committee, this option is for the matter to be referred back to the Committee for further consideration.

#### Advantages

- The Committee has further opportunity to consider changes to the Policy.

#### Disadvantages

- Opportunity to clarify the Policy may be lost.

## NEXT STEPS

- 21 If the Council accepts the Committee’s recommendations, the amended Policy will be sent to the Chief Executive of the Ministry of Business, Innovation and Employment, as required by the Act. The Dunedin City Council website and internal systems will also be updated to reflect the amended Policy.

## Signatories

Author:	Cr Lee Vandervis - Chairperson
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## Attachments

	Title	Page
<a href="#">↓A</a>	Amended Dangerous, Insanitary and Affected Buildings Policy	239
<a href="#">↓B</a>	Hearings Committee Minutes	244

**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision enables democratic local decision making and action by, and on behalf of communities. This decision promotes the social and economic well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Other strategic projects/policies/plans	<input type="checkbox"/>	<input type="checkbox"/>	✓

This policy review contributes to the priorities of healthy and safe people within the Social Wellbeing Strategy, and compelling destination within the Economic Development Strategy.

***Māori Impact Statement***

No specific impacts for mana whenua have been identified. However, in recognition of the Autūroa and Autakata pou in Te Taki Haruru, the two rūnaka were advised of the review and given the opportunity to provide feedback.

***Sustainability***

There are no specific implications for sustainability.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

There are no implications for these documents.

***Financial considerations***

There are no financial implications.

***Significance***

This review is considered low in terms of the Council’s Significance and Engagement Policy.

***Engagement – external***

The special consultative procedure was used to review the Policy with consultation open on the DCC website for at least one month. The review was advertised in the Otago Daily Times.

***Engagement - internal***

There has been internal engagement with Governance and Regulatory Services staff, and the Heritage team.

**SUMMARY OF CONSIDERATIONS**

***Risks: Legal / Health and Safety etc.***


There are no identified risks.

***Conflict of Interest***

No conflict of interest has been identified.

***Community Boards***

There are no specific implications for Community Boards.

<b>DANGEROUS, INSANITARY AND AFFECTED BUILDINGS POLICY</b>		 <b>DUNEDIN</b>   kaunihera CITY COUNCIL   a-rohe o Ōtepoti	
<b>Approved by:</b>	Council		
<b>Sponsor:</b>	General Manager Corporate and Regulatory		
<b>Department responsible:</b>	Building Services		
<b>Date adopted:</b>	January 2007	<b>Reviewed:</b>	17 April 2018
			25 May 2021
			23 April 2026
<b>Next review date:</b>	By 23 April 2031	<b>DOC ID:</b>	

## BACKGROUND

The Building Act 2004 requires territorial authorities to have a policy on dangerous, insanitary and affected buildings. The Dunedin City Council (“the Council”) recognises that provisions of the Building Act in regard to dangerous, insanitary and affected buildings reflect the Government’s broader concern with the health and safety of the public in buildings. The Council understands that the development of these policies is the responsibility of each territorial authority and has responded accordingly.

The policy has been developed in accordance with the purpose and principles of the Building Act 2004 which seeks to ensure that:

- people who use buildings can do so safely and without endangering their health;
- buildings have attributes that contribute appropriately to the health, physical independence and well-being of people who use them; and
- buildings are designed, constructed, and able to be used in ways that promote sustainable development.

The Council is committed to ensuring that Dunedin is a safe and healthy place to live. The Building Act provides the means to ensure buildings that become dangerous, insanitary or affected are managed in a timely manner to remove the danger and fix the insanitary conditions. The Council will administer the Building Act in a fair and reasonable way.

## DEFINITIONS

### Dangerous Buildings

Under section 121 of the Building Act 2004, a building is dangerous if:

- a) in the ordinary course of events (excluding the occurrence of an earthquake), if the building is likely to cause—
  - (i) injury or death (whether by collapse or otherwise) to any persons in it or to persons on other property; or

(ii) damage to other property; or

b) in the event of fire, injury or death to any persons in the building or to persons on other property is likely because of fire hazard or the occupancy of the building.

#### **Affected Buildings**

Under section 121A a building is an affected building for the purposes of this Act if it is adjacent to, adjoining, or nearby—

- a) a dangerous building as defined in section 121; or
- b) a dangerous dam within the meaning of section 153.

#### **Insanitary Buildings**

Under section 123 of the Building Act 2004, a building is insanitary if it is:

- a) offensive or likely to be injurious to health because—
  - (i) of how it is situated or constructed; or
  - (ii) it is in a state of disrepair; or
- b) has insufficient or defective provisions against moisture penetration so as to cause dampness in the building or in any adjoining building; or
- c) does not have a supply of potable water that is adequate for its intended use; or
- d) does not have sanitary facilities that are adequate for its intended use.

#### **Heritage Buildings**

Heritage buildings are considered to be buildings or structures that are—

- a) identified on the New Zealand Heritage Pouhere Taonga List;
- b) 'scheduled heritage buildings' or 'scheduled heritage structures' in the Dunedin City District Plan; or
- c) are 'character contributing buildings' located within a Heritage Precincts in the Dunedin City District Plan.

#### **POLICY**

Dangerous, insanitary and affected buildings will be dealt with by responding to complaints received from the public, advice received from Fire and Emergency New Zealand, New Zealand Police, social service agencies, a building tenant or other agency/department, and working with building owners to address the problem without delay.

When a building has been assessed as being either dangerous, insanitary or affected in terms of sections 121, 121A and 123 of the Building Act appropriate action will be taken.

## **1. Taking action on dangerous, insanitary and affected buildings**

- 1.1. On being satisfied that a building is dangerous, insanitary or affected, the Council will advise and liaise with the owner, where possible, to discuss action to be taken. If notification was received from Fire and Emergency New Zealand that the building was dangerous, it will liaise with Fire and Emergency New Zealand and the owner, where possible, to discuss the proposed action. If the building is a heritage building the Council will take into account its heritage values in determining a course of action, as set out in section 2 of this policy.
- 1.2. If the Council is satisfied that a building is dangerous, insanitary or affected and the building owner does not meet the requirements imposed on them as the owner, the Council may exercise any or all of its powers under sections 124-130 of the Building Act which include:
  - a) Installing hoardings or fences to prevent people from approaching the building nearer than is safe
  - b) Attaching notices warning people not to approach the building
  - c) Giving written notice requiring work to be carried out so that the building will not be dangerous, insanitary or affected within a stated time period
  - d) Initiating prosecution if buildings are used after notices or hoardings are in place
  - e) Initiating prosecution where there is failure to comply with the notice
  - f) Applying to a District Court to carry out the work to remove the danger or so that the building is no longer insanitary, or demolish, where the work is not completed or is not proceeding with reasonable speed
  - g) If immediate action is necessary to strengthen or fix dangerous, insanitary or affected conditions the Council may take that immediate action and in some cases may need to apply to a District Court to confirm the action
  - h) Recovering all costs involved from owner
  - i) Placing a charge on the land until the Council recovers the costs
- 1.3. Before exercising these powers, the Council will seek to meet with owners to discuss proposals to address the issue. The Council will also seek to meet with owners to discuss proposals to comply with a notice following issue of a formal notice. Notwithstanding this, the Council, as a responsible authority, will issue notices or take other actions which are reasonably required to protect the building occupants or members of the public from injury or death, or to protect damage to other property.
- 1.4. Building consent or certificate of acceptance may be required for certain alterations or demolition of a building.

## **2. When a dangerous, insanitary or affected building is also a heritage building**

- 2.1. When considering what action to take on heritage buildings that have become dangerous, insanitary or affected, the Council will take into account the heritage values of the building in determining possible courses of action.
- 2.2. For heritage buildings, Council's position is to avoid demolition, or removal of significant architectural features, wherever possible.
- 2.3. However, it may be that the dangerous or insanitary part of the building can be removed without affecting the overall heritage value of the place. Council's heritage advisors can provide advice about the heritage values of the place. An owner may engage suitably qualified professionals with heritage expertise to advise and recommend actions.
- 2.4. In addition to building consent or certificate of acceptance, a resource consent may also be required for alterations, partial demolition, or demolition of heritage buildings. Any proposed work affecting buildings or areas subject to heritage orders or encumbrances (for example a heritage covenant), may require additional approvals.
- 2.5. On any matters concerning demolition, sufficient information to support this course of action will need to be supplied to the Council. Council may choose to engage a suitably qualified professional to undertake a peer review of information provided to Council.
- 2.6. If the building is included on the New Zealand Heritage Pouhere Taonga List, the Council will notify New Zealand Heritage Pouhere Taonga of any notice requiring building work or restricting entry to that building.
- 2.7. Demolition of buildings constructed prior to 1900 is likely to trigger the archaeological provisions of the Heritage New Zealand Pouhere Taonga Act 2014. Early consultation with Heritage New Zealand is advised.

### **3. Financial assistance for heritage buildings**

- 3.1. Dunedin City Council administers the Dunedin Heritage Fund to support building owners with the conservation and retention of heritage buildings across the city. Work on a dangerous or insanitary heritage building may be eligible for an emergency application if the work is urgent. Owners are advised to consult with the Council's heritage advisor about the Dunedin Heritage Fund.

### **4. Health Act 1956**

- 4.1. Provisions also exist in the Health Act 1956 to deal with nuisance conditions related to certain matters associated with housing [under section 29(f)], overcrowding likely to be injurious to health, and under section 42, insanitary conditions likely to cause injury to the health of persons, or a dwelling that is otherwise unfit for human habitation.

### **5. Disputes**

- 5.1. If a building owner disputes the Council's decision, or proposed decision, or any other matter relating to the exercise of the Council's powers under sections 124 to 130 of the Building Act relating to dangerous, insanitary or affected buildings, they may apply for a determination

from the Chief Executive of the Ministry of Business, Innovation and Employment, as set out in the Building Act. Such a determination is binding on both parties.

## 6. Review

- 6.1. Any review, amendment or replacement of this policy must be in accordance with the provisions of the Local Government Act 2002, including the special consultative procedure outlined in section 83. This policy will not cease to have effect if it is due for review or is undergoing a review.

<b>Relevant Legislation:</b>	Building Act 2004 Local Government Act 2002 Heritage New Zealand Pouhere Taonga Act 2014 Resource Management Act 1991 Health Act 1956
<b>Associated Documents:</b>	The New Zealand Building Code Dunedin City District Plan

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**Hearings Committee - Bylaws and Policies**  
**Dangerous, Insanitary and Affected Buildings Policy**  
**MINUTES**

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Minutes of an ordinary meeting of the Dangerous, Insanitary and Affected Buildings Policy Hearings held in the Council Chambers, Dunedin Public Art Gallery, The Octagon, Dunedin on Monday 30 March 2026, commencing at 9.02 am

**PRESENT**

**Chairperson** Cr Lee Vandervis

**Members** Cr Russell Lund Cr Mandy Mayhem

**IN ATTENDANCE** Paul Henderson (General Manager Corporate and Regulatory Services); Mike Hart (Acting Manager Building Services), Grant Sutton (Principal Advisor Building Services), Mark Mawdsley (Team Leader Advisory Services) and Anne Gray (Policy Analyst) and Cr John Chambers

**Governance Support Officer** Lynne Adamson

**1 APOLOGIES**

There were no apologies.

**2 CONFIRMATION OF AGENDA**

Moved (Cr Lee Vandervis/Cr Mandy Mayhem):

That the Committee:

**Confirms** the agenda without addition or alteration.

**Motion carried (CH/2026/004)**

**3 DECLARATIONS OF INTEREST**

Members were reminded of the need to stand aside from decision-making when a conflict arose between their role as an elected representative and any private or other external interest they might have.

Moved (Cr Lee Vandervis/Cr Mandy Mayhem):

That the Committee:

- a) **Notes** the Elected or Independent Members' Interest Register; and
- b) **Confirms** the proposed management plan for Elected or Independent Members' Interests.

**Motion carried (CH/2026/005)**

**4 SPEAKING SCHEDULE**

**Michael Gibson**

Mr Michael Gibson spoke to his submission in support of the proposed Dangerous, Insanitary and Affected Building Policy. Mr Gibson spoke of the results of the Ministry of Business, Innovation and Employment (MBIE) inspections of rental properties last year. He commented that as landlords gave consent to the inspection he feared that as a result, the worst properties weren't inspected.

Mr Gibson responded to questions.

The Chair acknowledged, and the committee considered the other three submissions received.

**5 RESULTS OF CONSULTATION ON REVIEW OF DANGEROUS, INSANITARY AND AFFECTED BUILDINGS POLICY**

A report from Corporate and Regulatory informed the Hearings Committee on the submissions received on the review of the Dangerous, Insanitary and Affected Buildings Policy.

There were four submissions received with two submitters in agreement with the proposed Policy; one did not agree and one did not answer the question.

The General Manager Corporate and Regulatory Services (Paul Henderson), Acting Manager Building Services (Mike Hart), Principal Advisor Building Services (Grant Sutton) and Team Leader Advisory Services spoke to the report and responded to questions.

Moved (Cr Lee Vandervis/Cr Mandy Mayhem):

That the Committee:

- a) **Considers** the submissions on the review of the Dangerous, Insanitary and Affected Buildings Policy; and

- b) **Makes** recommendations to the Council on the Dangerous, Insanitary and Affected Buildings Policy.

**Motion carried (CH/2026/006)**

Moved (Cr Lee Vandervis/Cr Mandy Mayhem):

That the Committee

**Adjourns** the meeting for 5 minutes.

**Motion carried**

The hearing adjourned at 10.05 am and resumed at 10.17 am.

There was a continued discussion on the submissions and the proposed changes.

Moved (Cr Lee Vandervis/Cr Mandy Mayhem):

That the Committee:

- a) **Approves** all of the proposed track changes to the Dangerous, Insanitary and Affected Buildings policy with the inclusion of:
- i) Add the word “ordinary” to the definition of Dangerous buildings to accurately reflect this definition in the Act.
  - ii) Add the words 2.4 “Any proposed work affecting buildings or areas subject to heritage orders or encumbrances (e.g. a heritage covenant), may require additional approvals” for greater clarification.

**Motion carried (CH/2026/007)**

The meeting concluded at 10.20 am

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CHAIRPERSON

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## **FINANCIAL REPORT - PERIOD ENDED 28 FEBRUARY 2026**

Department: Finance

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### **EXECUTIVE SUMMARY**

- 1 This report provides the financial results for the period ended 28 February 2026 and the financial position as at that date.
- 2 As this is an administrative report only, there are no options or Summary of Considerations.

### **RECOMMENDATIONS**

That the Council:

- a) **Notes** the Financial Performance for the period ended 28 February 2026 and the Financial Position as at that date.

### **BACKGROUND**

- 3 This report attaches a financial update and financial statements for the period ended 28 February 2026.

### **DISCUSSION**

- 4 The net deficit (including Waipori) for the period ended 28 February 2026 was \$1.200 million, a \$9.655 million favourable variance to budget. A detailed commentary is provided in Attachment A (Financial Update). In summary, the following variances were recorded:
  - a) Revenue was \$294.418 million for the period, or \$5.095 million unfavourable to budget.
  - b) Expenditure was \$301.664 million for the period, or \$12.060 million favourable to budget.
  - c) The Waipori Fund has reported a net operating surplus for the period of \$6.046 million, \$2.690 million favourable to budget.
- 5 Capital expenditure was \$95.620 million for the period ended 28 February 2026 or 68.9% of the year-to-date budget.
- 6 The total loan balance at 28 February 2026 was \$678.273 million which was \$69.900 million less than budget.

**NEXT STEPS**

- 7 Month end financial reports will continue to be presented to future Council or Finance and Performance Committee meetings.

**Signatories**

Author:	Lawrie Warwood - Management Accountant
Authoriser:	Hayden McAuliffe - Financial Services Manager Carolyn Allan - Chief Financial Officer

**Attachments**

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**FINANCIAL UPDATE**

**For the period ended 28 February 2026**

This report provides a detailed commentary on the Council's financial result for the period ended 28 February 2026 and the financial position at that date.

**SUMMARY FINANCIAL INFORMATION**

<b>\$ Million</b>	<b>Actual YTD</b>	<b>Budget YTD</b>	<b>Variance YTD</b>		<b>Last Year YTD</b>
Revenue	294.418	299.513	(5.095)	U	262.168
Expenditure	301.664	313.724	12.060	F	287.612
Net Surplus/(Deficit) excluding Waipori	(7.246)	(14.211)	6.965	F	(25.444)
Waipori Fund Net	6.046	3.356	2.690	F	7.872
Net Surplus/(Deficit) including Waipori	(1.200)	(10.855)	9.655	F	(17.572)
<b>Capital Expenditure</b>	95.620	138.879	43.259		92.815
<b>Debt</b>					
Current Year Loan	27.300	86.000	58.700	F	46.351
Prior Year Loan	650.973	662.173	11.200	F	592.622
Accrued Interest	4.150	4.797	0.647	F	3.734
<b>Total Debt</b>	682.423	752.970	70.547	F	642.707

**STATEMENT OF FINANCIAL PERFORMANCE**

The statement of financial performance is provided in Attachment B.

The net deficit (including Waipori) for the period ended 28 February 2026 was \$1.200 million, a \$9.655 million favourable variance to budget.

## REVENUE

The total revenue for the period was \$294.418 million or \$5.095 million less than budget.

The major variances were as follows:

### External Revenue

*Actual \$62.915 million, Budget \$64.369 million, Unfavourable variance to budget \$1.454 million*

Compliance Solutions revenue was unfavourable to budget \$565k. Parking Enforcement revenue of \$1.588 million was unfavourable to budget \$551k largely because of lower staffing levels within the Parking Enforcement team (due to leave and vacancies). Other contributing factors include the free parking over the Christmas holiday period, timing of the introduction of Licence Plate Recognition (LPR), expected to be launched in the second quarter of 2026, and timing of recruitment.

Transport revenue was unfavourable to budget \$443k. NZTA recoveries were \$225k less than budgeted mainly because of subsidised state highway maintenance expenditure being under budget for the period, which are costs that are 100% on-charged to NZTA. Corridor accessway revenue was unfavourable to budget \$182k due to fewer applications than budgeted being received.

Parking Operations revenue was unfavourable to budget \$302k, partly due to on-street meter revenue and the timing of parking revenue from the new Mataukareau carpark, which opened towards the end of November. The unfavourable variance includes a one-off recovery relating to the new Mataukareau carpark building.

Building Services revenue was unfavourable to budget \$265k due to fewer consent applications and inspections than expected. This variance is offset by favourable year to date expenditure.

Community recreation revenue was unfavourable \$204k due mainly to Aquatic Services general admission fees, learn to swim and retail sales revenue below budget. Waterslide revenue was also below budget due to the facility being closed while maintenance was completed.

DPAG, Toitu and Lan Yuan revenue was unfavourable \$193k, with Art Gallery auditorium revenue, Toitu visitor programme revenue and admission fees, and Lan Yuan admission fees all below budget.

Waste Minimisation revenue was unfavourable to budget \$171k. Green Island landfill revenue was unfavourable \$585k due to less waste being received at the landfill than budgeted. Commercial waste volumes are currently approximately 15% lower than budget (noting that variable costs were favourable). Green Island landfill revenue for 2025/26 was over-estimated due to incorrect reporting of previous waste volumes. Partially offsetting this variance Waste strategy revenue was favourable to budget \$327k, reflecting a higher level of waste levy revenue from the Ministry for the Environment than expected. Revenue from the materials recover store was also favourable \$53k.

Offsetting these unfavourable variances:

Property Services revenue was favourable \$377k. Holding property revenue was favourable to budget due to unbudgeted recoveries from the Milners Rd property, and favourable rental revenue from the Forbury Park property. Community Property revenue was favourable to budget due to unbudgeted electricity recoveries from the Dunedin Ice Stadium. This variance was offset by a corresponding unfavourable variance in expenditure.

3 Waters revenue was favourable to budget \$266k, with water sales, backflow charges, meter rental and trade waste revenue ahead of budget.

### **Operating Grants Revenue**

*Actual \$8.733 million, Budget \$9.031 million, Unfavourable variance to budget \$298k*

Transport grants revenue was unfavourable to budget \$638k mainly reflecting less NZTA operating subsidy due to less expenditure in maintenance than budgeted for the period. Staff recovery subsidy is also unfavourable due to the number of FTE vacancies in subsidised roles.

Investment Account revenue was unfavourable to budget \$128k with Better Off Funding revenue being less than budgeted.

Offsetting these unfavourable variances to budget, 3 Waters revenue was favourable to budget \$349k due to \$331k of unbudgeted revenue for Local Water Done Well Transition costs.

Events revenue was favourable to budget \$110k due to an unbudgeted MBIE Promotional Fund grant providing funding for a range of planned events.

### **Capital Grants Revenue**

*Actual \$11.163 million, Budget \$15.953 million, Unfavourable variance to budget \$4.790 million*

Transport NZTA capital subsidy was unfavourable \$4.799 million, reflecting less subsidised capital expenditure to date.

### **Contributions Revenue**

*Actual \$3.814 million, Budget \$2.571 million, Favourable variance to budget \$1.243 million*

Contributions revenue was favourable to budget \$1.243 million reflecting a higher level of development contributions received than budgeted. Development contributions revenue for 3 Waters was favourable to budget \$428k, and Transport revenue was favourable to budget \$777k.

### **Internal Revenue**

*Actual \$29.862 million, Budget \$30.418 million, Unfavourable variance to budget \$556k*

Waste Minimisation landfill revenue was unfavourable to budget \$420k. Kerbside collection disposal fees were unfavourable \$86k, due to the volume of waste from the red bin collections being lower than forecast. This variance is offset by a favourable variance against budget for internal landfill disposal costs. Internal landfill revenue from wastewater treatment plant sludge disposal was unfavourable to budget \$334k due to less sludge being disposed of at the landfill than anticipated.

3 Waters internal revenue was unfavourable to budget \$153k, reflecting a lower level of Better Off Funding revenue than budgeted.

## **EXPENDITURE**

The total expenditure for the period was \$301.664 million or \$12.060 million less than budget.

The major variances were as follows:

### **Personnel Costs**

*Actual \$55.657 million, Budget \$58.396 million, Favourable variance to budget \$2.739 million*

This favourable variance to budget mainly reflects the number of vacancies during the eight months, including new positions included in the 2025/26 budget, some of which have now been filled.

### **Operations and Maintenance Costs**

*Actual \$60.130 million, Budget \$64.747 million, Favourable variance to budget \$4.617 million*

Waste Minimisation expenditure was favourable to budget \$1.487 million. ETS costs and variable landfill contract costs were favourable to budget \$1.060 million, reflecting the lower volumes of material entering the landfill and a lower carbon price than budgeted. Landfill monitoring expenditure was favourable to budget \$315k, due to a rationalisation of baseline environmental monitoring requirements for the Smooth Hill landfill and a temporary reduction in the frequency of bird monitoring surveys around Smooth Hill and Dunedin environs. The Kerbside Collections contract cost was favourable \$171k, mainly due to a combination of the volume of mixed recycling and glass collected and processed being lower than forecast, plus occasional positive returns on the sale of recyclable material.

3 Waters maintenance expenditure was favourable to budget \$1.402 million largely driven by lower than budgeted reactive plant and network maintenance requirements. This is due to management of planned maintenance, as staff seek preventative maintenance opportunities. Lead time for the start-up of a new inflow and infiltration programme also contributes to the favourable result. There is some significant planned maintenance expenditure anticipated before year-end.

Community recreation expenditure was favourable \$542k due to Aquatics plant maintenance (St Clair, Moana and Te Puna o Whakaehu), Parks and Reserves reactive maintenance requirements being less than anticipated to date, and the timing of building maintenance budgets. Cemeteries interment and building maintenance costs were also favourable.

BIS expenditure was favourable \$258k with little expenditure to date on specialist ITMS support and non-capital project management costs. The variable component of the ITMS managed services contract was also favourable.

Events expenditure was favourable \$247k, mainly due to the budget timing of several summer events.

Transport expenditure was favourable to budget \$238k. The environmental maintenance and unsealed and sealed pavement maintenance budgets were unfavourable, as staff continue to offset overspending in environmental and routine drainage (catchpits etc). There is a higher spend in environmental maintenance associated with keeping environments free of detritus and debris, including soil and stone displacement from hillsides onto certain transport corridors. The strength of the renewals programme over the past three years has provided sufficient network resilience to enable these short-term adjustments without materially compromising overall road condition. Overall expenditure is expected to be on budget for Transport maintenance at year end.

DPAG, Toitu and Lan Yuan expenditure was favourable \$216k mainly due to the timing of exhibition and collection management expenditure.

Fleet Operations expenditure was favourable \$198k due mainly to fuel and tyre costs being less than budgeted. Planned vehicle lease costs have yet to occur, resulting in a \$41k favourable expenditure variance.

These favourable variances were partially offset by:

Property expenditure was unfavourable to budget \$297k due mainly to higher than budgeted reactive maintenance costs for several properties resulting from the October wind event, and Community Housing grounds maintenance costs.

Governance expenditure was unfavourable to budget \$186k, mainly due to the incorrect spreading of the election costs budget.

#### **Occupancy Costs**

*Actual \$28.116 million, Budget \$28.566 million, Favourable variance to budget \$450k*

3 Waters expenditure was favourable to budget \$363k due mainly to insurance costs being favourable \$474k and \$104k favourable fuel costs to budget due to optimisation of plant. Offsetting this, electricity costs were unfavourable \$94k and water charges \$215k.

Transport expenditure was favourable to budget \$132k due to electricity costs being less than budget, particularly for street lighting. This was partly due to a refund of state highway lighting costs relating to the previous year.

Insurance costs across all activities were favourable to budget \$821k, reflecting a lower than budget premium for materials damage insurance plus a partial refund of prior years' Earthquake Levy.

#### **Consumable and General Costs**

*Actual \$18.505 million, Budget \$19.329 million, Favourable variance to budget \$824k*

3 Waters expenditure was favourable to budget \$760k due mainly to consultants costs being \$655k below budget partly due to engineering consultant services being less than expected, and partly due to an underspend in Better Off Funding projects.

Compliance Solutions expenditure was favourable to budget \$252k with Parking Services court lodgement fees, consultants, postage and refunds all being below budget to date.

Transport expenditure was favourable to budget \$192k, mainly due to the timing of consultants costs, which are expected to be slightly unfavourable at year-end.

Community recreation expenditure was favourable to budget \$180k partly due to the timing of consultants and legal costs.

Partly offsetting these favourable variances:

Resource Consents expenditure was unfavourable to budget \$362k, with consultant's fees being over budget due to outsourced planning consultants required to assist in dealing with the high number of resource consent applications.

Finance expenditure was unfavourable to budget \$217k mainly due to outsourced professional services relating to Local Water Done Well and cover for vacancies.

Property expenditure was unfavourable to budget \$179k due mainly to consultants costs, including for seismic and structural assessment work, as well as property valuation costs.

#### Grants and Subsidies Costs

*Actual \$10.756 million, Budget \$10.692million, Unfavourable variance to budget \$64k*

Grants expenditure was unfavourable to budget \$64k, mainly due to Property hall committee grants payments relating to the previous financial year, as well as timing of Community Development place based grants.

#### Internal Costs

*Actual \$29.862 million, Budget \$30.418 million, Favourable variance to budget \$556k*

3 Waters expenditure was favourable to budget \$314k resulting from improved treatment and incineration of the sludge at waste treatment plants and the transfer of some sludge to an external provider. This partly offsets the unfavourable internal landfill revenue reported above.

Investment Account expenditure was favourable \$147k with internal Better Off Funding grants being less than budgeted.

Waste Minimisation expenditure was favourable to budget \$93k with landfill disposal fees for kerbside collection and waste minimisation activities \$93k less than budgeted for the period. This also partly offsets the unfavourable internal landfill revenue reported above.

#### Depreciation Costs

*Actual \$80.727 million, Budget \$82.168 million, Favourable variance to budget \$1.441 million*

Depreciation costs overall were favourable to budget \$1.441 million across most activities.

The main reason for the favourable variance relates to under expenditure of the capital programme in 2024/25 and the timing of the completion of some projects. The main favourable variances were in BIS (\$401k), Property (\$487k) and Community Recreation (\$476k).

Amortisation for 3 Waters intangible assets was unfavourable to budget \$451k. Depreciation on all other 3 Waters assets was at budget level while final asset valuations are confirmed.

#### Interest Costs

*Actual \$17.911 million, Budget \$19.408 million, Favourable variance to budget \$1.497 million*

This favourable variance to budget reflected a lower interest rate for the first two quarters than the 4% rate budgeted, a lower opening debt balance than forecast at 30 June 2025 and less debt raised than budgeted in this financial year. Interest rates are reviewed quarterly by Dunedin City Treasury Limited. The actual quarterly interest rates are:

Actual Quarterly Interest Rates		
Q1	Jul-Sep	3.95%
Q2	Oct-Dec	3.75%
Q3	Jan-Mar	4.25%
Q4	Apr-Jun	4.23%

#### OPERATING FORECAST TO 30 JUNE

The forecast operating result for the remainder of the financial year is expected to remain overall to budget, although some favourable variances are anticipated to reduce by year end.

Income is forecast to remain unfavourable overall, primarily due to lower than budget NZTA subsidies and grants, continued pressure on parking operations revenue, and lower landfill tonnage impacting waste revenue. These variances are expected to be partially offset by favourable development contributions and property related revenue, which continue to track ahead of budget.

Expenditure is expected to move closer to budget, particularly within operations and maintenance costs for Transport and 3 Waters. Interest costs are expected to remain favourable due to lower than forecast debt levels, reflecting the capital programme.

#### WAIPORI FUND NET OPERATING RESULT

*Actual \$6.046 million surplus, Budget \$3.356 million surplus, Favourable variance to budget \$2.690 million.*

The Waipori Fund has reported a net operating surplus for the period of \$6.046 million, \$2.690 million more than budget. The value of the fund increased \$1.728 million for the month of February.

Equities saw an increase in value of \$1.048 million during February, while term investments increased in value \$722k and property investments decreased \$19k.

Year to date the fund remains ahead of budget.

#### STATEMENT OF FINANCIAL POSITION

The Statement of Financial Position is provided as Attachment C.

- Other current financial assets of \$6.420 million relate to the Waipori Fund.
- The loans balance at 28 February 2026 is \$682.423 million. This balance is made up as follows:

	Actual \$million	Budget \$million	Variance \$million	
Loan Balance	678.273	748.173	69.900	F
Accrued Interest on Loans	4.150	4.797	0.647	F
<b>Total Loans</b>	<b>682.423</b>	<b>752.970</b>	<b>70.547</b>	<b>F</b>

The loans balance at 28 February was \$678.273 million which was \$69.900 million less than budget. This is due mainly to the June loans balance being \$11.200 million less than forecast and the draw-down required for the capital programme being less than budget. Additional to the February loans balance there was accrued interest of \$4.150 million.

#### CAPITAL EXPENDITURE

A summary of the capital expenditure programme by Activity is provided as Attachment E. Attachment F provides the detailed capital expenditure programme.

Total capital expenditure for the period was \$95.620 million or 41.4% of the \$231 million full year budget.

Capital expenditure for 2025/26 is forecast to be \$181 million against a budget of \$231 million at 30 June 2026, mainly due to timing of some projects but also reflecting some savings. This forecast has decreased by

\$9 million from the \$190 million forecast in January. In summary, the main reasons for the forecast underspend are:

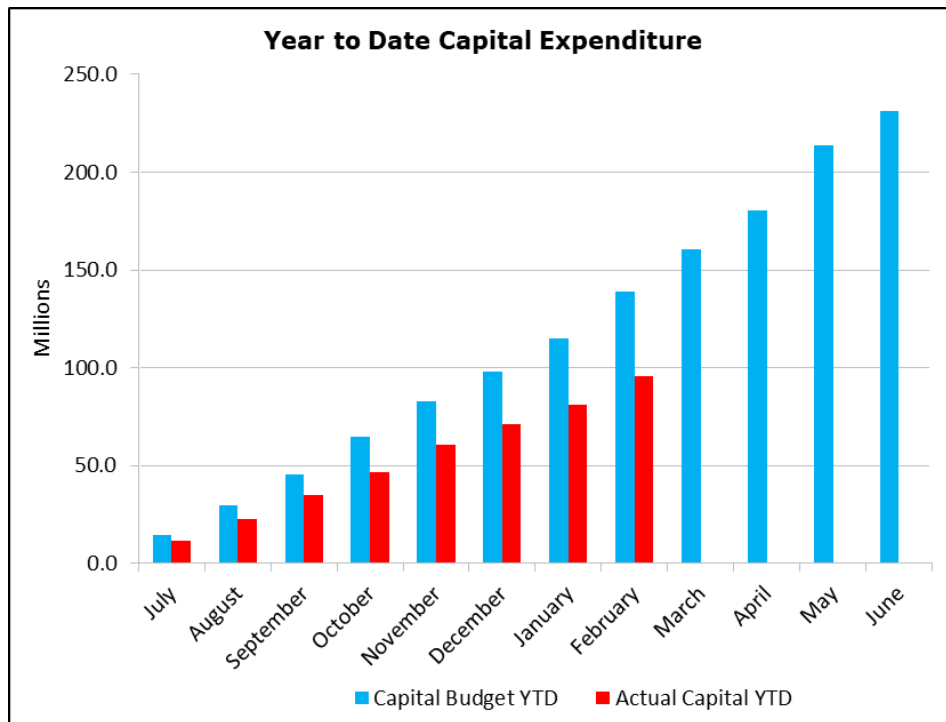
**Project Timing:**

- Waste Minimisation \$30.6 million - timing delays due to consents and ‘pre-loading’ the ground for a minimum of four months prior to construction commencing.
- Property \$5.1 million – largely timing delays in the Dunedin City Library refurbishment project and other renewals projects (e.g. Sims building, Dunedin Railway Station, roof renewals project and Olveston upgrade).
- Transport \$3.9 million –timing of the Albany St and Mosgiel Park and Ride projects.

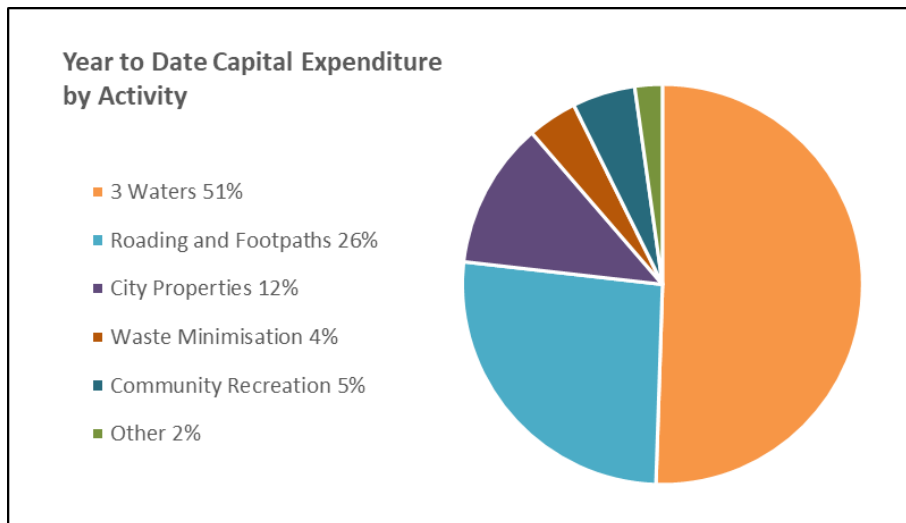
**Project Cost Savings:**

- Waste Minimisation \$1.9 million - savings in Glass Facility project.
- Property \$440k - savings in Fitzroy St housing renewal.
- Parks and Recreation \$3.5 million - savings in the Moana Pool redevelopment project due to scope changes.
- Transport \$1.7 million – savings in coastal protection and Central City Parking Management projects.

Forecasts are reviewed monthly and are based on best known information at the time. Further information on activity forecasts are provided within each activity section below.



The chart below shows the proportion of year-to-date capital expenditure by Activity:



**City Properties** capital expenditure was \$7.470 million favourable to budget.

Investment property renewals was under budget \$1.226 million, with little expenditure to date on the 130 Gt King St, Rosebank Road and Heriot Drive properties due to protracted planning and lease negotiations. Expenditure is forecast to be under budget \$600k as at June 2026.

The Dunedin City Library refurbishment renewal project was favourable \$2.145 million. The project is still in the planning and design phase. The project timing will now be considered as part of the next LTP, resulting in \$2.634 million forecast underspend in the current financial year.

The South Dunedin Library and Community Complex project was favourable \$533k due to the timing of the project, which is in its final stages. This favourable variance is expected to continue for the remainder of the year.

The Fitzroy St housing renewal was favourable \$907k, with physical works having begun in August. Expenditure on this project is forecast to be \$440k underspent reflecting savings arising during procurement. The project is forecast to finish on time and under budget.

Expenditure on the Town Hall/Municipal Chambers project was favourable \$459k due to the timing of the project expenditure. Expenditure is forecast to be under budget \$270k for the year.

The Civic Centre upgrade project was favourable \$508k to February due to timing of expenditure. The project is forecasted to be completed within budget.

The Edgar Centre refurbishment project was favourable \$693k due to timing of the project expenditure.

Spend on the High-Performance Sports building was under budget by \$220k, with the expenditure having been completed in the previous financial year.

Sargood Centre expenditure was favourable \$737k, with physical works expected to commence in May.

**Community Recreation** capital expenditure was \$3.044 million favourable to budget

Moana Pool redevelopment renewals was favourable \$2.334 million due to timing of programme works, including condition assessment and development of options for air handling units. After assessing the condition of the units, a solution was proposed that could cost \$3.5 million less. This is being developed further and checked from a design point of view. The planned work continues an ongoing programme of improvements at Moana Pool, including recently revamping the water slides, new wall tiles, upgraded family change facilities, as well as changes to plant/equipment areas.

Parks recreational facilities renewals was favourable \$318k due to the timing of hard surfaces, building & coastal facilities renewals, and is forecast to be underspent \$230k at year-end.

Cemeteries new capital was favourable \$236k due to the construction timing of the new Cemetery Development civil works contract. Physical works commenced at the end of January and are expected to be completed by the end of the financial year.

Botanic Garden renewals was favourable \$107k due to the timing of the completion of the café upgrade. The café work is on track, with handover to the operators for their fit-out scheduled in the last week of April.

**Creative and Cultural Vibrancy** capital expenditure was \$124k favourable to budget.

Library collection purchases were favourable \$131k, including purchases for the new library in South Dunedin.

Expenditure on the new theatrette project at Toitu was favourable \$33k, while expenditure on Art Gallery acquisitions was unfavourable \$57k.

**Governance and Support Services** capital expenditure was \$2.054 million favourable to budget

Fleet Operations renewals expenditure was favourable \$160k due to the timing of the vehicle replacement programme. The budget is expected to be fully spent by year-end.

BIS expenditure was favourable \$1.893 million with no expenditure to date on the Customer Self Service Portal and ITMS cloud migration projects. Capital expenditure is forecast to be underspent by \$1.1 million at year-end due to delays in the e-service project.

**Resilient City** capital expenditure was \$101k favourable to budget

There has been no expenditure to date on street trees and furniture, minor streetscape upgrades, or on Civil Defence plant upgrades.

**Roading and Footpaths** capital expenditure was \$13.912 million favourable to budget

New capital expenditure is forecast to be underspent for the year by \$4.228 million. Coastal protection expenditure is forecast to be underspent \$1.184 million, reflecting the better-than-expected performance of the geobag structure. Whether the remaining budget is required is contingent upon the impact of any future significant storm events on coastal assets.

Shaping Future Dunedin projects were favourable \$6.555 million and are forecast to be underspent \$3.946 million due to the phasing of costs. The Albany St project is forecast to be underspent by \$717k due to the timing of expenditure because the contract was only awarded in late December 2025. The Mosgiel Park and Ride is forecast to be underspent by \$3.200 million due to delays associated with consenting and KiwiRail approvals. The Central City Parking Management project is forecast to be \$511k less than budget due to overall project savings.

The Tunnels Trail project (Dunedin Urban Cycleways) was unfavourable \$330k and is forecast to be \$380k above budget at year-end.

Renewals expenditure is expected to be underspent by \$500k. This underspend is largely associated with the kerb/channel and footpath contract and is less than 2% of the overall renewals budget for 2025/26.

Resurfacing and rehabilitation activity has increased. Funding has been redirected from footpaths and kerbs until the new contract is awarded in the coming financial year.

### **3 Waters capital expenditure was \$5.282 million favourable to budget**

3 Waters is continually monitoring and re-prioritising the capital programme, and some projects have been accelerated whilst others have been deferred as solutions are further assessed. A number of project components have been re-assessed from new capital to renewals budget lines. The programme of work is forecast to be on budget at year-end.

Combined expenditure on Mosgiel Stormwater Upgrades and Mosgiel Stormwater Pumpstation (New Capital and Renewals) was \$99k favourable. This is under construction but has been re-phased due to a review and adoption of an alternative solution which presents better value and outcomes.

Port Chalmers Water Supply construction is underway; however, expenditure was favourable by \$121k due to a design review resulting in re-routing the pipeline and re-phasing of the project to deliver a more effective solution, following the October 2024 rain event.

Other Water and Wastewater Renewals are a combined \$7.216 million unfavourable as delivery had been accelerated in these areas to manage the overall renewals programme.

The scope of the Musselburgh to Tahuna Link has been reduced at this stage resulting in forecast expenditure of \$3.080 million against a budget of \$8.000 million. This is due to ongoing investigation work indicating the extent of renewals required may be reduced from those originally anticipated. Staff are assessing the most efficient solution currently.

The extent of stormwater renewals required (in areas currently being renewed) is currently forecast to be less than anticipated in the budget by approximately \$2.597 million. The extent of work required will become clearer as the projects progress. This is offset by other water and wastewater renewals which are forecast at approximately \$11.000 million over budget due to the acceleration of network water and wastewater renewals such as Kaikorai Valley Hills and North East Valley.

### **Vibrant Economy capital expenditure was \$96k unfavourable to budget**

Events new capital was unfavourable \$96k, relating to the purchase of new Dunedin and Ōtepoti branded light up letters as well as new Chinese festival lanterns and containers to store them.

### **Waste Minimisation capital expenditure was \$11.363 million favourable to budget**

Resource recovery park expenditure was favourable \$5.130 million. Otago Regional Council consent for civil works was received on 29 September 2025, and construction began in October; however, significant works were delayed until early December. This delay is expected to result in an underspend this financial year of approximately \$5.000 million, which will need to be factored into next financial year.

Material recovery facility (MRF) expenditure was favourable \$1.540 million. The building has been delayed due to a requirement to 'pre-load' the ground and prevent differential settlement. Pre-loading of the MRF is expected to be completed by the end of April, weather permitting, which will be followed by a pause in significant construction activity until September 2026. Completion will now be mid 2027; therefore, this delay is expected to result in an underspend this financial year of approximately \$17.300

million, which will need to be factored into next financial year. This also applies to the final payment for the MRF equipment supply, which had been budgeted to occur in May 2026.

Organics Processing Facility (OPF) was favourable \$1.537 million. The first major progress payment on the equipment supply for the organics facility was expected in January 2026. The pre-loading requirement affecting the MRF is also affecting the OPF, therefore this delay is forecast to result in an underspend this financial year of approximately \$7.800 million, which will need to be factored into next financial year. Pre-loading of the OPF is expected to be complete by the end of May, weather permitting, which will be followed by a pause in significant construction activity until October 2026.

Glass facility expenditure was favourable \$2.371 million. The site pre-loading work has required a temporary glass facility to be established. This temporary facility may prove suitable for long-term use which, if successful, will result in a project saving of approximately \$1.500 million. This saving is contingent upon the temporary facility becoming the long-term processing option.

Green Island landfill aftercare expenditure was favourable \$208k due to the timing of expenditure for a new leachate interceptor system along the southern edge of the landfill. Tenders for this work closed in November and work is currently underway.

Green Island landfill gas collection expenditure was favourable \$282k due to the timing of expenditure on the gas collection system.

**SUPPLEMENTARY INFORMATION**

**October 2024 Rain Event - \$3.854 million Unfavourable**

As at 28 February 2026 total operational costs relating to the October 2024 rain event totalled \$2.778 million, mainly being emergency maintenance through the Transport roading maintenance contract, but also including other emergency work such as sandbagging costs, floodwater pumping, refuse skip deployment and emergency welfare-related costs.

Capital expenditure relating to the rain event totalled \$5.556 million as at 28 February.

NZTA subsidy revenue received to date totals \$4.308 million, along with a welfare subsidy claim totalling \$50k received from NEMA. The remaining \$3.975 million is funded by the council.

<b>October 2024 Rain Event Financial Summary</b>	
<b>As at 28 February 2026</b>	
	<b>\$</b>
<b><u>Expenditure</u></b>	
Operating costs	2,711,917
Estimated personnel costs	66,162
Capital Expenditure	5,555,950
<b>Total Expenditure</b>	<b>8,334,029</b>
<b><u>Funded by:</u></b>	
NZTA Subsidy revenue received	4,038,403
NEMA Welfare costs reimbursement	50,485
DCC	4,245,141
<b>Total Revenue</b>	<b>8,334,029</b>
<b><u>Subsidy Summary</u></b>	
NZTA approved subsidy	4,687,000
Subsidy received to date	4,038,403
Subsidy yet to be received	648,597

**October 2025 Wind Event - \$732k Unfavourable**

As at 28 February 2026 total operational costs relating to the October 2025 wind event totalled \$991k, mainly being emergency maintenance through the Transport roading maintenance contract, as well as storm damage to reserves, damage to 3 Waters infrastructure and repairs to property (mainly Dunedin Ice Stadium and 20 Parry St).

Capital expenditure relating to the wind event totalled \$69k as at 28 February.

NZTA have approved subsidy at a FAR rate of 51%, totalling \$212k. This subsidy has been claimed in full as at 28 February. A request to NZTA to consider approving further funding has been made.

An insurance claim has been lodged relating to the ongoing costs of repairs to council properties. No insurance recoveries have been received to date.

<b>October 2025 Wind Event Financial Summary</b>	
<b>As at 28 February 2026</b>	
	\$
<b>Expenditure</b>	
Operating costs	991,421
Capital Expenditure	69,219
<b>Total Expenditure</b>	<b>1,060,640</b>
<b>Funded by:</b>	
NZTA Subsidy revenue received	211,782
Insurance Claim Recovery	0
DCC	848,858
<b>Total Revenue</b>	<b>1,060,640</b>
<b>Subsidy Summary</b>	
NZTA approved subsidy	211,782
Subsidy received to date	211,782
Subsidy yet to be received	0

#### **Potential impact of oil related cost escalations**

Fuel price volatility and potential fuel supply constraints present financial and operational risks for the Council. Staff are working across the organisation to establish a consistent approach to managing contract claims and adjustments arising from oil related cost escalations. Within 3 Waters, stationary diesel spend has averaged approximately \$30k per month year to date, primarily for critical treatment plant operations and wet weather or outage response, with year-to-date savings currently offsetting recent market price increases. However, recent fuel price rises may erode these savings over the remainder of the year and are expected to place upward pressure on contractor capital and operating costs, particularly for freight, chemical supply, tankering, and sludge haulage. These risks are being closely monitored and actively managed with suppliers.

Within Fleet Operations, fuel purchases have averaged \$20k per month for the eight months to February which has been below budget. The recent fuel price increases may reduce these savings over the remainder of the year. Staff are monitoring fleet fuel usage by activities.

#### **Group Debt Chart**

Attachment H includes a chart showing actual group and DCC debt for the years ending June 2004-2025. It provides forecast information for the years ending June 2026-2034 based on the current Statements of Intent (SOI), and the draft 2026/27 Annual Plan.

**DUNEDIN CITY COUNCIL**

**Statement of Financial Performance**  
For the Eight Months Ending 28 February 2026  
Amount : \$'000

Month Actual	Month Budget	Month Variance		Year to Date Actual	Year to Date Budget	Year to Date Variance	LY YTD Actual	LY Full Year Actual	Full Year Budget
<b>REVENUE</b>									
22,073	22,050	23	F	176,566	176,398	168	F	159,915	264,596
114	129	15	U	1,365	773	592	F	1,196	1,300
9,815	10,142	327	U	62,915	64,369	1,454	U	53,228	102,069
1,590	1,086	504	F	8,733	9,031	298	U	8,306	13,166
2,353	3,266	913	U	11,163	15,953	4,790	U	8,501	22,731
363	321	42	F	3,814	2,571	1,243	F	2,319	6,856
3,808	3,799	9	F	29,862	30,418	556	U	28,703	45,586
<b>40,116</b>	<b>40,793</b>	<b>677</b>	<b>U</b>	<b>294,418</b>	<b>299,513</b>	<b>5,095</b>	<b>U</b>	<b>262,168</b>	<b>456,304</b>
<b>EXPENDITURE</b>									
6,527	6,884	357	F	55,657	58,396	2,739	F	56,698	88,076
8,632	8,001	631	U	60,130	64,747	4,617	F	57,324	95,573
3,678	3,607	71	U	28,116	28,566	450	F	27,351	37,762
2,168	2,327	159	F	18,505	19,329	824	F	16,640	28,998
565	107	458	U	10,756	10,692	64	U	10,083	12,512
3,808	3,799	9	U	29,862	30,418	556	F	28,703	45,586
10,125	10,271	146	F	80,727	82,168	1,441	F	72,095	123,713
2,240	2,416	176	F	17,911	19,408	1,497	F	18,718	29,114
<b>37,743</b>	<b>37,412</b>	<b>331</b>	<b>U</b>	<b>301,664</b>	<b>313,724</b>	<b>12,060</b>	<b>F</b>	<b>287,612</b>	<b>461,334</b>
2,373	3,381	1,008	U	(7,246)	(14,211)	6,965	F	(25,444)	(5,030)
<b>NET SURPLUS (DEFICIT)</b>									
<b>Add</b>									
1,728	419	1,309	F	6,046	3,356	2,690	F	7,872	5,030
<b>4,101</b>	<b>3,800</b>	<b>301</b>	<b>F</b>	<b>(1,200)</b>	<b>(10,855)</b>	<b>9,655</b>	<b>F</b>	<b>(17,572)</b>	<b>(733)</b>
<b>NET SURPLUS (DEFICIT)</b>									

F: (favourable variance to budget) U: (unfavourable variance to budget)

**DUNEDIN CITY COUNCIL**

**Statement of Financial Position**  
**As at 28 February 2026**  
Amount : \$'000

30-Jun-25 LY Full Year Actual		28-Feb-26 This Month Actual	28-Feb-26 This Month Budget	30-Jun-26 Full Year Budget	28-Feb-25 LY Month Actual
	<b>Current Assets</b>				
15,837	Cash and cash equivalents	21,968	32,225	13,358	9,671
8,011	Other current financial assets	6,420	14,426	14,426	12,109
21,484	Trade and other receivables	28,328	34,948	33,245	20,952
489	Current Tax Asset	49	351	250	511
1,039	Inventories	978	675	675	667
2,221	Prepayments	1,598	2,109	2,109	1,293
49,081	<b>Total Current Assets</b>	59,341	84,734	64,063	45,203
	<b>Non Current Assets</b>				
211,438	Other non-current financial assets	219,070	206,116	206,560	208,511
138,889	Shares in subsidiary companies	138,889	138,889	141,794	136,339
4,571	Intangible assets	3,599	4,774	5,382	3,770
113,710	Investment property	113,710	113,409	119,563	110,440
4,906,980	Property, plant and equipment	4,922,813	4,834,283	4,999,121	4,543,604
5,375,588	<b>Total Non Current Assets</b>	5,398,081	5,297,471	5,472,420	5,002,664
5,424,669	<b>TOTAL ASSETS</b>	5,457,422	5,382,205	5,536,483	5,047,867
	<b>Current Liabilities</b>				
39,760	Trade and other payables	50,911	48,773	40,639	35,716
5,967	Short Term Borrowings	4,150	4,797	6,230	3,734
7,863	Revenue received in advance	4,645	5,308	5,663	4,895
9,001	Employee entitlements	9,538	8,355	11,025	9,579
62,591	<b>Total Current Liabilities</b>	69,244	67,233	63,557	53,924
	<b>Non Current Liabilities</b>				
650,973	Term Loans	678,273	748,173	783,173	638,973
1,482	Employee entitlements	1,482	1,246	1,216	1,246
20,573	Provisions	20,573	22,206	22,206	22,206
320	Other Non-Current Liabilities	320	320	320	320
673,348	<b>Total Non Current Liabilities</b>	700,648	771,945	806,915	662,745
735,939	<b>TOTAL LIABILITIES</b>	769,892	839,178	870,472	716,669
	<b>Equity</b>				
1,648,778	Accumulated funds	1,647,373	1,628,228	1,639,015	1,633,790
3,027,570	Revaluation reserves	3,027,570	2,904,062	3,016,192	2,686,879
12,382	Restricted reserves	12,587	10,737	10,804	10,529
4,688,730	<b>TOTAL EQUITY</b>	4,687,530	4,543,027	4,666,011	4,331,198
5,424,669		5,457,422	5,382,205	5,536,483	5,047,867

**Statement of Change in Equity**  
**As at 28 February 2026**  
Amount : \$'000

30-Jun-25 LY Full Year Actual		28-Feb-26 This Month Actual	28-Feb-26 This Month Budget	30-Jun-26 Full Year Budget	28-Feb-25 LY Month Actual
4,348,770	Opening Balance	4,688,730	4,553,882	4,553,882	4,348,770
(733)	Operating Surplus (Deficit)	(1,200)	(10,855)	-	(17,572)
340,693	Movements in Reserves			112,129	-
4,688,730		4,687,530	4,543,027	4,666,011	4,331,198

**DUNEDIN CITY COUNCIL**

**Statement of Cashflows**  
**For the Eight Months Ending 28 February 2026**  
Amount : '\$000

	Year to Date Actual	Year to Date Budget	Full Year Budget	LY YTD Actual
<b>Cash Flow from Operating Activities</b>				
<i>Cash was provided from operating activities:</i>				
Rates Received	184,410	178,446	264,381	164,534
Other Revenue	96,385	78,665	121,288	78,759
Interest Received	4,171	4,559	8,313	4,285
Dividend Received	4,038	7,238	10,815	914
Income Tax Refund	440	-	351	-
<i>Cash was applied to:</i>				
Suppliers and Employees	(191,419)	(181,604)	(263,344)	(178,472)
Interest Paid	(19,521)	(21,241)	(29,447)	(19,308)
<b>Net Cash Inflow (Outflow) from Operations</b>	<b>78,504</b>	<b>66,063</b>	<b>112,357</b>	<b>50,712</b>
<b>Cash Flow from Investing Activities</b>				
<i>Cash was provided from investing activities:</i>				
Sale of Assets	31	-	120	25
Reduction in Loans & Advances	-	-	-	-
Reduction in Investments Other	13,705	12,000	18,000	11,970
<i>Cash was applied to:</i>				
Increases in Loans & Advances	-	-	-	-
Increase in Investments DCHL	-	-	-	-
Increase in Investments Other	(15,848)	(13,000)	(21,905)	(15,697)
Capital Expenditure	(97,561)	(130,193)	(227,569)	(94,393)
<b>Net Cash Inflow (Outflow) from Investing Activity</b>	<b>(99,673)</b>	<b>(131,193)</b>	<b>(231,354)</b>	<b>(98,095)</b>
<b>Cash Flow from Financing Activities</b>				
<i>Cash was provided from financing activities:</i>				
Loans Raised	59,800	86,000	121,000	81,000
Increase in Short Term Borrowings	-	-	-	-
<i>Cash was applied to:</i>				
Loans Repaid	(32,500)	-	-	-
Decrease in Short Term Borrowings	-	-	-	(32,500)
<b>Net Cash Inflow (Outflow) from Financing Activity</b>	<b>27,300</b>	<b>86,000</b>	<b>121,000</b>	<b>48,500</b>
<b>Total Increase/(Decrease) in Cash</b>	<b>6,131</b>	<b>20,870</b>	<b>2,003</b>	<b>1,117</b>
<b>Opening Cash and Deposits</b>	<b>15,837</b>	<b>11,355</b>	<b>11,355</b>	<b>8,555</b>
<b>Closing Cash and Deposits</b>	<b>21,968</b>	<b>32,225</b>	<b>13,358</b>	<b>9,672</b>

**DUNEDIN CITY COUNCIL**

**Capital Expenditure Summary by Activity**  
**For the Eight Months Ending 28 February 2026**  
Amount : \$'000

Group	Year to Date	Year to Date	Year to Date	Year to Date	LY YTD	Full Year	YTD Actual vs	
	Actual	Budget	Variance	Variance %				Actual
City Properties	11,355	18,825	7,470	60.3%	F	19,496	22,388	50.7%
Community Recreation	4,871	7,915	3,044	61.5%	F	4,627	14,948	32.6%
Creative and Cultural Vibrancy	1,445	1,569	124	92.1%	F	1,084	2,220	65.1%
Governance and Support Service	475	2,529	2,054	18.8%	F	464	4,306	11.0%
Regulatory Services	-	5	5	0.0%	F	278	5	0.0%
Resilient City	-	101	101	0.0%	F	-	385	0.0%
Roading and Footpaths	25,168	39,080	13,912	64.4%	F	22,929	54,760	46.0%
3 Waters	48,373	53,655	5,282	90.2%	F	39,419	87,123	55.5%
Vibrant Economy	96	-	96	0.0%	U	-	-	0.0%
Waste Minimisation	3,837	15,200	11,363	25.2%	F	4,518	45,082	8.5%
	<b>95,620</b>	<b>138,879</b>	<b>43,259</b>	<b>68.9%</b>	<b>F</b>	<b>92,815</b>	<b>231,217</b>	<b>41.4%</b>

U: (unfavourable variance/overspend to budget) F: (favourable variance/underspend to budget)





**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget		
City Properties	Property-Housing	Renewals Capital	Asset Renewals	246,357	328,000	(81,643)	500,000		
			Housing Renewal	917,595	1,825,000	(907,405)	2,010,000		
		Total Renewals Capital			1,163,952	2,153,000	(989,048)	2,510,000	
	<b>Total Property-Housing</b>				<b>1,163,952</b>	<b>2,153,000</b>	<b>(989,048)</b>	<b>2,510,000</b>	
	Property-Operational	New Capital		Public Toilets Growth	668,216	375,000	293,216	375,000	
				South Dunedin Library and Community Complex	1,836,597	2,370,000	(533,403)	2,370,000	
		Total New Capital			2,504,813	2,745,000	(240,187)	2,745,000	
		Renewals Capital		Asset Renewals	1,411,299	1,035,000	376,299	1,285,000	
				Asset Renewals - Public Toilet Renewals	115,749	435,000	(319,251)	475,000	
				Civic Centre	3,492,231	4,000,000	(507,769)	4,000,000	
				Dunedin Library Refurbishment	55,458	2,200,000	(2,144,542)	2,750,000	
				Dunedin Public Art Gallery	195,249	215,000	(19,751)	215,000	
				Furniture	77,002	32,800	44,202	50,000	
				Olveston House Renewal	75,077	135,000	(59,923)	290,000	
				Toitū Otago Settlers Museum	144,515	125,000	19,515	125,000	
				Town Hall and Municipal Chambers	1,304,352	1,763,000	(458,648)	2,688,000	
		Total Renewals Capital			6,870,931	9,940,800	(3,069,869)	11,878,000	
		<b>Total Property-Operational</b>				<b>9,375,744</b>	<b>12,685,800</b>	<b>(3,310,056)</b>	<b>14,623,000</b>
		Property-Community	New Capital		CCTV George St	97,090	-	97,090	-
	Total New Capital				97,090	-	97,090	-	
	Renewals Capital			Asset Renewals	256,936	69,000	187,936	100,000	
				Community Hall Renewals	134,618	105,000	29,618	150,000	
				Dunedin Railway Station	(35,863)	95,000	(130,863)	450,000	
				Edgar Centre Refurbishment	47,370	740,000	(692,631)	860,000	
				High Performance Sports	-	220,000	(220,000)	220,000	
				Roof Renewal Programme	-	180,000	(180,000)	250,000	
				Sargood Centre	2,757	740,000	(737,243)	850,000	
Tarpits				(206)	-	(206)	-		
Total Renewals Capital			405,611	2,149,000	(1,743,389)	2,880,000			
<b>Total Property-Community</b>				<b>502,701</b>	<b>2,149,000</b>	<b>(1,646,299)</b>	<b>2,880,000</b>		
Property-Investment	Renewals Capital		Asset Renewals	189,560	1,416,000	(1,226,440)	1,770,000		
			Total Renewals Capital			189,560	1,416,000	(1,226,440)	1,770,000
<b>Total Property-Investment</b>				<b>189,560</b>	<b>1,416,000</b>	<b>(1,226,440)</b>	<b>1,770,000</b>		
Property-Holding	Renewals Capital		Asset Renewals	11,877	400,000	(388,123)	575,000		
			Total Renewals Capital			11,877	400,000	(388,123)	575,000
<b>Total Property-Holding</b>				<b>11,877</b>	<b>400,000</b>	<b>(388,123)</b>	<b>575,000</b>		
Parking Operations	Renewals Capital		Car Park Buildings Equipment	51,757	-	51,757	-		
			Parking Meter Renewals	59,414	21,000	38,414	30,000		
	Total Renewals Capital			111,171	21,000	90,171	30,000		
<b>Total Parking Operations</b>				<b>111,171</b>	<b>21,000</b>	<b>90,171</b>	<b>30,000</b>		
<b>City Properties Total</b>				<b>11,355,005</b>	<b>18,824,800</b>	<b>(7,469,795)</b>	<b>22,388,000</b>		

**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget	
Community Recreation	Aquatic Services	New Capital	Moana Pool Improvements	(5,839)	10,000	(15,839)	20,000	
			Mosgiel Pool	18,320	-	18,320	-	
		Total New Capital			12,481	10,000	2,481	20,000
		Renewals Capital	Moana Pool Renewals	2,138,533	4,473,000	(2,334,467)	8,634,000	
			Port Chalmers Pool Renewals	8,822	20,000	(11,178)	20,000	
			St Clair Pool Renewals	4,822	40,000	(35,178)	40,000	
	Total Renewals Capital			2,152,177	4,533,000	(2,380,823)	8,694,000	
	<b>Total Aquatic Services</b>				<b>2,164,658</b>	<b>4,543,000</b>	<b>(2,378,342)</b>	<b>8,714,000</b>
	Botanic Gardens	New Capital	Botanic Garden Improvements		17,698	15,000	2,698	30,000
			Total New Capital		17,698	15,000	2,698	30,000
		Renewals Capital	Botanic Garden Renewals		269,355	377,000	(107,645)	552,000
			Total Renewals Capital		269,355	377,000	(107,645)	552,000
	<b>Total Botanic Gardens</b>				<b>287,053</b>	<b>392,000</b>	<b>(104,947)</b>	<b>582,000</b>
	Cemeteries & Crematorium	New Capital	Cemetery Strategic Development		216,512	345,000	(128,488)	795,000
			City Wide Beam Expansion		56,560	164,285	(107,725)	230,000
		Total New Capital		273,072	509,285	(236,213)	1,025,000	
		Renewals Capital	Structures Renewals		27,550	60,000	(32,450)	130,000
			Total Renewals Capital		27,550	60,000	(32,450)	130,000
	<b>Total Cemeteries &amp; Crematorium</b>				<b>300,623</b>	<b>569,285</b>	<b>(268,662)</b>	<b>1,155,000</b>
	Parks & Recreation	New Capital	Destination Playgrounds		-	-	-	200,000
			Playground Improvements		112,380	111,000	1,380	297,000
			Recreation Facilities Improvem		309,953	270,000	39,953	420,000
			Track Network Development		9,377	15,000	(5,623)	30,000
Total New Capital		431,710	396,000	35,710	947,000			
Renewals Capital		Greenspace Renewals		387,845	340,000	47,845	460,000	
		Playground Renewals		751,474	810,000	(58,526)	1,425,000	
	Recreation Facilities Renewals		547,338	865,000	(317,662)	1,665,000		
Total Renewals Capital		1,686,656	2,015,000	(328,344)	3,550,000			
<b>Total Parks &amp; Recreation</b>				<b>2,118,366</b>	<b>2,411,000</b>	<b>(292,634)</b>	<b>4,497,000</b>	
<b>Community Recreation Total</b>				<b>4,870,700</b>	<b>7,915,285</b>	<b>(3,044,585)</b>	<b>14,948,000</b>	

**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget		
Creative and Cultural Vib	Dunedin Public Art Gallery	New Capital	Acquisitions - DPAG Society Funded	7,735	10,000	(2,265)	30,000		
			Acquisitions - Rates Funded	73,000	50,000	23,000	130,000		
			Acquisitions Donation Funded	36,000	-	36,000	35,000		
			Minor Capital Works/Equipment	34,183	50,000	(15,817)	100,000		
		Total New Capital			150,918	110,000	40,918	295,000	
		Renewals Capital	Heating and Ventilation System	-	10,000	(10,000)	30,000		
		Total Renewals Capital			-	10,000	(10,000)	30,000	
		<b>Total Dunedin Public Art Gallery</b>			<b>150,918</b>	<b>120,000</b>	<b>30,918</b>	<b>325,000</b>	
		Dunedin Public Libraries		New Capital	Heritage Collection Purchases-Rates Funded	31,067	40,000	(8,933)	60,000
					Heritage Collection Purchases-Trust Funded	1,133	9,400	(8,267)	10,000
South Dunedin Library Opening Collection	40,734				60,000	(19,266)	60,000		
Total New Capital				72,934	109,400	(36,466)	130,000		
Renewals Capital	Acquisitions - Operational Collection			601,802	704,000	(102,198)	996,000		
Minor Capital Equipment	39,670			32,000	7,670	55,000			
Total Renewals Capital			641,472	736,000	(94,528)	1,051,000			
<b>Total Dunedin Public Libraries</b>			<b>714,406</b>	<b>845,400</b>	<b>(130,994)</b>	<b>1,181,000</b>			
Toitu Otago Settlers Museum		New Capital	Acquisitions - Rates Funded	37,652	5,000	32,652	50,000		
			Minor Capital Works	37,182	20,000	17,182	40,000		
			New Gallery Space - Theatre	431,316	464,000	(32,685)	464,000		
		Total New Capital			506,149	489,000	17,149	554,000	
		Renewals Capital	Minor Equipment Renewals	49,167	75,000	(25,833)	100,000		
		Plant Renewal	24,589	40,000	(15,411)	60,000			
Total Renewals Capital			73,756	115,000	(41,244)	160,000			
<b>Total Toitu Otago Settlers Museum</b>			<b>579,905</b>	<b>604,000</b>	<b>(24,095)</b>	<b>714,000</b>			
<b>Creative and Cultural Vibrancy Total</b>				<b>1,445,228</b>	<b>1,569,400</b>	<b>(124,172)</b>	<b>2,220,000</b>		
Governance and Support	Fleet Operations	New Capital	EV Charging Infrastructure	-	-	-	250,000		
		Total New Capital			-	-	-	250,000	
		Renewals Capital	Fleet Replacement	185,719	346,000	(160,281)	590,000		
	Total Renewals Capital			185,719	346,000	(160,281)	590,000		
	<b>Total Fleet Operations</b>			<b>185,719</b>	<b>346,000</b>	<b>(160,281)</b>	<b>840,000</b>		
	Business Information Services	New Capital	eServices & Online Services	(184,751)	666,668	(851,419)	1,000,000		
			New & Refreshed Internal IT Systems	93,150	466,000	(372,850)	466,000		
			Replacement & Upgrade Internal	-	66,664	(66,664)	100,000		
			Total New Capital			(91,601)	1,199,332	(1,290,933)	1,566,000
			Renewals Capital	New & Refreshed Internal IT Systems	-	250,000	(250,000)	800,000	
Replacement & Upgrade Internal	380,966	733,332	(352,366)	1,100,000					
Total Renewals Capital			380,966	983,332	(602,366)	1,900,000			
<b>Total Business Information Services</b>			<b>289,365</b>	<b>2,182,664</b>	<b>(1,893,299)</b>	<b>3,466,000</b>			
<b>Governance and Support Service Total</b>				<b>475,084</b>	<b>2,528,664</b>	<b>(2,053,580)</b>	<b>4,306,000</b>		

**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget		
Regulatory Services	Compliance Solutions	Renewals Capital	Dog Park & Stock Pound Maintenance	-	5,000	(5,000)	5,000		
		Total Renewals Capital		-	5,000	(5,000)	5,000		
	<b>Total Compliance Solutions</b>				-	<b>5,000</b>	<b>(5,000)</b>	<b>5,000</b>	
<b>Regulatory Services Total</b>				-	<b>5,000</b>	<b>(5,000)</b>	<b>5,000</b>		
Resilient City	City Development	New Capital	Street Trees and Furniture	-	55,000	(55,000)	325,000		
		Total New Capital		-	55,000	(55,000)	325,000		
	<b>Total City Development</b>				-	<b>55,000</b>	<b>(55,000)</b>	<b>325,000</b>	
	Civil Defence	New Capital	Plant Equipment		-	41,250	(41,250)	55,000	
		Total New Capital			-	41,250	(41,250)	55,000	
	<b>Total Civil Defence</b>				-	<b>41,250</b>	<b>(41,250)</b>	<b>55,000</b>	
	Task Force Green	Renewals Capital	Minor Equipment Renewals		-	5,000	(5,000)	5,000	
Total Renewals Capital			-	5,000	(5,000)	5,000			
<b>Total Task Force Green</b>				-	<b>5,000</b>	<b>(5,000)</b>	<b>5,000</b>		
<b>Resilient City Total</b>				-	<b>101,250</b>	<b>(101,250)</b>	<b>385,000</b>		
Roading and Footpaths	Shaping Future Dunedin	New Capital	Central City Cycle & Pedestrian Improvements	601,623	2,600,000	(1,998,377)	3,000,000		
			Central City Parking Management	30,310	1,200,000	(1,169,690)	1,200,000		
			Harbour Arterial Efficiency Improvements	226,106	1,000,000	(773,894)	2,800,000		
			Mosgiel Park and Ride	187,338	2,800,000	(2,612,662)	5,000,000		
			Total New Capital			1,045,377	7,600,000	(6,554,623)	12,000,000
	<b>Total Shaping Future Dunedin</b>				<b>1,045,377</b>	<b>7,600,000</b>	<b>(6,554,623)</b>	<b>12,000,000</b>	
	Transport	New Capital	Coastal Plan		-	593,000	(593,000)	1,184,000	
			Crown Resilience Programme 2024-2027		-	-	-	750,000	
			Dunedin Urban Cycleways		1,761,364	1,431,000	330,364	1,431,000	
			Low Cost, Low Risk Improvement		342,716	650,000	(307,284)	1,000,000	
			Peninsula Connection Boardwalk		125,972	450,000	(324,028)	1,500,000	
			Total New Capital			2,230,051	3,124,000	(893,949)	5,865,000
			Renewals Capital	Emergency Works		2,888,376	3,940,000	(1,051,624)	3,940,000
				Footpath Renewals		39,029	3,814,000	(3,774,971)	5,904,000
				Gravel Road Re metaling		745,770	591,900	153,870	1,219,000
				Major drainage control		1,072,330	3,897,000	(2,824,670)	6,266,000
				Pavement Rehabilitation		3,896,858	2,668,000	1,228,858	3,335,000
				Pavement Renewals		9,769,641	9,435,000	334,641	11,135,000
				Structure Component Replacement		362,482	1,377,000	(1,014,518)	1,908,000
				Structure Component Replacement Seawalls		11,785	146,000	(134,215)	196,000
				Structure Component Replacement Seawalls Railings		-	300,000	(300,000)	400,000
				Traffic Services Renewal		926,450	1,208,000	(281,550)	1,613,000
			Total Renewals Capital			19,712,721	27,376,900	(7,664,179)	35,916,000
<b>Total Transport</b>				<b>21,942,772</b>	<b>30,500,900</b>	<b>(8,558,128)</b>	<b>41,781,000</b>		
Central City Upgrade	New Capital	Central City Upgrade Bath St		1,919,895	939,000	980,895	939,000		
		Central City Upgrade Retail Quarter		259,861	40,000	219,861	40,000		
		Total New Capital			2,179,756	979,000	1,200,756	979,000	
<b>Total Central City Upgrade</b>			<b>2,179,756</b>	<b>979,000</b>	<b>1,200,756</b>	<b>979,000</b>			
<b>Roading and Footpaths Total</b>				<b>25,167,906</b>	<b>39,079,900</b>	<b>(13,911,994)</b>	<b>54,760,000</b>		

**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget	
Three Waters	Stormwater	New Capital	Mosgiel Stormwater Pumpstation and Networks	1,885,551	1,616,000	269,551	1,616,000	
			Mosgiel Stormwater Upgrades	625,000	625,000	(0)	625,000	
			Network Resilience & Efficiency	-	30,835	(30,835)	50,000	
			New Capital Supporting Growth	68,095	269,485	(201,390)	437,000	
			New Resource Consents	-	13,565	(13,565)	22,000	
			South Dunedin Flood Alleviation	275,995	154,165	121,830	250,000	
			South Dunedin Short Term Option	140,111	416,665	(276,554)	750,000	
			Stormwater New Capital Other	18,537	-	18,537	-	
			<b>Total New Capital</b>	<b>3,013,289</b>	<b>3,125,715</b>	<b>(112,426)</b>	<b>3,750,000</b>	
			Renewals Capital	Central City Renewals	75,009	-	75,009	-
	Mosgiel Stormwater Pumpstation and Networks	16,776		385,415	(368,639)	625,000		
	Other Stormwater Renewals	1,553,196		3,488,485	(1,935,289)	5,657,000		
	Renewals Supporting Growth	20,750		1,171,665	(1,150,915)	1,900,000		
	Stormwater Pumpstation Renewal	(2,859)		-	(2,859)	-		
	<b>Total Renewals Capital</b>	<b>1,662,872</b>		<b>5,045,565</b>	<b>(3,382,693)</b>	<b>8,182,000</b>		
	<b>Total Stormwater</b>			<b>4,676,161</b>	<b>8,171,280</b>	<b>(3,495,119)</b>	<b>11,932,000</b>	
	Wastewater	New Capital	Bioresources Facility	55,134	185,000	(129,866)	300,000	
			Metro Wastewater Treatment Plant Resilience	(38)	2,644,265	(2,644,303)	4,288,000	
			Network Resilience & Efficiency	-	92,500	(92,500)	150,000	
			New Capital Supporting Growth	80,276	456,950	(376,674)	741,000	
			Rural Wastewater Schemes	6,802	863,335	(856,533)	1,400,000	
			Wastewater New Capital Other	1,419,474	285,517	1,133,958	463,000	
			<b>Total New Capital</b>	<b>1,561,648</b>	<b>4,527,567</b>	<b>(2,965,918)</b>	<b>7,342,000</b>	
		Renewals Capital	Central City Renewals	20,374	-	20,374	-	
			Metro Wastewater Treatment Plant Resilience	2,003,532	2,437,685	(434,153)	3,953,000	
			Musselburgh to Tahuna Link	49,647	2,666,666	(2,617,019)	8,000,000	
			Other Wastewater Renewals	10,588,247	5,425,430	5,162,817	8,798,000	
Renewals Supporting Growth			39,727	195,485	(155,758)	317,000		
Rural Wastewater Schemes			274,140	-	274,140	-		
Wastewater Pumpstation Renewal			2,687,374	2,775,000	(87,626)	4,500,000		
<b>Total Renewals Capital</b>			<b>15,663,041</b>	<b>13,500,266</b>	<b>2,162,775</b>	<b>25,568,000</b>		
<b>Total Wastewater</b>					<b>17,224,689</b>	<b>18,027,833</b>	<b>(803,143)</b>	<b>32,910,000</b>
Water Supply			New Capital	New Capital Supporting Growth	187,817	786,250	(598,433)	1,275,000
	Port Chalmers Water Supply	9,009		-	9,009	-		
	Water Efficiency	28,860		857,165	(828,305)	1,390,000		
	Water New Capital Other	3,077,081		3,453,953	(376,872)	5,601,000		
	Water Supply Resilience	2,904,430		2,082,485	821,945	3,377,000		
	<b>Total New Capital</b>	<b>6,207,197</b>	<b>7,179,853</b>	<b>(972,656)</b>	<b>11,643,000</b>			
	Renewals Capital	Central City Renewals	4,507	-	4,507	-		
		Dam Safety Action Plan	1,434	-	1,434	-		
		Other Water Renewals	11,590,935	9,537,998	2,052,937	13,765,000		
		Port Chalmers Water Supply	5,405,436	5,535,200	(129,764)	8,976,000		
		Renewals Supporting Growth	39,566	337,315	(297,749)	547,000		
Water Supply Resilience		3,223,130	4,865,001	(1,641,871)	7,350,000			
<b>Total Renewals Capital</b>	<b>20,265,008</b>	<b>20,275,514</b>	<b>(10,506)</b>	<b>30,638,000</b>				
<b>Total Water Supply</b>			<b>26,472,205</b>	<b>27,455,367</b>	<b>(983,162)</b>	<b>42,281,000</b>		

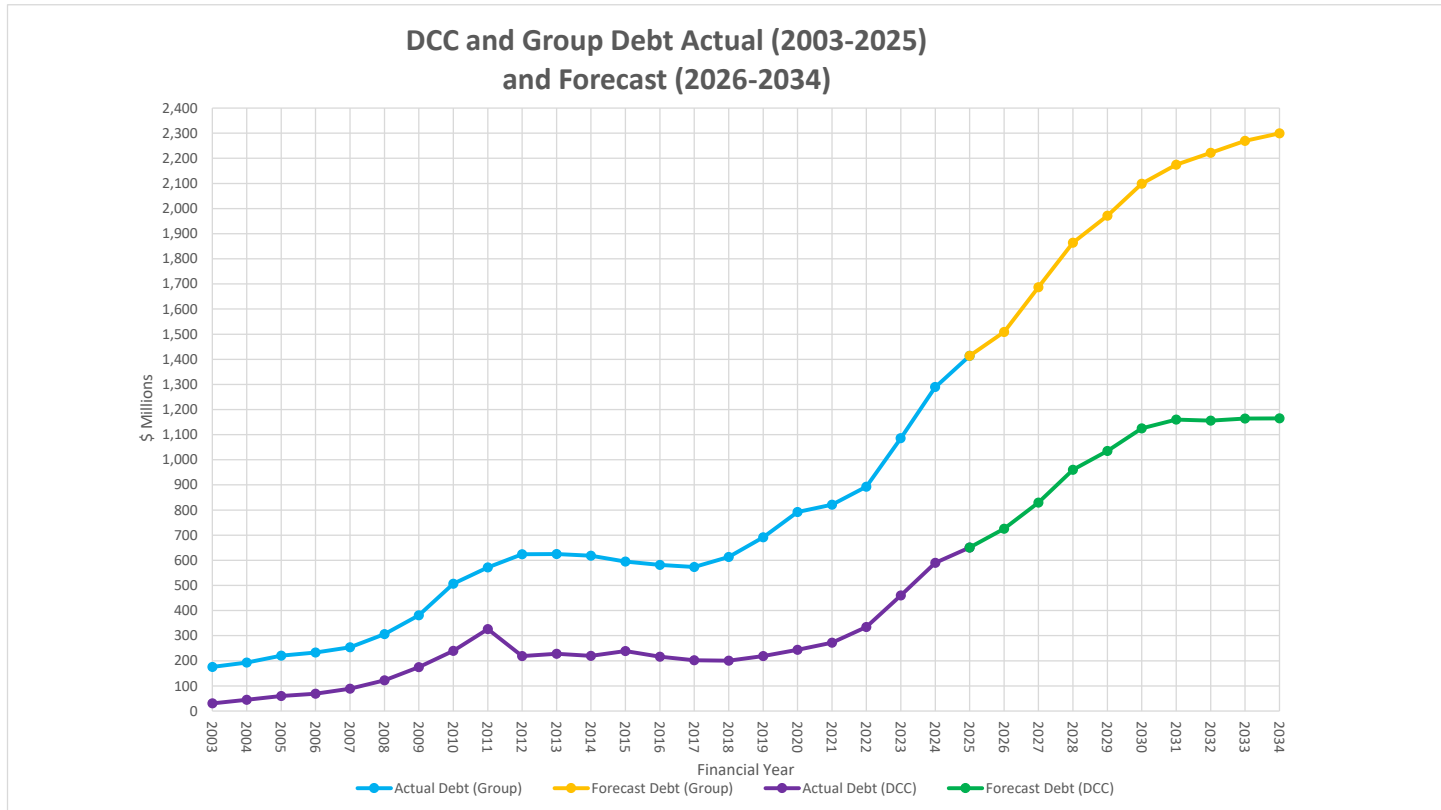
**Capital Expenditure Detail by Activity  
For the Month Ending 28 February 2026**

New Group Activity	Activity Name	Expenditure Type	Project Name	YTD Actual	YTD Budget	YTD Var	FY Budget
<b>Three Waters Total</b>				<b>48,373,056</b>	<b>53,654,480</b>	<b>(5,281,424)</b>	<b>87,123,000</b>
<b>Vibrant Economy</b>	<b>Destination Marketing</b>	New Capital	Digital Content - Camera and Video Gear	(573)	-	(573)	-
		Total New Capital		(573)	-	(573)	-
	<b>Total Destination Marketing</b>			<b>(573)</b>	<b>-</b>	<b>(573)</b>	<b>-</b>
	<b>Events</b>	New Capital	Plant Equipment	97,236	-	97,236	-
		Total New Capital		97,236	-	97,236	-
	<b>Total Events</b>			<b>97,236</b>	<b>-</b>	<b>97,236</b>	<b>-</b>
<b>Vibrant Economy Total</b>				<b>96,663</b>	<b>-</b>	<b>96,663</b>	<b>-</b>
<b>Waste Minimisation</b>	<b>Waste Futures</b>	New Capital	Bulk Waste System	189,945	150,000	39,945	500,000
			Community Recycling Centres	-	-	-	200,000
			Construction and Demolition Facility	80,521	200,000	(119,479)	400,000
			Glass Facility	3,538	2,375,000	(2,371,462)	2,525,000
			Material Recovery Facility	2,099,709	3,550,000	(1,450,291)	21,550,000
			Organics Facility	162,680	1,700,000	(1,537,320)	8,900,000
			Resource Recov Park Precinct	308,750	5,439,000	(5,130,250)	7,404,000
			Smooth Hill Landfill	26,182	-	26,182	-
		Total New Capital		2,871,325	13,414,000	(10,542,675)	41,479,000
	<b>Total Waste Futures</b>			<b>2,871,325</b>	<b>13,414,000</b>	<b>(10,542,675)</b>	<b>41,479,000</b>
	<b>Waste &amp; Environmental Solutic</b>	New Capital	Community Recycling Hubs	20,431	20,000	431	25,000
			Green Island Landfill Aftercare	466,961	675,000	(208,039)	1,577,000
			Green Island Landfill Gas Collection System	117,585	400,000	(282,415)	650,000
			Green Island Landfill Leachate System	28,040	-	28,040	-
			Green Island Landfill Southern Valley Leachate Drain	151,133	325,000	(173,867)	800,000
			Sawyers Bay Closed Landfill	607	-	607	-
		Total New Capital		784,758	1,420,000	(635,242)	3,052,000
		Renewals Capital	Forester Park Landfill Culvert	1,465	-	1,465	-
			Green Island Landfill and Transfer Station	10,400	95,000	(84,600)	155,000
			Green Island Leachate System Pump/Pumpstation	9,539	7,000	2,539	15,000
			Kerbside Bin Replacements	63,241	153,750	(90,509)	205,000
			Middlemarch Closed Landfill	-	-	-	11,000
			Public Place Recycling and Rubbish Bins	95,773	110,000	(14,227)	165,000
		Total Renewals Capital		180,417	365,750	(185,333)	551,000
	<b>Total Waste &amp; Environmental Solution</b>			<b>965,176</b>	<b>1,785,750</b>	<b>(820,574)</b>	<b>3,603,000</b>
<b>Waste Minimisation Total</b>				<b>3,836,500</b>	<b>15,199,750</b>	<b>(11,363,250)</b>	<b>45,082,000</b>
<b>Grand Total</b>				<b>95,620,142</b>	<b>138,878,529</b>	<b>(43,258,387)</b>	<b>231,217,000</b>

**DUNEDIN CITY COUNCIL**

**Summary of Operating Variances**  
**For the Eight Months Ending 28 February 2026**  
Amount : '\$'000

Group	Year to Date Surplus(Deficit)			Year to Date Variance Favourable (Unfavourable)							
	Actual	Budget	Variance	Rates Revenue	Operating Revenue	Internal Revenue	Staff	Ops & Other Exps	Internal Costs	Interest	Depr'n
City Properties	(1,632)	(1,767)	135	-	100	(18)	(176)	(569)	-	260	538
Community Recreation	1,048	(215)	1,263	-	(173)	-	156	706	6	92	476
Creative and Cultural Vibrancy	(1,323)	(1,776)	453	-	(300)	(3)	214	303	2	35	202
Governance and Support Service	(716)	(2,002)	1,286	(57)	747	37	152	(106)	139	(74)	448
Regulatory Services	338	277	61	-	(915)	(11)	943	18	18	-	8
Resilient City	89	(529)	618	-	(38)	2	382	276	(4)	-	-
Roading and Footpaths	1,221	4,556	(3,335)	(2)	(5,103)	-	671	562	(10)	368	179
Treaty Partnership	236	95	141	-	-	15	(11)	137	-	-	-
3 Waters	(7,992)	(12,481)	4,489	192	1,051	(153)	354	2,524	314	657	(450)
Vibrant Economy	336	(294)	630	-	95	(5)	51	492	(2)	-	(1)
Waste Minimisation	1,149	(75)	1,224	35	(171)	(420)	3	1,484	93	159	41
<b>Total Council (excluding Waipori)</b>	<b>(7,246)</b>	<b>(14,211)</b>	<b>6,965</b>	<b>168</b>	<b>(4,707)</b>	<b>(556)</b>	<b>2,739</b>	<b>5,827</b>	<b>556</b>	<b>1,497</b>	<b>1,441</b>
Waipori Fund	6,046	3,356	2,690	-	2,702	-	-	(12)	-	-	-
<b>Total Council</b>	<b>(1,200)</b>	<b>(10,855)</b>	<b>9,655</b>	<b>168</b>	<b>(2,005)</b>	<b>(556)</b>	<b>2,739</b>	<b>5,815</b>	<b>556</b>	<b>1,497</b>	<b>1,441</b>



**Sources:**

Actual debt: Dunedin City Council annual reports from 2003 to 2025.

Forecast debt (Group): Dunedin City Treasury Ltd Statement of Intent for the year ending 30 June 2026, Dunedin City Holdings Ltd projections and the DCC Draft Annual Plan 2026/27.

## PROPOSED EVENT ROAD CLOSURES

Department: Transport

### EXECUTIVE SUMMARY

- 1 The DCC has received temporary road closure applications relating to the following events:
  - i) International Dance Day Celebration
  - ii) Graduation Parades
- 2 This report recommends that Council approves the temporary closure of the affected roads.

### RECOMMENDATIONS

That the Council:

- a) **Resolves** to close the roads detailed below (pursuant to Section 319, Section 342, and Schedule 10 clause 11(e) of the Local Government Act 1974 (LGA 1974)):

**i) International Dance Day Celebration**

Saturday, 2 May 2026*	12.00pm to 3.00pm	<ul style="list-style-type: none"> <li>• George Street, between Hanover Street and St Andrew Street</li> </ul>
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\*Contingency day is Sunday, 3 May 2026

**ii) Graduation Parades**

Saturday, 9 May 2026  AND  Saturday, 16 May 2026	11.00am to 11.45am	<ul style="list-style-type: none"> <li>• Great King Street, between Frederick Street and Albany Street</li> </ul>
	11.10am to 12.00pm	<ul style="list-style-type: none"> <li>• Frederick Street, between Great King Street and George Street</li> <li>• George Street, between Frederick Street and Moray Place</li> </ul>
	11.10am to 12.30pm	<ul style="list-style-type: none"> <li>• Moray Place, between George Street and Upper Stuart Street</li> <li>• Filleul Street, between Moray Place and St Andrew Street.</li> </ul>
	11.30am to 12.15pm	<ul style="list-style-type: none"> <li>• Intersection of George Street and Moray Place.</li> </ul>

### BACKGROUND

- 3 Council's Dunedin Festival and Events Plan supports the goal of a successful city with a diverse, innovative, and productive economy and a hub for skill and talent.
- 4 The areas proposed to be used for these events are legal roads and can therefore be temporarily closed to normal traffic if statutory temporary road closure procedures are followed. The

procedures are set out in Section 319 of the LGA 1974 and give Council the power to stop or close any road (or part of a road) within the parameters of Section 342 and Schedule 10 of the LGA 1974 (Schedule 10 is included as Attachment A).

- 5 These procedures include:
- Consultation with the New Zealand Transport Authority Waka Kotahi and the Police.
  - Public notice being given of the proposal to close any road (or part of a road), and public notice of a decision to close the road.
  - Council being satisfied that traffic is not likely to be unreasonably impeded.
- 6 A resolution of Council is required where a proposal to temporarily close a road relates to public functions.
- 7 Council is required to give public notice of its decision. This notice will be published after this meeting and prior to the event, if approved.

## **DISCUSSION**

### **Consultation and Notification**

- 8 The Police and the New Zealand Transport Authority Waka Kotahi have no objections to the proposed road closures.
- 9 On Saturday, 14 March 2026 the proposed temporary road closures were advertised in the Otago Daily Times (Attachment B) with a deadline for feedback.
- 10 Schedule 10 clause 11(e) states a road cannot be closed more than 31 days in the aggregate in any one year. This limit will not be exceeded by the approval of the proposed temporary road closures.

### **Traffic Impacts**

- 11 The event locations of these events have had identical road closures for the same, or similar event(s) in prior years without causing unreasonable delays to the travelling public.
- 12 Emergency services and public transport services will be managed through the temporary traffic management process.
- 13 The Temporary Traffic Management Plan process ensures that other issues such as temporary relocation of certain parking (e.g. taxi, mobility and Authorised Vehicles Only) are managed.

## **OPTIONS**

- 14 Note any amendment to this report's recommendations cannot be implemented without further consultation with the affected parties, New Zealand Transport Agency Waka Kotahi, the Police, and verifying that traffic impacts are acceptable.

### **Option One – Recommended Option**

- 15 That the Council closes the sections of road as recommended in this report.

*Advantages*

- Roads can be closed, and the event will be able to proceed.
- The closures will assist in realising the economic, social, and cultural benefits associated with the events.

*Disadvantages*

- There will be temporary loss of vehicular access through the closed areas. However, there are detours available, and safety can be assured using temporary traffic management.

**Option Two – Status Quo**

16 That the Council decides not to close the roads in question.

*Advantages*

- There would be no detour required for the travelling public, and the roads would be able to be used as normal.

*Disadvantages*

- The events would not be able to go ahead, and the benefits of the events would be lost.

**NEXT STEPS**

17 Should the resolution be made to temporarily close the roads, Council staff will accept the temporary traffic management plans that have been received for the events and notify the public of the closures.

**Signatories**

Authoriser:	Jeanine Benson - Group Manager Transport Scott MacLean - General Manager, City Services
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**Attachments**

	<b>Title</b>	<b>Page</b>
<a href="#">↓A</a>	Local Government Act 1974, Schedule 10	281
<a href="#">↓B</a>	ODT Advert - 14 March 2026	286

**SUMMARY OF CONSIDERATIONS**

***Fit with purpose of Local Government***

This decision promotes the social well-being of communities in the present and for the future.

***Fit with strategic framework***

	Contributes	Detracts	Not applicable
Social Wellbeing Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Economic Development Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
Environment Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Arts and Culture Strategy	✓	<input type="checkbox"/>	<input type="checkbox"/>
3 Waters Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Future Development Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Integrated Transport Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Parks and Recreation Strategy	<input type="checkbox"/>	<input type="checkbox"/>	✓
Other strategic projects/policies/plans	✓	<input type="checkbox"/>	<input type="checkbox"/>

Events contribute to the Strategic Framework. Events contribute to the Economic Development Strategy, the Social Wellbeing Strategy. There is a Festival and Events Plan 2018-2023.

***Māori Impact Statement***

Mana whenua have not been directly engaged with in relation to these road closures.

***Sustainability***

There are no implications for sustainability.

***LTP/Annual Plan / Financial Strategy /Infrastructure Strategy***

There are no implications, as the decision is a regulatory one and there are no direct costs to Council.

***Financial considerations***

There are no financial implications. The cost of the proposed road closure is not a cost to Council.

***Significance***

This decision is considered low in terms of the Council's Significance and Engagement Policy.

***Engagement – external***

There has been external engagement (as required by the LGA 1974), with the Police and New Zealand Transport Agency Waka Kotahi. Affected parties were notified and provided a time period for feedback.

***Engagement - internal***

There has been engagement with DCC Events and Transport. There is support for the events to proceed.

***Risks: Legal / Health and Safety etc.***

There are no identified risks should the recommended resolution be made.

***Conflict of Interest***

There are no known conflicts of interest.

***Community Boards***

There are no implications for Community Boards.

Schedule 10

Local Government Act 1974

Version as at  
1 July 2022

**Schedule 10**  
**Conditions as to stopping of roads and the temporary prohibition of**  
**traffic on roads**

ss 319(h), 342

Schedule 10: inserted, on 1 April 1979, by section 3(1) of the Local Government Amendment Act 1978 (1978 No 43).

*Stopping of roads*

- 1 The council shall prepare a plan of the road proposed to be stopped, together with an explanation as to why the road is to be stopped and the purpose or purposes to which the stopped road will be put, and a survey made and a plan prepared of any new road proposed to be made in lieu thereof, showing the lands through which it is proposed to pass, and the owners and occupiers of those lands so far as known, and shall lodge the plan in the office of the Chief Surveyor of the land district in which the road is situated. The plan shall separately show any area of esplanade reserve which will become vested in the council under section 345(3).

Schedule 10 clause 1: amended, on 1 October 1991, by section 362 of the Resource Management Act 1991 (1991 No 69).

- 2 On receipt of the Chief Surveyor's notice of approval and plan number the council shall open the plan for public inspection at the office of the council, and the council shall at least twice, at intervals of not less than 7 days, give public notice of the proposals and of the place where the plan may be inspected, and shall in the notice call upon persons objecting to the proposals to lodge their objections in writing at the office of the council on or before a date to be specified in the notice, being not earlier than 40 days after the date of the first publication thereof. The council shall also forthwith after that first publication serve a notice in the same form on the occupiers of all land adjoining the road proposed to be stopped or any new road proposed to be made in lieu thereof, and, in the case of any such land of which the occupier is not also the owner, on the owner of the land also, so far as they can be ascertained.
- 3 A notice of the proposed stoppage shall, during the period between the first publication of the notice and the expiration of the last day for lodging objections as aforesaid, be kept fixed in a conspicuous place at each end of the road proposed to be stopped:

provided that the council shall not be deemed to have failed to comply with the provisions of this clause in any case where any such notice is removed without the authority of the council, but in any such case the council shall, as soon as conveniently may be after being informed of the unauthorised removal of the notice, cause a new notice complying with the provisions of this clause to be affixed in place of the notice so removed and to be kept so affixed for the period aforesaid.

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Version as at 1 July 2022	Local Government Act 1974	Schedule 10
4	If no objections are received within the time limited as aforesaid, the council may by public notice declare that the road is stopped; and the road shall, subject to the council's compliance with clause 9, thereafter cease to be a road.	
5	If objections are received as aforesaid, the council shall, after the expiration of the period within which an objection must be lodged, unless it decides to allow the objections, send the objections together with the plans aforesaid, and a full description of the proposed alterations to the Environment Court.	Schedule 10 clause 5: amended, on 2 September 1996, pursuant to section 6(2)(a) of the Resource Management Amendment Act 1996 (1996 No 160).
6	The Environment Court shall consider the district plan, the plan of the road proposed to be stopped, the council's explanation under clause 1, and any objection made thereto by any person, and confirm, modify, or reverse the decision of the council which shall be final and conclusive on all questions.	Schedule 10 clause 6: replaced, on 1 October 1991, by section 362 of the Resource Management Act 1991 (1991 No 69). Schedule 10 clause 6: amended, on 2 September 1996, pursuant to section 6(2)(a) of the Resource Management Amendment Act 1996 (1996 No 160).
7	If the Environment Court reverses the decision of the council, no proceedings shall be entertained by the Environment Court for stopping the road for 2 years thereafter.	Schedule 10 clause 7: amended, on 2 September 1996, pursuant to section 6(2)(a) of the Resource Management Amendment Act 1996 (1996 No 160).
8	If the Environment Court confirms the decision of the council, the council may declare by public notice that the road is stopped; and the road shall, subject to the council's compliance with clause 9, thereafter cease to be a road.	Schedule 10 clause 8: amended, on 2 September 1996, pursuant to section 6(2)(a) of the Resource Management Amendment Act 1996 (1996 No 160).
9	Two copies of that notice and of the plans hereinbefore referred to shall be transmitted by the council for record in the office of the Chief Surveyor of the land district in which the road is situated, and no notice of the stoppage of the road shall take effect until that record is made.	
10	The Chief Surveyor shall allocate a new description of the land comprising the stopped road, and shall forward to the Registrar-General of Land or the Registrar of Deeds, as the case may require, a copy of that description and a copy of the notice and the plans transmitted to him by the council, and the Registrar shall amend his records accordingly.	Schedule 10 clause 10: amended, on 12 November 2018, by section 250 of the Land Transfer Act 2017 (2017 No 30).

Schedule 10

Local Government Act 1974

Version as at  
1 July 2022

*Temporary prohibition of traffic*

- 11 The council may, subject to such conditions as it thinks fit (including the imposition of a reasonable bond), and after consultation with the Police and the New Zealand Transport Agency, close any road or part of a road to all traffic or any specified type of traffic (including pedestrian traffic)—
- (a) while the road, or any drain, water race, pipe, or apparatus under, upon, or over the road is being constructed or repaired; or
  - (b) where, in order to resolve problems associated with traffic operations on a road network, experimental diversions of traffic are required; or
  - (c) during a period when public disorder exists or is anticipated; or
  - (d) when for any reason it is considered desirable that traffic should be temporarily diverted to other roads; or
  - (e) for a period or periods not exceeding in the aggregate 31 days in any year for any exhibition, fair, show, market, concert, film-making, race or other sporting event, or public function:

provided that no road may be closed for any purpose specified in paragraph (e) if that closure would, in the opinion of the council, be likely to impede traffic unreasonably.

Schedule 10 clause 11: replaced, on 14 August 1986, by section 14(1) of the Local Government Amendment Act (No 3) 1986 (1986 No 50).

Schedule 10 clause 11: amended, on 26 March 2015, by section 5 of the Local Government Act 1974 Amendment Act 2015 (2015 No 20).

- 11A The council shall give public notice of its intention to consider closing any road or part of a road under clause 11(e); and shall give public notice of any decision to close any road or part of a road under that provision.

Schedule 10 clause 11A: inserted, on 14 August 1986, by section 14(1) of the Local Government Amendment Act (No 3) 1986 (1986 No 50).

- 11B Where any road or part of a road is closed under clause 11(e), the council or, with the consent of the council, the promoter of any activity for the purpose of which the road has been closed may impose charges for the entry of persons and vehicles to the area of closed road, any structure erected on the road, or any structure or area under the control of the council or the promoter on adjoining land.

Schedule 10 clause 11B: inserted, on 14 August 1986, by section 14(1) of the Local Government Amendment Act (No 3) 1986 (1986 No 50).

- 11C Where any road or part of a road is closed under clause 11(e), the road or part of a road shall be deemed for the purposes of—

- (a) *[Repealed]*
- (b) the Traffic Regulations 1976:

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**Local Government Act 1974**

Schedule 10

- (c) the Transport (Drivers Licensing) Regulations 1985:
- (d) *[Repealed]*
- (e) the Transport (Vehicle Registration and Licensing) Notice 1986:
- (ea) the Land Transport Act 1998:
- (f) any enactment made in substitution for any enactment referred to in paragraphs (a) to (ea)—

not to be a road; but nothing in this clause shall affect the status of the road or part of a road as a public place for the purposes of this or any other enactment.

Schedule 10 clause 11C: inserted, on 14 August 1986, by section 14(1) of the Local Government Amendment Act (No 3) 1986 (1986 No 50).

Schedule 10 clause 11C(a): repealed, on 10 May 2011, by section 100(3) of the Land Transport (Road Safety and Other Matters) Amendment Act 2011 (2011 No 13).

Schedule 10 clause 11C(d): repealed, on 1 May 2011, by section 35(4) of the Land Transport Amendment Act 2009 (2009 No 17).

Schedule 10 clause 11C(ea): inserted, on 1 March 1999, by section 215(1) of the Land Transport Act 1998 (1998 No 110).

Schedule 10 clause 11C(f): amended, on 1 March 1999, by section 215(1) of the Land Transport Act 1998 (1998 No 110).

- 12 The powers conferred on the council by clause 11 (except paragraph (e)) may be exercised by the chairman on behalf of the council or by any officer of the council authorised by the council in that behalf.
- 13 Where it appears to the council that owing to climatic conditions the continued use of any road in a rural area, other than a State highway or government road, not being a road generally used by motor vehicles for business or commercial purposes or for the purpose of any public work, may cause damage to the road, the council may by resolution prohibit, either conditionally or absolutely, the use of that road by motor vehicles or by any specified class of motor vehicle for such period as the council considers necessary.
- 14 Where a road is closed under clause 13, an appropriate notice shall be posted at every entry to the road affected, and shall also be published in a newspaper circulating in the district.
- 15 A copy of every resolution made under clause 13 shall, within 1 week after the making thereof, be sent to the Minister of Transport, who may at any time, by notice to the council, disallow the resolution, in whole or in part, and thereupon the resolution, to the extent that it has been disallowed, shall be deemed to have been revoked.
- 16 No person shall—
  - (a) use a vehicle, or permit a vehicle to be used, on any road which is for the time being closed for such vehicles pursuant to clause 11; or

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Schedule 11

**Local Government Act 1974**

Version as at  
1 July 2022

- (aa) without the consent of the council or the promoter of any activity permitted by the council, enter or attempt to enter, or be present, on any road or part of a road that is for the time being closed to pedestrian traffic pursuant to clause 11; or
- (b) use a motor vehicle, or permit a motor vehicle to be used, on any road where its use has for the time being been prohibited by a resolution under clause 13.

Schedule 10 clause 16(aa): inserted, on 14 August 1986, by section 14(2) of the Local Government Amendment Act (No 3) 1986 (1986 No 50).

### **Schedule 11**

#### **Width of roads, access ways, and service lanes**

*[Expired]*

s 325(1)

Schedule 11: expired, on 1 January 1993, by section 325(3).

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**ODT Advert – 14 March 2026**

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## **TEMPORARY ROAD CLOSURE PROPOSALS**

### **May Graduation Parades – various streets**

The Council is considering closing Great King Street, between Frederick and Albany Streets, 11 am to 11.45am; Moray Place, between George and upper Stuart Streets, and Filleul Street, between Moray Place and St Andrew Street, 11.10am to 12.30pm; Frederick Street, between Great King and George Streets and George Street, between Frederick Street and Moray Place, 11.10am to 12pm; Intersection of George Street and Moray Place, 11.30am to 12.15pm, on Saturday, 9 May and Saturday 16 May, for these events.

### **International Dance Day Celebration – George Street**

The Council is considering closing George Street, between Hanover and St Andrew Streets, on Saturday, 2 May, 12pm to 3pm, for this event. Contingency date Sunday, 3 May.

These proposals will be considered at the meeting of the Dunedin City Council at 10am on Thursday, 23 April. Please provide any feedback on the proposal by emailing [tmp@dcc.govt.nz](mailto:tmp@dcc.govt.nz) before 5pm on Saturday, 21 March.

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## RESOLUTION TO EXCLUDE THE PUBLIC

That the Council excludes the public from the following part of the proceedings of this meeting (pursuant to the provisions of the Local Government Official Information and Meetings Act 1987) namely:

General subject of the matter to be considered	Reasons for passing this resolution in relation to each matter	Ground(s) under section 48(1) for the passing of this resolution	Reason for Confidentiality
C1 Confirmation of the Confidential Minutes of Ordinary Council meeting - 25 March 2026 - Public Excluded	<p>S7(2)(b)(ii) The withholding of the information is necessary to protect information where the making available of the information would be likely unreasonably to prejudice the commercial position of the person who supplied or who is the subject of the information.</p> <p>S7(2)(g) The withholding of the information is necessary to maintain legal professional privilege.</p> <p>S7(2)(h) The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.</p> <p>S7(2)(i) The withholding of the information is necessary to enable the local authority to</p>	.	

	<p>carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).</p> <p>S7(2)(a) The withholding of the information is necessary to protect the privacy of natural persons, including that of a deceased person.</p> <p>S7(2)(d) The withholding of the information is necessary to avoid prejudice to measures protecting the health and safety of members of the public.</p> <p>s48(1)(d) Check to make report confidential.</p>		
<p>C2 Confidential Council Action List Update</p>	<p>S7(2)(b)(ii) The withholding of the information is necessary to protect information where the making available of the information would be likely unreasonably to prejudice the commercial position of the person who supplied or who is the subject of the information.</p> <p>S7(2)(g) The withholding of the information is necessary to maintain legal professional privilege.</p> <p>S7(2)(h)</p>	<p>S48(1)(a) The public conduct of the part of the meeting would be likely to result in the disclosure of information for which good reason for withholding exists under section 7.</p>	

	<p>The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.</p> <p>S7(2)(i) The withholding of the information is necessary to enable the local authority to carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).</p>		
C3 Homelessness Outreach Service Procurement	<p>S7(2)(h) The withholding of the information is necessary to enable the local authority to carry out, without prejudice or disadvantage, commercial activities.</p> <p>S7(2)(i) The withholding of the information is necessary to enable the local authority to carry on, without prejudice or disadvantage, negotiations (including commercial and industrial negotiations).</p>	S48(1)(a) The public conduct of the part of the meeting would be likely to result in the disclosure of information for which good reason for withholding exists under section 7.	
C4 Destination Product Development	<p>S7(2)(h) The withholding of the information is necessary to enable the local authority to carry out, without prejudice or</p>	S48(1)(a) The public conduct of the part of the meeting would be likely to result in the disclosure of information for which good reason for	

	disadvantage, commercial activities.	withholding exists under section 7.	
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This resolution is made in reliance on Section 48(1)(a) of the Local Government Official Information and Meetings Act 1987, and the particular interest or interests protected by Section 6 or Section 7 of that Act, or Section 6 or Section 7 or Section 9 of the Official Information Act 1982, as the case may require, which would be prejudiced by the holding of the whole or the relevant part of the proceedings of the meeting in public are as shown above after each item.